

REGULAR COUNCIL MEETING AGENDA FOR WEDNESDAY AUGUST 7, 2024

We respectfully acknowledge that the land on which we gather and work is on the Unceded Traditional Territory of the K'ómoks First Nation, the traditional keepers of this land.

Meeting Location: Council Chambers, 1801B Beaufort Avenue, Comox

Call to Order: 5:00 p.m.

1. INTRODUCTION AND APPROVAL OF LATE ITEMS: NIL

2. ADOPTION OF AGENDA:

- a. [Adoption of Agenda](#)

THAT the August 7, 2024, Regular Council Meeting agenda be Adopted.

3. DELEGATIONS:

- (7) a. [Inspector Scott Mercer, OIC \(Comox Valley RCMP\): 2024 1st Quarter Report](#)
- (15) b. [Norman Laube, OCP/RZ 23-4 \(1966 Guthrie Road\): Overview of proposed 6-storey rental apartment and retail project at Highland Village.](#)
- c. [Chris Fields \(Rynic\): Economic Development and Downtown Action Plan](#)

4. ADOPTION OF MINUTES:

- (15) a. [Regular Council Meeting Minutes](#)
- THAT the Minutes of the Regular Council Meeting, held in Council Chambers on Wednesday, July 10, 2024, be Adopted.*
- (21) b. [Public Hearing Meeting Minutes](#)
- THAT the Minutes of the Public Hearing Meeting, held in d'Esterre Seniors' Centre on Wednesday, July 10, 2024, be Adopted.*

5. COUNCIL COMMITTEE MINUTES AND REPORTS: NIL

6. CONSENT AGENDA:

- (40) a. [Consent Agenda](#)

THAT the Consent Agenda items as follows be received:

(40)

a. [Consent Agenda](#)

1. *Laura Gilbert and William Hall Letter: Traffic Calming on Noel Avenue between the Intersections of Noel and Torrence Road and Noel and Guthrie Road*
2. *Marina and (James) Michael Fraser: Parking Complaint - Neptune Way*
3. *Tom Stauffer: Requesting a Bylaw Regarding Cats in Comox*
4. *Joyce and Jim Sparks: Solid Waste Cart Changes*
5. *Craig Freeman, Director (Merville Community Association): Relocation Request: Shakesides - Mack Liang Property*
6. *Sue Campbell: Request for Bylaw Amendment Regarding Noise*
7. *Bob Wells, Mayor (City of Courtenay): Strengthening Communities Grant*
8. *Lisa Domae, President and CEO (North Island College): Strategic Plan Year 3 Dashboard Report*
9. *Tiffany Trownson, Director of Public Affairs (BC Care Providers Association/EngAge BC): Intermunicipal Mobile Business Licence for Home Health Care*
10. *Laura Donovan: Letter Request - Suicide Prevention Support for Indigenous Children*
11. *Darrell Tomkins: Vandalism and Security in Marina Park*
12. *Debbi Topp: Digital Screen at Marina Park*
13. *Trish Johnson: Aggressive Dog and Pig Complaint*
14. *Nattanya Birkhaven: Parking Issues*

7. UNFINISHED BUSINESS:

(69)

a. [2077 Hector Road and 941 Aspen Road: RZ OCP 23-2, PR 24-9, Post-Public Hearing Report](#)

1. *That Comox Official Community Plan Amendment Bylaw 1685.11 be amended at Second Reading by:*
 - a) *deleting section 2(1) regarding Schedule "A" Part 2: Objectives and Policies, Section 2.1.1.3 Residential Policies;*
 - b) *deleting the text, "and a new east-west neighbourhood trail" from section 2(2)(d);*

- (69) a. [2077 Hector Road and 941 Aspen Road: RZ OCP 23-2, PR 24-9, Post-Public Hearing Report](#)
1. c) replacing "Schedule 4" in section 2(2)(e) with "Schedule 3";

d) replacing "Schedule 5" in section 2(2)(f) with "Schedule 4";

e) replacing Schedule 2 with a new "Schedule 2, as included in Attachment 5 of the August 7, 2024 Staff Report from the Planner II, titled "2077 Hector Road and 941 Aspen Road: RZ OCP 23-2, PR 24-9"; and,

f) renumbering accordingly.
 2. THAT Comox Zoning Amendment Bylaw 1850.46 be amended at Second Reading by:

a) deleting the text, "or required storey differential" from section 2Ea);

b) deleting the text, "or required storey differential" from section 2Ec);

c) deleting from Schedule 1A:
i) subsections (1) and (2) from section 211.8, and
ii) section 211.10;

d) deleting from Schedule 1B;
i) subsections (a) and (b) from subsection 211.9(1), and
ii) section 212.11;

e) deleting from Schedule 1C;
i) subsections (a) and (b) from subsection 310.8(1), and
ii) section 310.10; and,

f) renumbering accordingly.
 3. That Comox Subdivision and Development Servicing Amendment Bylaw 1261.18 be amended at Second Reading by:

a) deleting subsection 2(1)(f) and figure C-4.; and,

b) renumbering accordingly.

- (69) a. [2077 Hector Road and 941 Aspen Road: RZ OCP 23-2, PR 24-9, Post-Public Hearing Report](#)

4. *That Schedule 1, Outstanding Item 3(c) of Planning Report RZ OCP 23-2, PR 24-9 dated August 7th, 2024 be replaced with the following :*

Long term protection of the environmental areas and provision of a 15.0 metres natural buffer for a wetland, in accordance with RPBio report including a Wetland Management Plan (WMP) by an RPBio specifying measures for the protection of the wetland during land clearing and construction on 2077 Hector which includes oversight and monitoring requirements by a Qualified Environmental Professional during land clearing and construction. The WMP should also include reporting requirements to the Town during and after land clearing and construction is complete, as well as any other measures specified by the RPBio which includes measures already outlined in the Environmental Impact Assessment by Roe Environmental Ltd dated March 13, 2023, and the Environmental Memo by Roe Environmental Ltd dated May 7th 2024.

- (195) b. [Permissive Tax Exemption Policy CCL-025.01](#)

THAT Permissive Tax Exemption Policy CCL-025 be adopted as amended to include section 6.05, which states, "Any property that engages in behaviour which Council deems to be abhorrent, egregious, or criminal may be ineligible for future Permissive Tax Exemptions for a set period of time."

8. SPECIAL REPORTS: NIL

9. BYLAW ADOPTIONS: NIL

10. NEW BUSINESS:

(Separate attachment on website)

- a. [1966 Guthrie Road \(Phase 3\): First and Second Reading, Comox OCP Amendment Bylaw 1685.10 & Comox Zoning Amendment Bylaw 1850.43](#)

1. *THAT Comox OCP Amendment Bylaw 1685.10 be given First Reading .*
2. *THAT, having considered the Town of Comox May 2020 Housing Needs Report and Summary Form, the Comox Strathcona Waste Management 2012 Solid Waste Management Plan and the impact on the current Financial Plan, Comox Official Community Plan Amendment Bylaw 1685.10 be given Second Reading.*
3. *THAT Comox Zoning Amendment Bylaw 1850.43 be given First and Second Reading.*
4. *THAT a Public Hearing in respect of Comox OCP Amendment Bylaw 1685.10 and Comox Zoning Amendment Bylaw 1850.43 be scheduled for September 4, 2024, 6:00 pm at d'Esterre House 1801 Beaufort Avenue, that it include online participation via Zoom, and that the Town publish the requisite notices as required by the Local Government Act.*

- (202) b. [Planning Report PR 24-10: Amend Building Bylaw](#)
That Comox Building Bylaw Amendment No. 1472.14 be given First, Second, and Third Readings.
- (214) c. [2023 Statement of Financial Information](#)
THAT the 2023 Statement of Financial Information report as attached to the August 1, 2024, report from the Directory of Finance, titled "2023 Statement of Financial Information (SOFI)", be approved.
- (254) d. [Flag Protocol and Community Events Recognition Policies](#)
 1. *THAT the Flag Protocol Policy CCL-058 be adopted as amended to incorporate the flag-raising procedures from the Community Events Recognition Policy CCL-059, and to add September 30th, National Day for Truth and Reconciliation, to the protocol for flags flown at half-mast.*
 2. *THAT the discontinuation of the Community Events Recognition Policy CCL-059 be Approved.*
- (262) e. [Economic Development and Downtown Action Plan](#)

11. NOTICES OF MOTION: NIL

12. CORRESPONDENCE:

- (286) a. [Valerie Greenlaw \(Royal Canadian Legion\): 2024 Remembrance Day Parade and Flyby Request](#)
That the July 18, 2024 letter from Valerie Greenlaw of the Royal Canadian Legion Branch 160 Comox, requesting the Town's support for the annual Remembrance Day Ceremony and associated events be received and that permission be granted to:
- (a) Block off Comox Ave from Ellis to Nordin, 1 0:00am to 11 :30am for the Remembrance Day Parade and Ceremonies, for which we require the road permit.*
- (b) Use of the two (2) reviewing stands and barricades*
- (c) Permission to put up no marking signs on both sides of Comox Ave between Church and Port Augusta from 7:00am to 11 :30am, and*
- (d) Provide a letter to the Branch approving a flyby by the RCAF with the following working to be included:*
- "The municipality of Comox, British Columbia, Canada is supportive of a flyby by RCAF Aircraft as low as 500 feet for the planned Remembrance Day Ceremony being held in Comox, BC Canada, for transit, practice and shows on November 11 , 2024".*

- (287) b. [Nicole Darling, Director of Corporate Services \(Simba Investments\): One Pond Solution OPS Letter from Shawn Vincent: Pritchard 1000](#)

13. LATE ITEMS: NIL

14. REPORTS FROM MEMBERS OF COUNCIL: NIL

15. MEDIA QUESTION PERIOD: NIL

16. PUBLIC QUESTION PERIOD: NIL

17. RESOLUTION TO GO IN-CAMERA:

- a. [Exclude the Public](#)

That the Public be Excluded from the In-Camera session of Council on Wednesday August 7, 2024 pursuant to the following sub-sections of section 90 of the Community Charter:

(1)(a) personal information about an identifiable individual who holds or is being considered for a position as an officer, employee or agent of the municipality or another position appointed by the municipality;

18. RISE AND REPORT FROM IN-CAMERA: NIL

ADJOURNMENT



CORPORATE OFFICER



RECEIVED

July 19, 2024

TOWN OF COMOX

LOG: 24-290	REFER:	AGENDA: 07-Aug-24
FILE: 7580-20	ACTION: MR	

Copies: Council
JW/SR/CD

Comox Valley RCMP Detachment

QUARTERLY REPORT

TOWN OF COMOX

April 1, 2024 – June 30, 2024

Comox Valley RCMP Detachment Quarterly Report

April 1, 2024 to June 30, 2024

The Comox Valley Royal Canadian Mounted Police (RCMP) provides quarterly updates on policing in the community. The quarterly reports coincide with the Comox Valley RCMP Annual Performance reporting time lines in conjunction with Community Priorities.

First Quarter: April 1 to June 30

Second Quarter: July 1 to September 30

Third Quarter: October 1 to December 31

Fourth Quarter: January 1 to March 31

Calls for Service

In the first quarter of the 2024-2025 fiscal year, there were 621 Calls for Service in Comox. This was 17 files less than the 638 Calls for Service in the first quarter of the previous year.

Year	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Total
2019	203	169	210	247	262	254	262	281	230	217	195	191	2721
2020	220	253	220	222	257	253	265	296	225	250	203	153	2817
2021	162	153	168	220	217	216	299	240	208	191	189	190	2453
2022	160	160	182	183	191	171	233	228	206	225	157	197	2293
2023	161	162	194	153	248	237	295	271	208	243	224	194	2590
2024	159	184	177	199	209	213							1141

Most Common Call Types

In the first quarter of the 2024-2025 fiscal year, the most common Call for Service in Comox was Traffic Incident. Relative to the first quarter of the previous year, there were notable increases in Fraud (67 percent increase; 10 files), Missing Person (380 percent increase; 19 files), and Suicidal Person (229 percent increase; 16 files) Calls for Service. The Calls for Service related to Fraud ranged from fake deals on Facebook Marketplace to sophisticated international scams. Most of the Missing Person files pertained to residents of a Youth Group Home. With regard to the Suicidal Person files, there was no trend related to the person who was the subject of the Call for Service or the location of the file.

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	Final Case Type	First Quarter Data						% Change from 23-24 Q1	Difference from 23-24 Q1
		2019	2020	2021	2022	2023	2024		
1	TRAFFIC INCIDENT	69	68	88	62	70	72	3%	2
2	CHECK WELLBEING	33	46	47	49	50	53	6%	3
3	PROPERTY	60	47	39	28	58	52	-10%	-6
4	THEFT	70	37	39	14	30	35	17%	5
5	SUSPICIOUS CIRCUMSTANCES	35	36	32	32	35	34	-3%	-1
6	ASSIST POLICE / FIRE / AMBULANCE	13	23	24	16	21	31	48%	10
7	DISTURBANCE	41	44	34	34	54	28	-48%	-26
8	FRAUD	11	16	22	19	15	25	67%	10
9	MISSING PERSON	10	6	6	6	5	24	380%	19
10	SUICIDAL PERSON	8	7	13	14	7	23	229%	16

Most Common Call Locations

In the first quarter of the 2024-2025 fiscal year, the location in Comox with the most Calls for Service was a Youth Group Home.

	Location	First Quarter Data						% Change from 23-24 Q1	Difference from 23-24 Q1
		2019	2020	2021	2022	2023	2024		
1	Youth Group Home	1	0	0	0	12	23	92%	11
2	2187 COMOX AVE (Seascope Apartments)	16	12	13	4	11	16	45%	5
3	123 PORT AUGUSTA ST (Marina)	11	17	7	8	23	16	-30%	-7
4	0 COMOX	2	4	7	2	3	12	300%	9
5	12 HAWKINS RD (Goose Spit)	10	4	8	2	7	8	14%	1
6	1300 ELLENOR RD (BC Ferries Little River)	5	4	4	8	7	7	0%	0
7	215 PORT AUGUSTA ST (Comox Centre Mall)	7	19	16	10	8	6	-25%	-2
8	2100 GUTHRIE RD (Times Square Plaza)	1	2	0	0	2	6	200%	4
9	1250 KNIGHT RD	12	2	6	13	13	6	-54%	-7
10	COMOX AVE / ANDERTON RD	1	3	1	2	2	5	150%	3

Violent Crime

In the first quarter of the 2024-2025 fiscal year, there were 39 Violent Crime files in Comox. This was a 1-file decrease from the 40 Violent Crime files in the first quarter of the previous year.

Types of Violent Crime

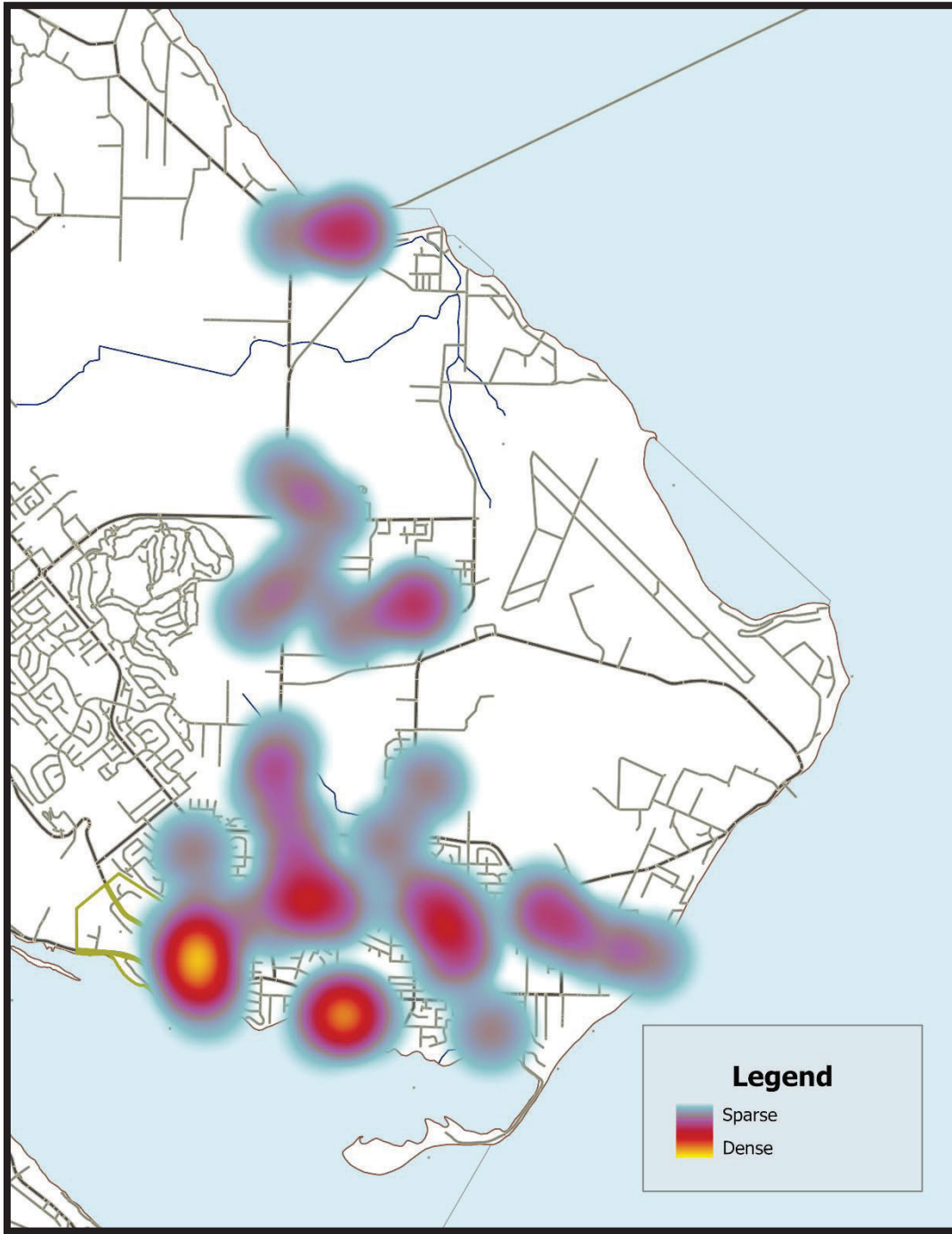
In the first quarter of the 2024-2025 fiscal year, the most common type of Violent Crime file in Comox was Assault. Relative to the first quarter of the previous fiscal year, there were notable increases in Harassment and Sex Offence files.

File Type	First Quarter Data						% Change from 23-24 Q1	Difference from 23-24 Q1
	2019	2020	2021	2022	2023	2024		
ASSAULTS	18	11	13	21	21	17	-19%	-4
HARASSMENT	14	15	13	10	4	9	125%	5
SEX OFFENCES	2	3	2	1	2	6	200%	4
UTTER THREATS	18	8	6	6	12	5	-58%	-7
EXTORTION	0	0	0	0	1	1	0%	0
KIDNAPPING	0	0	0	0	0	1		1
ROBBERY	2	0	0	0	0	0		0
Grand Total	54	37	34	38	40	39	-3%	-1

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Map of Violent Crime in Comox

In the first quarter of the 2024-2025 fiscal year, there were hotspots for Violent Crime files in West Comox and the Downtown Central Business District.



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Property Crime

In the first quarter of the 2024-2025 fiscal year, there were 101 Property Crime files in Comox. This was a 2-file increase over the 99 Property Crime files in the first quarter of the previous year.

Types of Property Crime

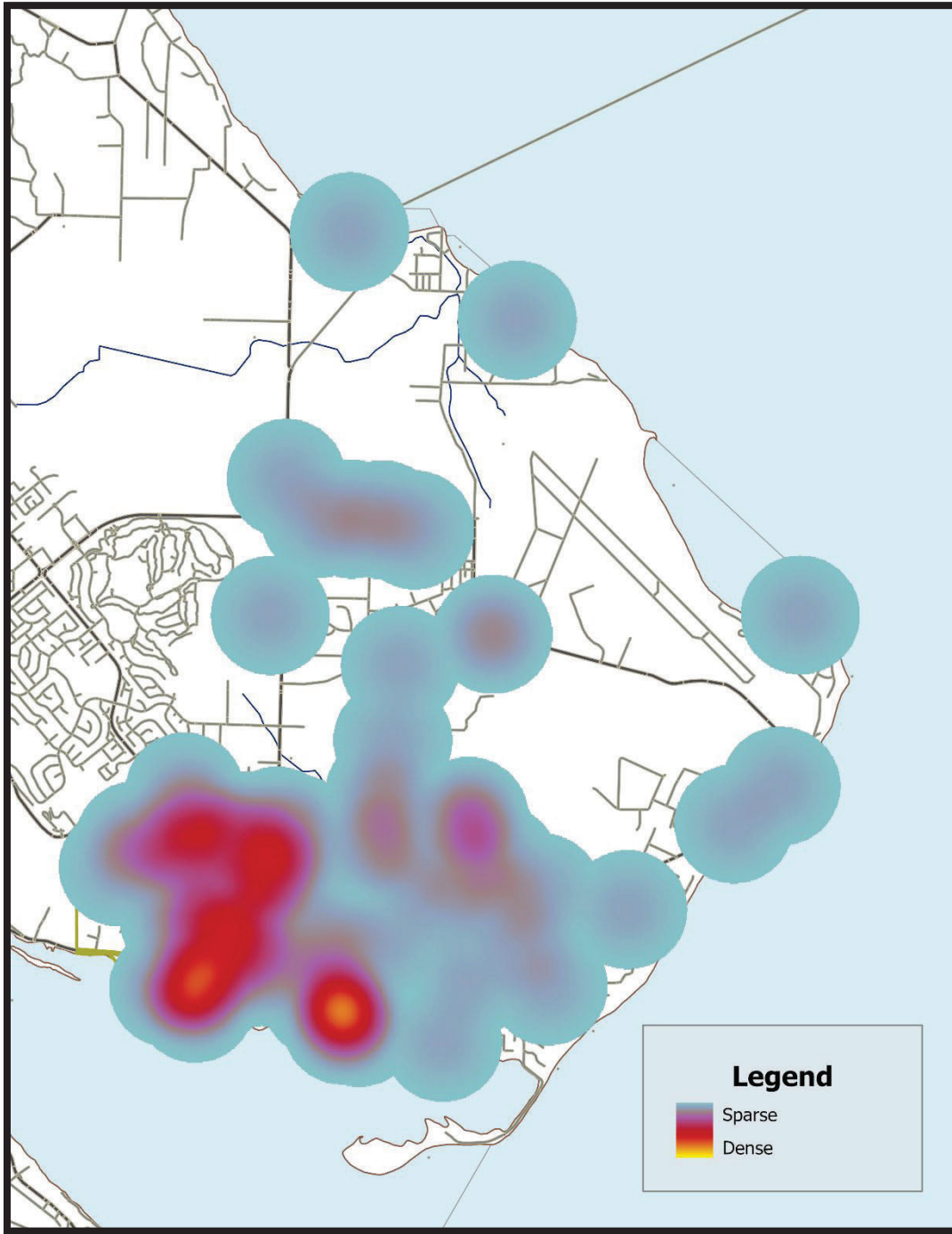
In the first quarter of the 2024-2025 fiscal year, the most common type of Property Crime file in Comox was Mischief to Property. Relative to the first quarter of the previous fiscal year, there was a notable increase in Fraud files.

File Type	First Quarter Data						% Change from 23-24 Q1	Difference from 23-24 Q1
	2019	2020	2021	2022	2023	2024		
MISCHIEF TO PROPERTY	60	27	33	42	32	30	-6%	-2
FRAUDS	15	15	23	21	16	28	75%	12
THEFT FROM VEHICLE	51	12	18	2	14	17	21%	3
OTHER THEFT U/5000	15	26	19	8	14	10	-29%	-4
BIKE THEFT	7	5	2	1	2	6	200%	4
AUTO THEFT	3	4	2	0	4	3	-25%	-1
BREAK & ENTER - BUS	6	4	3	0	2	2	0%	0
ARSON	0	0	0	0	0	2		2
OTHER GO	0	4	0	1	5	1	-80%	-4
SHOPLIFTING	1	2	4	1	2	1	-50%	-1
BREAK & ENTER - RES	6	6	7	3	3	1	-67%	-2
THEFT FROM MAIL	3	0	0	0	0	0		0
OTHER THEFT O/5000	0	1	0	0	1	0	-100%	-1
BREAK & ENTER - OTH	4	5	1	4	3	0	-100%	-3
POSSESS STOLEN PROPERTY	1	0	1	1	1	0	-100%	-1
Grand Total	172	111	113	84	99	101	2%	2

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Map of Property Crime by Area of Comox

In the first quarter of the 2024-2025 fiscal year, there were hotspots of Property Crime files in West Comox and in the area of the Marina.



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CDSA Offences

In the first quarter of the 2024-2025 fiscal year, there were 2 CDSA Offence files in Comox.

File Type	First Quarter Data						% Change from 23-24 Q1	Difference from 23-24 Q1
	2019	2020	2021	2022	2023	2024		
DRUG POSSESSION	2	3	0	1	0	1		1
DRUG TRAFFICKING	2	2	1	0	0	1		1
DRUGS OTHER	2	1	0	0	0	0		0
Grand Total	6	6	1	1	0	2		2

Traffic Offences

In the first quarter of the 2024-2025 fiscal year, there were 7 Traffic Offence files in Comox.

File Type	First Quarter Data						% Change from 23-24 Q1	Difference from 23-24 Q1
	2019	2020	2021	2022	2023	2024		
IMPAIRED OP MOTOR VEHICLE	11	12	9	8	9	12	33%	3
PROHIBITED DRIVING	0	1	0	0	0	0		0
DANGEROUS OP MOTOR VEHICLE	0	0	1	0	0	0		0
Grand Total	11	13	10	8	9	12	33%	3

UNCLASSIFIED



TOWN OF COMOX
Minutes of the Regular Council Meeting,
held in Council Chambers on Wednesday July 10, 2024

Present: Mayor N. Minions
 Councillors S. Blacklock, K. Grant, C. Haslett,
 J. Kerr (electronically), J. Meilleur, M. Swift

Absent: Nil

Staff Present: J. Wall, Chief Administrative Officer
 S. Russwurm, Corporate Officer
 E. Henley, Director of Finance
 G. Schreiner, Fire Chief
 S. Ashfield, Director of Operations

Call to Order:

The meeting was called to order at 5:00 p.m. with 0 members of the public in attendance.

Mayor Minions respectfully acknowledged that the land on which we gather and work is on the unceded traditional territory of the K'ómoks First Nation, the traditional keepers of this land.

1. INTRODUCTION AND APPROVAL OF LATE ITEMS: NIL

2. ADOPTION OF AGENDA:

a. Adoption of Agenda

Adoption of Agenda

THAT the July 10, 2024, Regular Council Meeting agenda be Adopted.

(2024.255) -- CARRIED

3. DELEGATIONS:

a. Tanya Massa, Tourism Development Specialist (Experience Comox Valley): Experience Comox Valley and the Tourism Strategy Plan

Tourism Strategy Plan process

Tanya Massa from Experience Comox Valley provided an overview of their most recent activities, successes, results, as well as an update on the Tourism Strategy Plan process and engagement to date.

b. Kailee Mortimer, Community Consulant (Urban Systems): Town of Comox Climate Action Plan

Climate Action Plan

Kailee Mortimer from Urban Systems provided an overview of the Town's Climate Action Plan which identifies six focus areas, 15 goals, and 34 actions to support climate mitigation and adaptation.

REGULARLY MOVED AND SECONDED THAT THE MEETING RECESS AT 5:54 P.M. UNTIL AFTER THE PUBLIC HEARING ON OCP AMENDMENT & REZONING APPLICATIONS: OCP RZ 23-2 (2077 HECTOR AND 941 ASPEN ROADS) AND OCP RZ 24-3 (2123 HECTOR ROAD)

CARRIED

THE MEETING WAS CALLED BACK TO ORDER AT 8:09 P.M.

4. ADOPTION OF MINUTES:

a. Regular Council Meeting Minutes (June 5, 2024)

Regular Council Meeting Minutes

THAT the Minutes of the Regular Council Meeting, held in Council Chambers on Wednesday, June 5, 2024, be Adopted as Amended to reflect Acting Mayor Swift in the K'ómoks First Nation Acknowledgement.

(2024.256) -- CARRIED

b. Special Council Meeting Minutes (June 12, 2024)

Special Council Meeting Minutes

THAT the Minutes of the Special Council Meeting, held in Council Chambers on Wednesday, June 12, 2024, be Adopted.

(2024.257) -- CARRIED

c. Special Council Meeting Minutes (June 19, 2024)

THAT the Minutes of the Special Council Meeting, held in Council Chambers on Wednesday, June 19, 2024, be Adopted.

(2024.258) -- CARRIED

d. Special Council Meeting Minutes (July 4, 2024)

THAT the Minutes of the Special Council Meeting, held in Council Chambers on Wednesday, July 4, 2024, be Adopted.

(2024.259) -- CARRIED

5. COUNCIL COMMITTEE MINUTES AND REPORTS: NIL

6. CONSENT AGENDA:

a. Consent Agenda

THAT the Consent Agenda items as follows be received for information:

- 1. Barb Tribe - Garbage Bin Injury
- 2. Michael Seib - Reducing the Towns Expenditures and Lowering the Tax Burden
- 3. Gord Schreiner, Fire Chief (Comox Valley Fire Rescue): Thank-You Card for Delivering Neighbourhood Emergency Preparedness Program
- 4. Brian Sims, Associate Director and Executive Director, (Serious and Organized Crime Division, Ministry of Public Safety and Solicitor General): Major Crime Unit Initiative
- 5. Sarah Weber, President & CEO (C3 Alliance): 11th Annual Resource Breakfast Series Invite - Sept 17-19, 2024
- 6. Lesley Giroday, President (Comox Valley Land Trust): Participation in a Future Class Action Lawsuit Against Fossil Fuel Companies

(2024.260) -- CARRIED

a. Consent Agenda

7. *Christine Baxter, Executive Director, CYMC: Request for Letter of Support*

8. *Steve Hill: Council Criteria to Justify Property Tax Exemptions*

9. *Jennifer Groundwater: Three-way Stop Request for Intersection of Church and Beaufort Ave*

10. *Bowinn Ma, Ministry of Emergency Management and Climate Readiness (Government of BC): Response to Mayor Bob Wells Regarding Designation of Search and Rescue*

2. *THAT Item 4. (Brian Sims, Associate Director and Executive Director, (Serious and Organized Crime Division, Ministry of Public Safety and Solicitor General): Major Crime Unit Initiative) be removed from the consent agenda for discussion, and*

THAT Mayor Minions follow up with the regional leadership team for updates, and for staff to check on the opt-out option.

(2024.261) -- CARRIED

3. *THAT Item 9. (Jennifer Groundwater: Three-way Stop Request for Intersection of Church and Beaufort Ave) be removed from the consent agenda for discussion.*

(2024.262) -- CARRIED

7. UNFINISHED BUSINESS: NIL

8. SPECIAL REPORTS: NIL

9. BYLAW ADOPTIONS: NIL

10. NEW BUSINESS:

a. Community Works Fund Agreement

Community Works Fund Agreement

THAT the 2024-2034 Community Works Fund Agreement between the Town of Comox and the Union of BC Municipalities, as included in the July 10, 2024 Regular Council Meeting agenda, be approved; and

THAT the Mayor and the Corporate Officer be authorized to execute the agreement.

(2024.263) -- CARRIED

b. Grant In Aid Program Options

Grant In Aid Program Options

1. *THAT Council Grant in Aid Policy CCL-031 be replaced with the draft policy attached to the July 10, 2024, report from Jordan Wall titled 'Grant In Aid Program Options'.*

AMENDING MOTION:

THAT the main motion be amended to add the text ", as amended to specify annual funding in section 6.01, to limit funding for the Coalition to End Homelessness to capital project spending and to source funding for the Coalition to End Homelessness from the Affordable Housing Reserve Fund."

(2024.264) -- CARRIED

b. Grant In Aid Program Options

Grant In Aid Program Options

2. MOTION AS AMENDED:

THAT Council Grant in Aid Policy CCL-031 be replaced with the draft policy attached to the July 10, 2024, report from Jordan Wall titled 'Grant In Aid Program Options', as amended to specify annual funding in section 6.01, to limit funding for the Coalition to End Homelessness to capital project spending and to source funding for the Coalition to End Homelessness from the Affordable Housing Reserve Fund."

(2024.265) -- CARRIED

3. THAT Policy No. CCL-031, Council Grant-in-Aid Policy, be amended to limit grants in aid to not more than \$151,000 per year.

(2024.266) -- CARRIED

[Opposed: Councillor JMeilleur]

4. THAT funds allocated to Nautical Days, Summerfest, the Downtown Halloween Event, and Christmas Tree Lighting be internalized as costs and not included in the Grant in Aid policy.

(2024.267) -- CARRIED

c. Permissive Tax Exemption Policy Adoption

Permissive Tax Exemption Policy Adoption

1. THAT draft Permissive Tax Exemption Policy CCL-025 as attached to the July 10, 2024 report from Jordan Wall, titled "Permissive Tax Exemption Policy Adoption" be amended to remove provision 6.04(a).

(2024.268) -- CARRIED

2. THAT draft Permissive Tax Exemption Policy CCL-025 as attached to the July 10, 2024 report from Jordan Wall, titled "Permissive Tax Exemption Policy Adoption" be amended to remove provision 6.05.

(2024.269) -- CARRIED

[Opposed: Mayor Minions, Councillors JKerr JMeilleur]

3. THAT draft Permissive Tax Exemption Policy CCL-025, as amended to remove sections 6.04(a) and 6.05, be adopted.

(2024.270) -- DEFEATED

[Opposed: Mayor Minions, Councillors SBlacklock CHaslett KGrant JKerr JMeilleur MSwift]

4. THAT draft Permissive Tax Exemption Draft Policy CCL-025 be forwarded to the next Regular Council Meeting with information on religious institutions included in the report.

(2024.271) -- CARRIED

11. NOTICES OF MOTION: NIL

12. CORRESPONDENCE:

a. Bob Wells, Mayor (City of Courtenay): Strengthening Communities' Grant

Strengthening Communities' Grant

THAT the letter dated June 13, 2024, from Mayor Bob Wells, City of Courtenay, be received.

(2024.272) -- CARRIED

b. Vickey Brown, Mayor (Village of Cumberland): Strengthening Communities’ Grant

Strengthening Communities’ Grant

THAT the letter dated June 27, 2024, from Mayor Vickey Brown, Village of Cumberland, be received.

(2024.273) -- CARRIED

c. Serina Allison, Lead Teacher in Environmental Sustainability & Numeracy (School District 71 Youth Climate Event Committee): Donation Request-SD71 Youth Climate Conference

SD71 Donation Request

THAT the letter dated June 24, 2024, from Serina Allison, SD71, be received, and

THAT the Town of Comox offer in-kind support.

(2024.274) -- CARRIED

d. Amy Morris, Executive Director (Vancouver Humane Society): Request to Support UBCM Black Bear Motion

UBCM Black Bear Motion

THAT the letter dated July 4, 2024, from Amy Morris, Vancouver Humane Society, be received.

(2024.275) -- CARRIED

13. LATE ITEMS: NIL

14. REPORTS FROM MEMBERS OF COUNCIL: NIL

17. RESOLUTION TO GO IN-CAMERA: NIL

18. RISE AND REPORT FROM IN-CAMERA: NIL

Adjournment:

Regularly moved and seconded that the meeting adjourn at 9:24 p.m.

CARRIED

Certified correct pursuant to Section 97(1)(b) of the Community Charter.

MAYOR

CORPORATE OFFICER



REQUEST TO APPEAR AS A DELEGATION

TOWN OF COMOX

1809 Beaufort Avenue Ph: (250) 339-2202 Email: town@comox.ca
Comox BC V9M 1R9 Fx: (250) 339-7110

REQUESTS TO APPEAR BEFORE COUNCIL OR THE STRATEGIC PLANNING COMMITTEE MUST BE SUBMITTED NO LATER THAN WEDNESDAY NOON, THE WEEK PRIOR TO THE MEETING.

RECEIVED

July 25, 2024

TOWN OF COMOX

LOG: 24-301	REFER:	AGENDA: RCM 07-Aug-24
FILE: 3360-20/20	ACTION: MR	

Copies: Council, JW/SR/CD/RP

Name(s) of person(s) speaking: NORMAN LAUBE		Organization you are representing: OWNER/DEVELOPER = ACI COMOX INVESTMENTS LTD ANTAR PROPERTIES	
Primary purpose of Organization: DEVELOP + OWN RENTAL APARTMENTS AND RETAIL		Number of members: 2	
Mailing address of Organization: 40 LAUBENFELS DEVELOPMENT ADVISORS 1653 KERRSTEAD PLACE		Contact Name: NORMAN LAUBE	
City: NORTH VANCOUVER		Phone: 778-9531653	
Postal Code: V7J 3T4	Email: NORMAN@LAUBENFELS.COM		
Subject matter: PROVIDE OVERVIEW OF PROPOSED 6-STORY RENTAL APARTMENT & RETAIL PROJECT AT HIGHLAND VILLAGE - 1966 BUTHRIE ROAD			
Specific request of Council, if any (i.e., letter of support, funding): CONSIDER THE IMPORTANCE AND VALUE TO THE COMMUNITY OF PROVIDING 256 DIVERSE RENTAL APARTMENTS AND 9 RETAIL UNITS AT THIS LOCATION.			
Requested meeting and date: AUGUST 7, 2024		AV equipment required: POWERPOINT PROJECTOR	
Date of application: July 25, 2024	Signature of applicant: 	Print name: NORMAN LAUBE	

Please Note:

1. Regular Council and Strategic Planning Committee Meetings start at 5:00 p.m. Delegations are dealt with at the beginning of each meeting.
2. Maximum presentation time is 10 minutes including questions, unless previously approved by the Chair.
3. Presenters are to address Council or the Strategic Planning Committee, and not the audience.
4. All presentation materials/handouts must be submitted no later than Thursday noon, the week prior to the meeting. If the Friday prior to the meeting is a statutory holiday, then presentation materials must be submitted by Wednesday noon.
5. Please ensure that your cell phone is turned OFF during the meeting.

**Public Hearing Report and Meeting Minutes
held in d’Esterre Seniors Centre, 1801 Beaufort Avenue
on Wednesday July 10, 2024**

<u>Present:</u>	Mayor	N. Minions
	Councillors	S. Blacklock, K. Grant, C. Haslett, J. Kerr (electronic), J. Meilleur, M. Swift
	Absent	Nil
	Staff	J. Wall, Chief Administrative Officer S. Russwurm, Corporate Officer E. Henley, Director of Finance G. Schreiner, Fire Chief S. Ashfield, Director of Operations

The Public Hearing Meeting was called to order at 6:00 p.m. with 69 members of the public in attendance.

Mayor Minions respectfully acknowledged that the land on which we gather and work is on the Unceded traditional territory of the K'ómoks First Nation, the traditional keepers of this land.

PUBLIC HEARING STATEMENT

Mayor N. Minions made the following statement:

This Public Hearing is hereby convened pursuant to Section 464 of the Local Government Act for the purpose of hearing representations those persons who believe that their interest is affected by the proposed:

1. OCP AMENDMENT & REZONING APPLICATIONS:

- **OCP RZ 23-2 (2077 Hector and 941 Aspen Roads)**
- **OCP RZ 24-3 (2123 Hector Road):**
 - a. Comox Official Community Plan Amendment Bylaw 1685.11
 - b. Comox Zoning Amendment Bylaw 1850.46
 - c. Comox Phased Development Agreement Authorization Bylaw 2024: Aspen Hector

The proposed bylaws have received first and second reading but have not passed third reading or been adopted by Council. Notices of this Public Hearing have been published in accordance with the requirements of the Local Government Act. Also, a copy of the proposed bylaws, supporting documentation and any submissions to Council received from the public have been available for inspection at Comox Town Hall as required by the Local Government Act. Copies

are also available for review at the desk of the clerk. Note that this Public Hearing is being conducted virtually and in-person.

To maintain order and to ensure everyone has a reasonable opportunity to be heard, I ask that each person wishing to speak first sign the Speaker's List, located at the desk of the Clerk. For Zoom Meeting Link users, hover over 'Reactions' in the bottom footer of the Zoom screen and click the 'Raise Hand' icon, or press *9 on your phone, and wait to be asked to speak by the Chair. All speakers will be asked to speak by the Chair. Once called by the Chair, please begin your presentation to Council by clearly stating your name and address (virtual presenters first click the 'unmute' button or press *6 on your phone).

Please limit your presentation to five minutes. After all have had an opportunity to be heard, anyone wishing to have further input may once again sign the Speaker's List or "raise their hand".

Council will not debate with each other or members of the public. Council will not answer questions; we are here to hear from you. Technical questions may be directed by the Chair to the staff.

Everyone will be given a reasonable opportunity to be heard at this meeting. No one will be, or should feel, discouraged or prevented from making his or her views heard. Please refrain from any conduct that would intimidate others or discourage them from speaking.

In addition, if you wish to provide a written submission during this Public Hearing, you may do so by leaving a copy at the desk of the Clerk or by sending an email to council@comox.ca. Note that written submissions (including emails to Council) must be received before the end of the Public Hearing.

1. OCP AMENDMENT & REZONING APPLICATIONS:

- **OCP RZ 23-2 (2077 Hector and 941 Aspen Roads)**
- **OCP RZ 24-3 (2123 Hector Road):**

Application Summary

a. Comox Official Community Plan Amendment Bylaw 1685.11

In general terms, the purpose of proposed Official Community Plan Amendment Bylaw No. 1685.11 is to:

- a) Add 941 Aspen Road, 2077 Hector Road and 2123 Hector Road to the list of those properties, with an OCP designation of Residential: Low Rise Apartments, Townhouses and Ground Orientated Infill, that are exempt from the OCP policy restricting building height to a maximum of four-storeys.

b) Designate the western portion of 941 Aspen Road with an OCP designation of Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill, and as being within Development Permit Areas # 3 General Multi-Family and # 11 Wildlife Corridor.

c) Designate the eastern portion of 941 Aspen Road with an OCP designation of Mixed Use: Commercial – Residential, and as being within Development Permit Areas #4 Mixed Use: Commercial – Residential and # 11 Wildlife Corridor.

d) Designate 2077 Hector Road with an OCP designation of Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill, and as being within Development Permit Areas # 3 General Multi-Family, # 7 Riparian Areas and # 11 Wildlife Corridor.

e) Adding to the Parks, Trails and Open Spaces Network, an extension of the Hector Greenway along the northern boundary of 2077 Hector Road f) Adding the extension of Hector Road along the northern boundary of 2077 Hector Road to the Road Network as a Minor Collector.

g) Adding the extension of Hector Road along the northern boundary of 2077 Hector Road to the Bicycle Network as a Proposed Major Bike Route.

b. Comox Zoning Amendment Bylaw 1850.46:

In general terms, the purpose of proposed Comox Zoning Amendment Bylaw 1850.46 is to:

a) Create new zones RM 6.1 (Apartment), RM 6.2 (Apartment and Townhouse) and C 7.1 (Commercial-Residential);

b) Rezone the western portion of 941 Aspen Road from CVRD zoning CR-1 to RM 6.1 (Apartment);

c) Rezone the eastern portion of 941 Aspen Road from CVRD zoning CR-1 to C7.1 (Commercial-Residential); and

d) Rezone 2077 Hector Road from CVRD zoning CR-1 to RM 6.2 (Apartment and Townhouse).

c. Comox Phased Development Agreement Authorization Bylaw 2024: Aspen Hector

In general terms, the purpose of proposed Comox Phased Development Agreement Authorization Bylaw 2024: Aspen Hector is to:

Authorize the Town to enter into a phased development agreement with the owners of 941 Aspen and 2077 Hector Roads to limit the Town's ability to change the proposed zoning of 941 Aspen and 2077 Hector Roads for a term of 10 years in exchange for securing a \$728,000 affordable housing contribution for the first phase of the development, a \$100,000 playground amenity contribution, and commercial space for a childcare facility.

PUBLIC HEARING SUBMISSIONS

Summary of written submissions:

1. Email received July 2, 2024, from Catherine Hannon, expressing concerns regarding the added transportation strain from proposed housing expansions and urged Council to consider the need to preserve natural ecosystems amidst demographic shifts and high-density development proposals.
2. Email received July 1, 2024, from Dr. Elaine Kerr, Courtenay, advising Council to protect green spaces and endangered species during new developments in Comox, prioritizing community conservation efforts over developer interests.
3. Email received July 2, 2024, from Ian Davies, 672 Olympic Drive, expressed concerns about poor cell service in parts of Comox, and requested Council require new apartment towers install equipment to improve cell coverage in the area.
4. Letter received July 2, 2024, from Trevor Dickie, 2123 Hector Road, expressed concern that the proposed developments lack sufficient sanitary sewer capacity, urging the council to address this issue before expanding the Official Community Plan (OCP).
5. Email received July 3, 2024, from Lisa Low, 955 Sylvan Road, expressed concerns about the proposed developments not fitting the rural feel, and the increasing traffic and noise, impacting privacy, and straining already full hospitals and schools.
6. Website submission received July 6, 2024, from Cameron Cowan, 873 Aspen Road, expressed concerns about the proposal to build apartments and townhouses across from their home on Aspen Road and requested that the area remain single-family or that multi-family units be built on a new street off Aspen Road.
7. Email received July 7, 2024, from Lynne and Bob Yaskiw, 2779 Fife Place, expressed concerns about large apartments on Aspen Road, preferring them near city centers, and would like to see clearer plans, smaller buildings, heat pumps, fences, trees, and enough parking.
8. Email received July 6, 2024, from Julie Micksch, 906 Acacia Road, expressed concerns about protecting local wetlands, considering alternative stormwater solutions, preserving more trees, and addressing traffic issues to minimize environmental and community

impacts.

9. Email received July 8, 2024, from Jodi MacLean, 2220 Neptune Way, expressed support for having 25% of units with 2 or more bedrooms and suggested increasing this requirement, backed EV-ready parking, appreciated efforts to protect the local ecosystem, proposed making it easier to walk to bus stops (including allowing electric kick-scooters on buses), and opposed commercial services in residential areas.
10. Email received July 9, 2024, from Nancy McNair, 2330 Suffolk Crescent, expressed opposition to the proposed developments.
11. Email received July 8, 2024, from Hans and Marie Jacobs, 2326 Suffolk Crescent, expressed concerns about the size and height of the developments and their impact on local wildlife, questioned public engagement, and recommended keeping buildings to four stories, protecting the environment, involving the public more, and using affordable housing funds promptly.
12. Email received July 8, 2024, from Maureen Denny, 1515 Balsam Avenue, expressed concerns about how increased population and traffic could strain our roads and water supply amid climate change, stressed the importance of protecting endangered trees and wildlife, and suggested the development should be smaller or modified to keep Comox a pleasant place to live and prevent overcrowding.
13. Email received July 8, 2024, from Marina and (James) Michael Fraser, 2285 Neptune Way, expressed concerns about parking, safety at intersections due to parked vehicles, and providing enough parking for proposed developments, and advised the Council to consider the increase in neighborhood activity and incidents of vandalism.
14. Email received July 9, 2024, from Julie Micksch, provided an Arborist report regarding her July 6, 2024, email, for reference.
15. Email received July 9, 2024, from Maurito Prato, Co-Chair, Comox Valley Food Policy Council, advising the enhancement of riparian and wildlife corridor protection, improvement of stormwater management, reduction of paved areas, preservation and expansion of tree canopy cover, and provision of adequate food-growing spaces in the Aspen-Hector development, all aimed at supporting sustainability and community well-being during ongoing development in Comox.
16. Email received July 9, 2024, from Saskia de Quaasteniet, 3414-2200 Murrelet Drive, expressed opposition to the proposal, citing concerns about its excessive scale for our area and its potential damage to the environment and green spaces.
17. Email received July 9, 2024, from Cheryl Taylor, 481 Holly Place, expressed concerns about green space, height, and size of the proposed development, citing potential ecological impacts, traffic issues, and the strain on local services in Comox.

18. Email received July 9, 2024, from Bev Slater, 889 Acacia Road, expressed opposition to the proposed development citing concerns about overcrowding, traffic congestion, strain on local services, and emphasizes the need to preserve green space for future generations in Comox.
19. Email received July 9, 2024, from Grace and Deane Clarke, 2180 Stadacona Drive, expressed concerns about environmental impacts, access to green space, safety for pedestrians, and provisions for affordable housing and senior care in the proposed development plans.
20. Email received July 9, 2024, from Brian Slater, 889 Acacia Road, expressed concerns about the proposal being too large for the area and the potential strain on local infrastructure, citing worries that it may only benefit shareholders rather than the community.
21. Email received July 9, 2024, from Michael Turnbull, 2072 Hector Road, expressed concerns about how population growth might strain local services, opposed allowing taller apartment buildings beyond current limits, suggested adding a connecting road for emergency access, proposed creating a trail network, and questioned how water management and infrastructure upgrades will be handled.
22. Email received July 9, 2024, from Stacey Gardiner Hartman & Nancy Gardiner Jones, 2200 Murrelet Drive, expressed support for the proposed developments, highlighting secure features like controlled entrances and surveillance, attractive architecture, well-maintained grounds, community respect, and the affordability of the living environment at Aspen View Highstreet.
23. Email received July 9, 2024, from Kirk Waugh and Yvonne van den Brink, 1920 Guthrie Road, expressed opposition due to inadequate infrastructure for sewer, water, and roads, increased traffic hazards, and concerns about the impact on the rural character and environment of the area.
24. Email received July 9, 2024, from Riley Whyte, Aspen View, expressed positive experiences of living at Aspen View for over two years emphasizing Aspen View's welcoming community, modern amenities, and the sense of home and security it has provided.
25. Email received July 10, 2024, from Susanna Scott, 1959 Toronitz Road, expressed opposition due to concerns about inadequate infrastructure to support current and future residents, increased traffic issues, and potential impacts on nearby farming and community safety.
26. Email received July 10, 2024, from Maggie Doucette, 2225 Crown Isle Drive, expressed concerns that the proposed development could harm neighbours and strain the Comox Valley with increased traffic, suggesting smaller-scale housing options or preserving the area as a natural forest.

27. Email received July 9, 2024, from Marie Jacobs, 2326 Suffolk Crescent, expressing opposition due to concerns that mega-developments like Highstreet's are unnecessary for filling Comox's housing needs, worsen traffic issues, and do not adapt to community standards or regional planning considerations.
28. Email received July 9, 2024, from Ray Kolodychuk, Idiens Way, expressed concerns about concerned about worsening traffic issues, including speeding that poses safety risks, especially for those needing to back into driveways, which must be addressed before considering adding more vehicles to the area.
29. Email received July 10, 2024, from Paul Jordan, 2221 Idiens Way, expressed opposition due to concerns about its negative impact on rural residents' quality of life, increased traffic issues, and environmental sustainability, highlighting the adverse effects experienced on Idiens Way after similar developments.
30. Email received July 10, 2024, from Joanne McKechnie, 1611 Noel Avenue, expressed concerns about impacts of the proposed developments, regarding environmental preservation, increased traffic, and the disrespectful process towards rural neighbors, and asked for wider natural buffers and community input in the development process.
31. Email received July 10, 2024, from Diane and Jay Van Oostdam, 1740 Linden Ave, expressed support for necessary development to accommodate more residents while preserving the environment but stressed community and ecological impacts, advising independent assessments by biologists, ecologists, and hydrologists to protect ecosystems like Brooklyn Creek and the critically imperiled red-listed wetland during water shortages and climate concerns.
32. Email received July 10, 2024, from Jenna Matkoski, 328-680 Murrelet Drive, expressed concerns that the proposed development on Hector Road will significantly alter its character, suggesting measures like 15m property buffers, use of temporary water storage ponds to prevent invasive species, and protecting critically imperiled red-listed wetlands, highlighting the need for thorough environmental assessments to safeguard the Brooklyn Creek watershed.
33. Letter received July 9, 2024, from John Neilson, President, Brooklyn Creek Watershed Society, advised that Council defer approval of the Broadstreet development at 2123 Hector Rd pending an independent hydrological review, considering the cumulative impact of concurrent developments on the Brooklyn Creek watershed amidst increasing drought conditions, and requested adherence to federal and provincial regulations to protect aquatic ecosystems and fish habitats.
34. Email received July 10, 2024, from Laurie Cavaggion, 2026 Hector Road, expressed concerns about the proposed developments changing the rural feel and suggest keeping large property buffers, using dry water storage ponds to avoid invasive species, and protecting the sensitive wetlands and Brooklyn Creek watershed by consulting experts.

35. Email received July 10, 2024, from Vedanand Leeladhur, Aspen Highstreet, expressed support for the proposed developments because Aspen Highstreet offers a secure, environmentally friendly atmosphere with well-maintained facilities, social activities, a gym, and free WiFi, all of which contribute to a homely and healthy lifestyle.
36. Email received July 10, 2024, from Neal De Groote, 2110 Hector Road, supported development in the area, but expressed concerns about the proposed density, traffic impact, integration with existing neighborhoods, light pollution, buffers, and the lack of plans for low-income housing, urging for thoughtful consideration of community input to enhance the project's suitability for both current and future residents.
37. Email received July 10, 2024, from Wayne Matkoski, 2044 Hector Road, suggested broader property buffers to protect habitat and privacy, expressed opposition to permanent water ponds due to invasive species risks, and urged expert evaluation of critically imperiled red-listed wetlands and the Brooklyn Creek watershed's ecological impacts in the proposed developments.
38. Email received July 10, 2024, from Karen Jensen, 2136 Stadacona Drive, expressed concerns about proposed developments, questioning the lack of communication from Broadstreet Development, seeking details on tree retention and wetland reports, and advocating for lower-density housing to preserve community character and address infrastructure challenges in the Comox Valley.

Summary of Speakers:

1. Julie Micksch (via Zoom), 906 Acacia Road, expressed concerns about the environmental impact on wetlands, citing the necessity for 30-meter buffers supported by research, the importance of consulting a professional land hydrologist, implementing stormwater ponds to manage runoff, and the preservation of trees and greenways.
2. Paul Jordan, 2221 Idiens way, expressed concerns about large size of the project, traffic impacts and quality of life in rural areas.
3. Barbara Leiman, 2243 Heron Crescent, expressed concerns about the timing of the hearing during summer months and the impact on local services due to the large size of the project.
4. Stephanie Abbat-Slater, 889 Acacia Road, opposes the project due to concerns about the magnitude of the project and the impacts of adding a large number of people to a small area.
5. Brody Abbat-Slater, 889 Acacia Road, supported development but opposed the proposed developments, expressing concerns about the need to consider existing structures, biodiversity, traffic, healthcare, etc.

6. Margaret Waterton, Sheraton Road, expressed concerns about traffic flow around Aspen/Guthrie and surrounding area, as well as transit and traffic impacts in the Comox Valley.
7. Lyn Yaskiw, 2779 Fife Place, expressed concerns about placing large apartments on Aspen Road, adequate parking, advocated for medium-density housing and townhomes, and questioned the timing of the public hearing during the summer when many residents are out of town.
8. Yvette Crane, 108-2250 Manor Place, expressed concerns about the potential loss of green spaces, wildlife habitats, and mature trees, stressing their importance for local wildlife, cooler temperatures, and water conservation, and suggested a sustainable development approach focusing on infill within existing town limits, tree preservation, urban forest planning, citizen consultation, and advisory group formation.
9. David Dyer, 944 Aspen Road, requested walking paths be continued as part of the overall project.
10. Wayne Matkoski, 2044 Hector Road, expressed concerns about the magnitude of the project and the impact of adding a large number of people to a small area, as well as the need for large buffers to maintain the rural atmosphere.
11. Irena Burns, 2266 Idiens Way, expressed concerns about traffic impacts, noting that the Anderton/Idiens intersection is a high-impact area needing more controls such as additional lights or a traffic circle.
12. Trevor Dickie, Broadstreet Properties, 2123 Hector Road, provided details on their proposed development, noting that pathways should not have been included in the Bylaw and recommending its amendment, raised concerns about sanitation capacity for servicing new developments, and plans for a gradual, staged construction process.
13. Gary Hynds, 2217 Heron Crescent, raised concerns about the target demographics and affordability of proposed developments, and questioned the readiness of electricity, road infrastructure, schools, medical services, and job opportunities capable of sustaining an additional 2,000 residents.
14. Kenny Shaw, 807 Anderton Rd, expressed concerns about water runoff issues, noting his annual need to replace gravel in his driveway due to this problem, and raised questions about the management of water runoff.
15. John Neilson, 1500 Balmoral, President of Brooklyn Creek Watershed Society, expressed opposition to the proposed developments and urged a hydrologist's assessment to understand their cumulative effects of stormwater on the watershed, and emphasizing compliance with provincial and federal requirements based on information from the Department of Fisheries and Oceans.

16. Dave Munro, 2221 Neptune Way, expressed concerns about school capacity and the need for more teachers, and the high rental costs making it hard for teachers to move here because affordable housing is difficult to find.
17. Neil Bolton, Highstreet Ventures, 602-1708 Dolphin Road, as the developer and applicant for this project, they recognize the housing shortage and aim to provide daycare and homes to help address a 1% vacancy rate. The project focuses on rental properties or adjusts based on market demands, includes a tree retention strategy, wetland protection, and infrastructure upgrades, targeting residents aged 20-35 up to retirement, and offering economic benefits to the Town.
18. Clyde Woolman, 952 Sylvan Road, questioned whether the proposed development was four stories or six stories and requested clarification.

THE MEETING RECESSED AT 7:26 P.M. AND WAS CALLED BACK TO ORDER AT to 7:28 P.M.

Jordan Wall, CAO, provided a statement to the audience clarifying that the change in the OCP aims to maintain consistency aligning with updated aeronautical zoning regulations that are being modernized from old 1960s regulations. This consistency in the OCP is crucial for accommodating future expectations under new aeronautical regulations and removing previous lower height limits. The developer remains committed to pursuing four-story buildings.

NOTE: The OCP provides a broader framework for development, while specific zoning will further restrict permissible activities.

19. Joanne McKechnie, 1611 Noel Ave, expressed concerns about the 2012 Official Community Plan (OCP) for not adequately addressing natural resources and wildlife corridors, emphasizing the need to delay decisions until completing our urban forest strategy and updating the OCP, and underscoring the importance of protecting our community's natural resources.
20. Jay VanOstda, 1740 Linden Avenue, expressed concerns about the water implications of 1000 new households and 2000 additional people already under level 2 water restrictions, advising a plan for water-saving through more efficient usage, additional water costs, and mandatory household cisterns, aiming for smart growth that minimizes valley-wide impacts.
21. Neal DeGroot, 2110 Hector Road, expressed concerns about density, noting other developments throughout the Comox Valley in various stages of planning or completion; while not opposed, emphasized the need to carefully review the numbers.
22. Lisa Low (via Zoom), 955 Sylvan Road, expressed concerns that, unlike the Quality Foods location in an urban setting, the proposed rural developments will significantly impact current residents' quality of life, and developers, being market-driven, may change their plans to build 4-stories after the bylaw is amended to permit 6-story buildings.

23. Marie Jacob, 2326 Suffolk Crescent, expressed concerns about the traffic and impacts of all developments, noting that affordable housing isn't helping those with limited income, that development decisions affect all of Comox Valley and need to be paced and sustainable, and that the Valley does not have infinite capacity.
24. Lisa Bernard, 6505 Rennie Road, requested clarity about how Bills 44, 46, and 47 affect high-density developments and the role of municipalities, including Comox, in making decisions about density, while also considering our progress towards becoming complete communities and addressing traffic concerns.
25. Neil Bolton (2nd time), Highstreet Ventures, 602-1708 Dolphin Road, clarified that they are only considering building 4-story buildings.
26. Trevor Dickie (2nd time), Broadstreet Properties, 2123 Hector Road, amendment bylaw on map 3 is old version. The correct version was provided with the notice.
27. Garry Hynds (2nd time), 2217 Heron Crescent, requested clarity about what the cost per unit will be.
28. Yvette Crane (2nd time), 2250 Manor Place, expressed support for diverse housing options, including supportive, low-income, nonprofit, cooperative, and cohousing, stressing the need for varied solutions to housing needs.
29. Paul Jordan (2nd time), 2221 Idiens Way, expressed concerns about urban roads entering quiet rural areas, leading to environmental impacts on riparian areas near Idiens Way, where pollution from tires and exhaust has harmed wild frogs, and raised concerns about increased traffic and speeding affecting rural quality of life.

Close of Public Hearing:

There being no further speakers wishing to make representation, Mayor Minions made the following statement:

I remind those present that legal considerations prevent the members of Council from considering any representation made after the close of this Public Hearing item.

Before closing this Public Hearing item, I am going to call three times for any further speakers.

For the first time, is there anyone who wishes to make any further representation? For the second time, is there anyone who wishes to make any further representation? For the third time, is there anyone who wishes to make any further representation?

There being no further speakers, I declare this Public Hearing item closed.

Submissions received at the meeting, are attached to these Minutes:

- Hand delivered: Yvette Crane, 108-2250 Manor Place
- Emailed: Sam Sulek, no address provided
- Emailed: Sarah Waddington, no address provided
- Emailed: David Sulek, no address provided
- Emailed: Clarice Coty, 698 Aspen Road

Adjournment:

Regularly moved and seconded that the meeting adjourn at 7:59 p.m.

CARRIED

Certified correct pursuant to Section 97(1)(b) of the Community Charter.

MAYOR

Certified fair and accurate pursuant to Section 465(6) of the Local Government Act.

CORPORATE OFFICER

RECEIVED

July 10, 2024

LOG: 24-271	REFER:	AGENDA:
FILE: 3360-20-20	ACTION: File	

Yvette Crane
#108-2250 Manor Place
Comox, B.C. V9M 1C5

July 10, 2024 **TOWN OF COMOX** Cfile: 3360-20-2023.02

TO: Town of Comox Mayor and Council
Copies: Council
JW/SR/RB/PN/SA/CP/CD

RE: Public Hearing re Application RZ OCP 23-2 (Bylaws 1685.11, 1850.46, 2024)

Dear Mayor Minions and Comox Town Council:

My name is Yvette Crane and I live at [REDACTED] in Comox.

My husband & I are new to Comox, having moved here two years ago. What originally attracted us to Comox was its small town / rural feel, its abundant green spaces, and the lovely parks -- but also 'wild' spaces and trees everywhere we looked.

Now I am deeply concerned that these precious wild spaces in our town, beloved by human and wildlife, appear to be disappearing under our watch. This includes acres of mature second growth forest and trees as well as small pockets of urban wild spaces. These are so important to wildlife; for cooling temperatures; for water conservation, and in mitigating the effects of climate change.

I also see grassroots efforts to propose another way of looking at our remaining wild spaces, other ways to develop without destroying the character and natural beauty of this town.

Here is my request of you:

- 1) Please halt or at least slow this current massive development, by choosing NOT to approve these by law changes here tonight
- 2) Envision a new way to develop. I personally am in favour of higher density – WITHIN THE CURRENT FOOTPRINT of our Town.
NOT on green field space. Let's use the urban footprint we already have and strive to stay within it.
No more annexing of rural lands.
- 3) Let's look to infill development and increasing density within the current Town limits. I see empty lots and also small & large homes on enormous lots.
Some possible positive examples I am aware of include laneway cottages; "granny flats", duplexes and four-plexes.
- 4) Keep our skyline and wonderful views open to everyone to enjoy. Avoid the feeling and environmental unsustainability of the 'concrete jungle' by keeping the height limits as per the old OCP
- 5) Keep 100% of the trees! Let's get a real tree bylaw in place, where is our Urban Forest Plan and the new OCP we as citizens can contribute to?

My concern is that all this wonderful & important planning is happening much too slowly and too late

I believe that our Town should be a Town for its citizens, not a place for large development companies and their owners to make huge profits today by destroying the natural resources that belong to all of us, thus imperiling all of our futures. (Water issues; drought; air quality; heat domes and so on.)

6) My final request to you is to look at consulting citizens proactively. Can we set up a Citizens Advisory Group that advises the Town on development and other urgent matters? Other Towns & Cities have these. What an important way to truly involve citizens.

I have more suggestions around that, around truly engaging citizens and gathering their input, and around how to help citizens through the Town bureaucratic process when they do want to get involved. Happy to chat further on this.

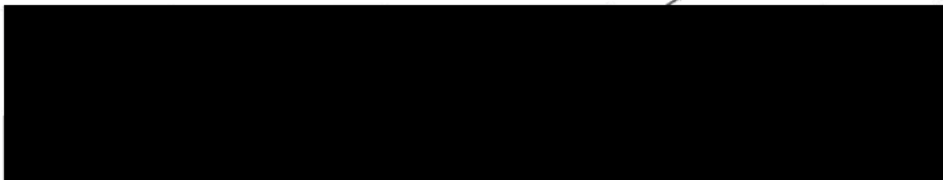
Comox is growing – (hopefully not out of all recognition).

We need to grow too, in the way Council and the Town office engage with the people of Comox, as well.

Thank you for all your work, each and every one of you as Council and staff. It is appreciated. And thank you for your time here tonight.

Sincerely,


Yvette Crane



RECEIVED

July 10, 2024

TOWN OF COMOX

LOG: 24-272	REFER:	AGENDA:
FILE: 3360-20-20	ACTION: File	

Cfile:3360-20-2023.02 RZ 23-2 OCP 2077 Hector and Aspen

Copies: Council
JW/SR/RB/PN/SA/CP/CD

From: sam [REDACTED]
Sent: Wednesday, July 10, 2024 7:31 PM
To: Town of Comox – Administration <town@comox.ca>
Subject: Bylaw 1685.11, Bylaw 1850.46, Bylaw 2024 re 941 and 2077 Hector Rd

To whom it may concern,

I oppose this development.

Sam Sulek

RECEIVED

July 10, 2024

TOWN OF COMOX

LOG: 24-273	REFER:	AGENDA:
FILE: 3360-20-20	ACTION: File	

Cfile:3360-20-2023.02 RZ 23-2 OCP 2077 Hector and Aspen

From: S Waddington [REDACTED]

Copies: Council

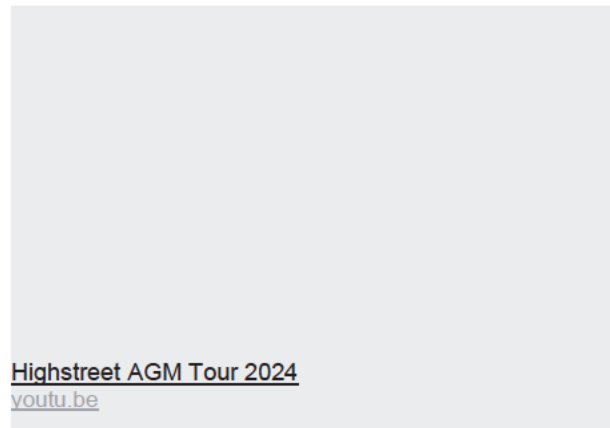
Sent: Wednesday, July 10, 2024 6:41 PM

JW/SR/RB/PN/SA/CP/CD

To: Town of Comox – Administration <town@comox.ca> [REDACTED]

Subject: Highstreet doesn't care about communities

If you watch the just posted Highstreet AGM recap, you get the sense Highstreet is a great company doing great things.



I beg to differ. "Sustainability" is not plowing down existing forests with rare species and increasingly important ecosystems that will help us adapt to climate change, and provide habitat to wildlife, protect water and a myriad of other benefits. The community has said clearly they do not want this development, and when things such as pathways and trails were proposed Highstreet said no. Why in our own communities are we at the mercy of private development when it comes to designing and creating the communities we live in, with our needs in mind? Highstreet will be long gone if this goes through, and the community lives with the aftermath of whatever they decide to build, with their top motive being profits, with little consideration to any created impacts.

In Sidney BC, Highstreet has displaced tenants from affordable housing in order to demolish two buildings consisting of 20 units to build luxury condos - and during a housing crisis! Meanwhile, Sidney council is agonizing over how to address housing. If these units are demolished, the current number of below market units will be halved. Another example of their fine "community building" and "sustainable development".

I hope Comox will realize it would be a horrible decision to let them go ahead with these plans, and to remember other development proposals are always on the horizon. **It's time we listen to what communities want over developers**, as we are the ones who live in these communities. Please do the right thing, protect the land, ecosystems, and the communities values - and consider projects that take the bigger picture into account.

Comox is beautiful and people don't want to lose that.

Sincerely,

Sarah Waddington

RECEIVED

July 10, 2024

TOWN OF COMOX

LOG: 24-274	REFER:	AGENDA:
FILE: 3360-20-20	ACTION: File	

Cfile:3360-20-2023.02 RZ 23-2 OCP 2077 Hector and Aspen

From: david sulek [REDACTED]
 Sent: Wednesday, July 10, 2024 8:57 PM
 To: Town of Comox – Administration <town@comox.ca>
 Subject: Bylaw 1685.11,Bylaw1850.46, Bylaw2024 re941 and Hector Road

Copies: Council
JW/SR/RB/PN/SA/CP/CD

To the Mayor and Council@ Comox

I am highly opposed to such a huge development in our quaint beautiful town and surrounding community. With the increase of 2000 plus new residents invading our peaceful and quiet little town it is going to completely change all of our daily lives. Being a senior citizen who has lived here for over 40 years , I have seen so many changes since being transferred here and decided to retire here in the Comox Valley mainly because of the quality of the laid back life style and the beauty of the entire area. Since arriving here, the population and housing has tripled. The traffic and crowded streets is becoming more of a daily challenge. Everyone used to say Let Courtney expand and leave the Comox area small and relaxed. We now see more and more apartments being built and lots that were single residents now have 4 to 6 houses on that lot. Contractors are laughing all the way to the bank but the rest of the citizens are facing the problems associated with the increased traffic and crowded Stores and Malls.

Why isn't such a life changing decision being put out to referendum and let the citizens of the community decide. This is to big of decision to be made by the Mayor and a few council members.

David Sulek
 Daye Rd.
 Comox.

RECEIVED

July 10, 2024

TOWN OF COMOX

LOG: 24-275	REFER:	AGENDA:
FILE: 3360-20-20	ACTION: File	

Cfile:3360-20-2023.02 RZ 23-2 OCP 2077 Hector and Aspen

From: Clarice Coty [REDACTED]
Sent: Wednesday, July 10, 2024 4:59 PM
To: council <council@comox.ca>
Subject: Delivery Status Notification (Failure)

Copies: Council
 JW/SR/RB/PN/SA/CP/CD

Dear Comox Council

I spoke at the Public Hearing for Aspen View four years ago, describing my experience as a landlord when I advertised my Comox property for rent. I had multiple single parents, couples and families apply to live in this two bedroom apartment.

At the time, it was very emotional for me to choose a tenant, and in the end, I chose the mom who had a three year old child - and this mom was 8 months pregnant with her second child. No one was willing to rent to her.

Having a granddaughter the same age as her daughter, I was moved by her situation. I wanted to give her a chance. I did, and she and her daughters are still living there today. They are the best tenants.

I write to council members to consider all of the other families out there with young children who require a two or three bedroom condo or townhouse to live in. Rental properties, especially for families, are desperately needed.

I also hope that Highstreet will allow all of their rentals to be pet friendly, as this can be most helpful to children, adults and to seniors.

I am in favour of:

- Comox Official Community Plan Amendment Bylaw 1685.11
- Comox Phased Development Agreement Authorization Bylaw 2024: Aspen Hector
- Comox Zoning Amendment Bylaw 1850.46:

Sincerely
 Clarice Coty

[REDACTED]

RECEIVED

JUL 08 2024

Laura Gilbert and William Hall

Comox, BC

TOWN OF COMOX

Mayor Minions and Comox Town Council
1809 Beaufort Avenue
Comox, BC
V9M 1R9

Table with 3 columns: LOG, REFER, AGENDA, FILE, ACTION. Values include 24-234, 0220-20-20, MR, RCM 07-Aug-24.

Cfile: 0220-20-2024/5400-12

July 8, 2024

Copies: Council
JW/SA/CP/SR/CD

Dear Madam Mayor and Members of the Comox Town Council,

We would like to request assessment for traffic calming on Noel Avenue between the intersections of Noel and Torrence Road and Noel and Guthrie Road.

We live along this stretch of road and notice a significant volume of traffic during morning and afternoon commute times, in addition to clusters of cars traveling late at night. Many of these vehicles are traveling past at speeds in excess of what is comfortable for walking along the sidewalk, or for doing yard maintenance in our front yard. In addition, vehicles traveling at higher speeds create more noise which is frequently bothersome late at night.

There are no designated bike lanes along this stretch of road and cyclists often use the sidewalk rather than the roadway, which also affects pedestrian comfort when using the sidewalk.

There are multiple schools in the near vicinity: Phil and Jennie Gagliardi Academy, Ecole Robb Elementary school, and Highland Secondary. Many students travel this route to school by foot, or on bikes or scooters. The safety of these children and adolescents is affected by the road layout and traffic, and understandably, students often cycle on the sidewalk rather than on the road.

Noel Avenue is designated as a major collector route in the Traffic Calming Policy Manual, and we would expect a certain volume of traffic. We suspect, however, that a significant proportion of drivers are exceeding the posted speed limit. There are two stretches of reduced speed limit along Noel Avenue (in front of the Phil and Jennie Gagliardi Academy, and in front of the Comox Recreation Centre), and further interventions to moderate traffic along the section outlined here would make our neighborhood safer and more pleasant to live in.

Thank you in advance for reviewing this request.

Sincerely,

Laura Gilbert and William Hall

RECEIVED

July 9, 2024

TOWN OF COMOX

LOG: 24-241	REFER:	AGENDA: RCM 07-Aug-24
FILE: 5480-02	ACTION: MR	

Copies: Council
JW/SA/CP/TH/SR/CD

From: Marina [REDACTED]
Sent: Tuesday, July 9, 2024 9:58 AM
To: Town of Comox – Administration <town@comox.ca>
Subject: Parking Issues - RE: Application: RZ OCP 23-2. Bylaws 1685.1; 1850.46; 2024

Mayor and Council
Town of Comox,

I am sending you these pictures of this vehicle that is parked directly across the street from our driveway. The owner of this vehicle works at the hair salon at Urban Center. I have tried to have a conversation with him to park somewhere else as it's residential parking. He quotes the Bylaw that allows him to park here. I've asked him to be neighbourly and not park in residential. There's other places he can park where there's no houses. He says he feels like "he's being bullied". He refuses to have a conversation with me. I feel he is parking here to "annoy" the residents of Neptune Way. When Benco was proposing to build Urban Corner, he indicated there would be sufficient parking for businesses. This person parks his vehicle along our street from 9 am to sometimes as late as 7 pm.

With the development proposed at Neptune Way and Aspen that includes business, I can foresee that Neptune Way will become a parking lot if sufficient parking is not provided on site.

I am not happy about this person continually parking here whilst he goes off to work at Urban Corner. He's not the only person from Urban Corner that chooses to park on Neptune Way.

Respectfully submitted,

Marina and (James) Michael Fraser

[REDACTED]

RECEIVED
JUL 12 2024
TOWN OF COMOX

[REDACTED]
To whom it may concern in the city of Comox.

I am getting extremely frustrated about the lack of things that I can do about cats wandering through my yard at all hours of the day. I called the SPCA and they say they can do nothing. I have contacted the animal control officer and he says there is nothing he can do. I can't even live trap them in my yard and take them to the SPCA.

There are five cats in my area, Moralee Drive, that are allowed to wander at all hours of the day and night. These cats are attracted to the birds in my yard and in the hedge that I am required by the city to maintain. Also, they are doing their business in my gardens treating them like litter boxes. I have tried to talk to people in the area but they just do not care and say there is no Bylaw to prevent the cats from wandering. To try to deter the cats I have sprayed a mixture of rosemary, garlic, lemon and vinegar around the yard. I have put wire fencing around the wooden fence top. These deterrents do not seem to work as they just get used to them and I still have the problems. I have to put out a fair bit of money for these deterrents and I still get no satisfaction. Why should I have to pay more money to take care of their pets. Because the cats are not bothered by the actions that I have tried I now have evidence that the cats are about at all hours. I have a game cam in my yard that has videos and pictures of the wandering cats.

I do not have anything against cats or the people that own them. Actually I like cats but I wish people would take responsibility for their animals. Since these people do not wish to take responsibility, I believe that the city council must approve a bylaw to prevent people from allowing their pets to wander at all hours of the day. If the cats were prevented from wandering from nine at night to seven in the morning, I believe that this would solve a majority of these problems. There are a lot of people in the city of Comox that I have talked to that are also disgusted with this problem. For your consideration!

Tom Stauffer

[REDACTED]
Comox [REDACTED]

LOG: 24-278	REFER:	AGENDA: RCM 07-Aug-24
FILE: 0220-20-2C	ACTION: MR	

Cfile: 0220-20-2024

Copies: Council
JW/SR/CD

RECEIVED

July 15, 2024

TOWN OF COMOX

LOG: 24-279	REFER:	AGENDA: RCM 07-Aug-24
FILE: 5350-01	ACTION: MR	

Copies: Council
JW/SR/CD

From: J Sparks [REDACTED]

Sent: Friday, July 12, 2024 2:06 PM

To: Town of Comox - Public Works <publicworks@comox.ca>; Chris Haslett <chaslett@comox.ca>; Jonathan Kerr <jkerr@comox.ca>; Jenn Meilleur <jmeilleur@comox.ca>; Ken Grant <kgrant@comox.ca>; Maureen Swift <mswift@comox.ca>; Nicole Minions <NMinions@comox.ca>; Steve Blacklock <sblacklock@comox.ca>

Subject: Re: Town of Comox solid waste cart changes

To the Town of Comox, elected council and Mayor Minions,

I am taking time from my day to express disagreement to the fee imposed to make changes to the waste carts.

In light of the bin designation for each household we find this fee unreasonable and unwarranted. If we, as homeowners, had opted for the size of bins that were delivered, we would understand the fee. However, as you are no doubt aware, the size of bins were assigned to us and are the same size as neighbours with bigger households. As homeowners we were not consulted as to the appropriate size for our household. We are a two person household that intentionally manages our waste so as to keep it minimal. Our bins are taken to the curb about once/month if not less frequently. Even then, the bins are far less than full. This has been our practice for many years prior to the new bins. We have been provided with the wrong size bins and should not have to pay a fee to rectify it.

We would like to change the blue and the green bin to the next size down without the \$35/bin fee to do so. If, in the future, we require additional changes, we fully accept and understand that there will be fees.

We look forward to your response and a resolution to our concern.

Sincerely,

Joyce and Jim Sparks, Comox constituents

[REDACTED]
Comox, BC [REDACTED]
[REDACTED]

On Thu, May 30, 2024 at 9:27 AM Town of Comox - Public Works <publicworks@comox.ca> wrote:

Hello,

We heard from you a while back regarding the new automated curbside cart collection service. At that time, you expressed an interest in changing your cart for a larger/smaller size or even adding an additional cart to the basic 3-cart set.

We are writing now to let you know that a new **Cart Changes** webpage has been created with some information about this as well as some tips and resources for managing waste – please click the link below or type [comox.ca/cartchanges](https://www.comox.ca/cartchanges) into any website browser.

<https://www.comox.ca/cartchanges>.

Once it is ready, the **Cart Change Application Form** will be available on that webpage (sometime prior to July 2024) however, submitted applications will not be reviewed until July onward.

Take care,



Kiley Graham

Clerk II, Public Works Department, Town of Comox

[1390 Guthrie Road, Comox, BC V9N 0A5](https://www.comox.ca)

P 250-339-5410

E publicworks@comox.ca

[Comox.ca](https://www.comox.ca) | [Facebook](#) | [Twitter](#) | [LinkedIn](#) | [YouTube](#)

I respectfully acknowledge that the land on which we gather and work is on the unceded traditional territory of the K'ómoks First Nation, the traditional keepers of this land.

RECEIVED

July 15, 2024

TOWN OF COMOX

From: craig freeman [REDACTED]
Sent: Monday, July 15, 2024 2:16 PM
To: council <council@comox.ca>
Subject: Heritage Buildings

LOG: 24-280	REFER:	AGENDA: 07-Aug-24
FILE: 6130-20- M	ACTION: MR	

Cfile: 6130-20- MLP Mack Laing Park, 6800-01,
2430-20-2015-01 Mack Laing Trust

Copies: Council
JW/SA/RN/MS/SR/CD

July 1, 2024

Mayor and Council

Comox, BC

Dear Mayor and Council,

It has come to our attention, out here at the Merville Community Association (MCA), that the Mack Liang property, Shakesides, is slated to be demolished as the town of Comox does not wish to preserve it as a heritage building. If it is possible to move the structure, rather than destroy it, we feel that the house would be a welcome addition to the heritage buildings that we already have on site here in Merville. It might even be a less costly procedure than eradicating the house.

If this suggestion regarding the former residence of Mack Liang is appealing to you, please feel free to contact us to discuss the idea.

Sincerely,

Craig Freeman

MCA

RECEIVED

July 16, 2024

TOWN OF COMOX

LOG: 24-283	REFER:	AGENDA: RCM 07-Aug-24
FILE: 0220-20	ACTION: MR	

From: Sue Campbell [REDACTED]
Sent: Tuesday, July 16, 2024 4:23 PM
To: council <council@comox.ca>
Subject: Request for bylaw amendment

Copies: Council
 JW/SR/CD

Dear Mayor and Council

We are tenants of apartment building in the Beaufort, Nordin area of Comox and believe we have the right to quiet enjoyment of our properties.

However, on Saturday July 13, 2024 an outdoor function at Martine's Bistro, with loud karaoke music and singing and cheering, went on until midnight. Then several dozen guests congregated on the front lawn until 1:00 am waiting for transportation, talking loudly, laughing and swearing - a lot of swearing. This meant many of us did not get to sleep until 2 or 3 in the morning.

When we phoned the town of Comox to seek guidance, we were told the present bylaws do not support resident claims to quiet enjoyment.

We, therefore, request that the Mayor and Council amend the present bylaws to prohibit electronic noise within the Town of Comox after 10 pm and prohibit loud vocal noise after 10 pm until 7 am.

Thank you for considering our request.

Attached are 18 signatures

July 15, 2024

Mayor and Council

Town Hall, 1809 Beaufort Ave

Comox B.C. V9M 1R9

Dear Mayor and Council

We are tenants of apartment buildings in the Beaufort, Nordin area of Comox and believe we have the right to quiet enjoyment of our properties. However, on Saturday July 13, 2024 an outdoor function at Martine's Bistro, with loud karaoke music and singing and cheering, went on until midnight. Then several dozen guests congregated on the front lawn until 1:00 am waiting for transportation, talking loudly, laughing and swearing - a lot of swearing. This meant that many of us did not get to sleep until 2 or 3 in the morning.

When we phoned the town of Comox to seek guidance, we were told the present bylaws do not support resident claims to quiet enjoyment.

We, therefore, request that the Mayor and Council amend the present bylaws to prohibit electronic noise within the Town of Comox after 10 pm AND prohibit loud vocal noise after 10 pm until 7 am.

Thank you for considering our request.

Yours sincerely

Name

Address

Pamela Calatt

Sue Campbell

Rolando P. Espinoza

Helen Rabski

Karen Bauer

Comox

Comox bc

Comox

Comox

Comox

Request for Bylaw
Amendment 1



TOWN OF COMOX

July 12, 2024

File No. 0540-06

Mayor & Council
Town of Comox
1809 Beaufort Avenue
Comox, BC

LOG: 24-277	REFER:	AGENDA: RCM
FILE: 0400-60	ACTION: MR	07-Aug-24

Copies: Council
JW/SR/CD

Sent via email only:
council@comox.ca

Dear Mayor and Council:

RE: Strengthening Communities Grant

Thank you for your support of the Strengthening Communities Grant program.

We are attaching the latest update about the program illuminating a snapshot of activity between January and March 2024. As you will read we have realized many positive outcomes with this funding to support people experiencing homelessness in the Comox Valley. Most of our partners indicate they will continue to provide services after the funds are exhausted. We are working with others to explore additional resources. We look forward to providing a fulsome final report in the fall.

Yours truly,

Mayor Bob Wells
City of Courtenay



Briefing Note

To: Council

File No.: 1845-20-SCS

From: Director of Recreation, Culture, and Community Services

Date: June 12, 2024

Subject: **Strengthening Communities' Grant Update**

PURPOSE:

To update Council and local partner governments on the progress made and key outcomes resulting from the Strengthening Communities' Grant. This report covers the period from January 1 – March 31, 2024.

BACKGROUND:

In 2021 the Comox Valley Regional District (CVRD) was awarded a regional grant of \$1.093 million through the Strengthening Communities' Grant Program to better support people experiencing homelessness in the Comox Valley. The City is administering those funds on the region's behalf. The project funding was set to conclude on June 30, 2023.

At the May 31, 2023 Council meeting staff informed Council that a request had been made with local government partners for additional funding to support a one-year extension of the Strengthening Communities' Grant. The funding request was to cover activities previously approved under the SCG that could not continue without another source of identified funding. This included staff and security at Connect for daytime services, peer outreach, community engagement, training, and rural outreach.

At the September 13, 2023 Council meeting staff informed Council that the extension had been approved and that the City would receive \$898,603 to fund approved services through June 30, 2024. This brought the total funding approved to \$1,961.763.

At the March 13, 2024 Council meeting, staff informed Council that funding had been received and that staff had amended service agreements with principal partner agencies to reflect the new term and funding services through to June 30, 2024 to support the initiation and/or continued implementation of grant funding activities.

DISCUSSION:

The Strengthening Communities' Grant (SCG) supports the following activities and it is important to note that funding is only eligible for the following approved grant activities:

- **A Daytime Community Access Hub** that operates six days a week providing services focused on health, hygiene, personal safety, and administrative support to those in need. Funding supports security services and the creation/facilitation of a Community Advisory Committee.
- **Rural outreach** on Denman and Hornby Islands to facilitate connections to housing and support services.

- **A peer-based outreach team** to provide outreach and conduct cleaning and remediation activities around locations where support services are provided.
- **Community engagement and anti-stigma** outreach activities, including expanded community meal events hosted at Sunday Service for residents, local businesses and people experiencing homelessness.
- **Training and capacity building** activities across municipalities for local government and First Nation employees, politicians, outreach workers and volunteers focusing on trauma-informed

Update since last report

In this period the City continued to support and to administer SCG funds to three principal partners – Comox Valley Transition Society (CVTS), Sunday Station (SS) and Hornby and Denman Community Health Care Society (HDCHCS). These agencies continued to provide a number of essential services to people who were experiencing homelessness, mental health issues and/or addictions – from referrals to physicians and other health and social service providers, to food, showers, outdoor gear and shelter. Remarkably, 84,616 interactions with workers in these service agencies, and/or the peer workers supported by these agencies, have been recorded – demonstrating the incredible impact these services are having and the significant need that exists in our community for those who are experiencing homelessness or the negative impacts of substance use or mental illness.

Milestones

- CVTS was awarded additional funds to hire more people with lived experience to participate in street clean-up crews. This enabled street clean-ups morning, afternoon, and evenings, seven days per week.
 - This funding was made available due to a change in service provider capacity over the course of the grant period.
- The Community Advisory Committee for Connect continued to meet regularly with representation from the RCMP, City (Bylaw Services and Community Services), CVTS, and VIHA.
 - The focus has been on the timely sharing of information, and the coordination of services to improve the health and well-being of people who depend on the services offered at Connect, as well as the surrounding community.
- The City's Recreation, Culture and Community Services division coordinated a new spring/summer series of free workshop and training opportunities for front line workers who are supporting or interacting with people experiencing homelessness, addiction or mental health challenges.
 - The current line-up of courses includes: *Responding with Respect on the Frontline, Introduction to Trauma Informed Practice, Let's Get REAL about Mental Health: Exploring Empathy, Mental Health First Aid Standard, Gender Diversity in the Workplace, Not Just Naloxone, The Power of Coercive Control, and Prevention First: Introduction to Crisis.*

- Through the SCG grant, organizations are also able to register for an annual membership with the Homelessness Services Association of BC, where they can access additional free training and resources.

While not directly related to the SCG, it is relevant to note that since the last briefing note BC Housing announced that land has been purchased and plans are underway to build a new supportive housing development, as well as a purpose-built shelter in Courtenay. These new services will make a significant impact in meeting the needs of those experiencing homelessness in our community, which is critical as the SCG funding comes to an end June 20, 2024. The statistics and reporting from through the SCG program undoubtedly contributed to the successful advocacy efforts that resulted in this housing investment in Courtenay.

Metrics (January 1, 2024 to March 31, 2024)

Qualitative Feedback

Impact Statement – Connect

- *‘I’m motivated to continue to do this work because I like working with people and believe that we all have a responsibility to support the most vulnerable individuals in our society. I believe that Connect is an essential part of this community, and were it not for the tireless efforts of our team I have no doubt at all that many, many, people would have died from overdoses or other adverse health effects resulting from homelessness. Though options for people to exit homelessness into affordable housing are very limited we do the best that we can to support the people who depend on Connect’.* Connect Staff.

Impact Statement – Recreation, Culture and Community Services, City of Courtenay

- *“Trauma informed training programs are really popular. The {Introduction to Trauma informed Practice} workshop is about trying to get people to gain an understanding about how trauma that people have faced in the past shapes how they behave as an adult. Trauma training can help reduce stigma. It can help people to understand it from a different lens, to take away stigma, whether it is fear or judgement or anything like that.”* - RCCS Staff.

Impact Statements – Hornby and Denman Community Health Care Society

- *“Having a weekly, consistent presence at outreach spaces has created a friendly, familiar, approachable face for our service in the community. Being present and supporting people in these meaningful spaces opens the door to be approached by community members afterwards to initiate more in-depth and private supportive conversations about mental health and substance use.”* - HDCHCS Staff.
- *“Prior to our providing outreach support at the Hornby Kitchen and Denman Island Foodbank, this work was being done informally by the staff and volunteers at these spaces, who were simultaneously trying to provide their own services. These workers have seen the*

need for professional outreach services and are grateful for our presence to provide practical and mental health support to service users.” – HDCHCS Staff.

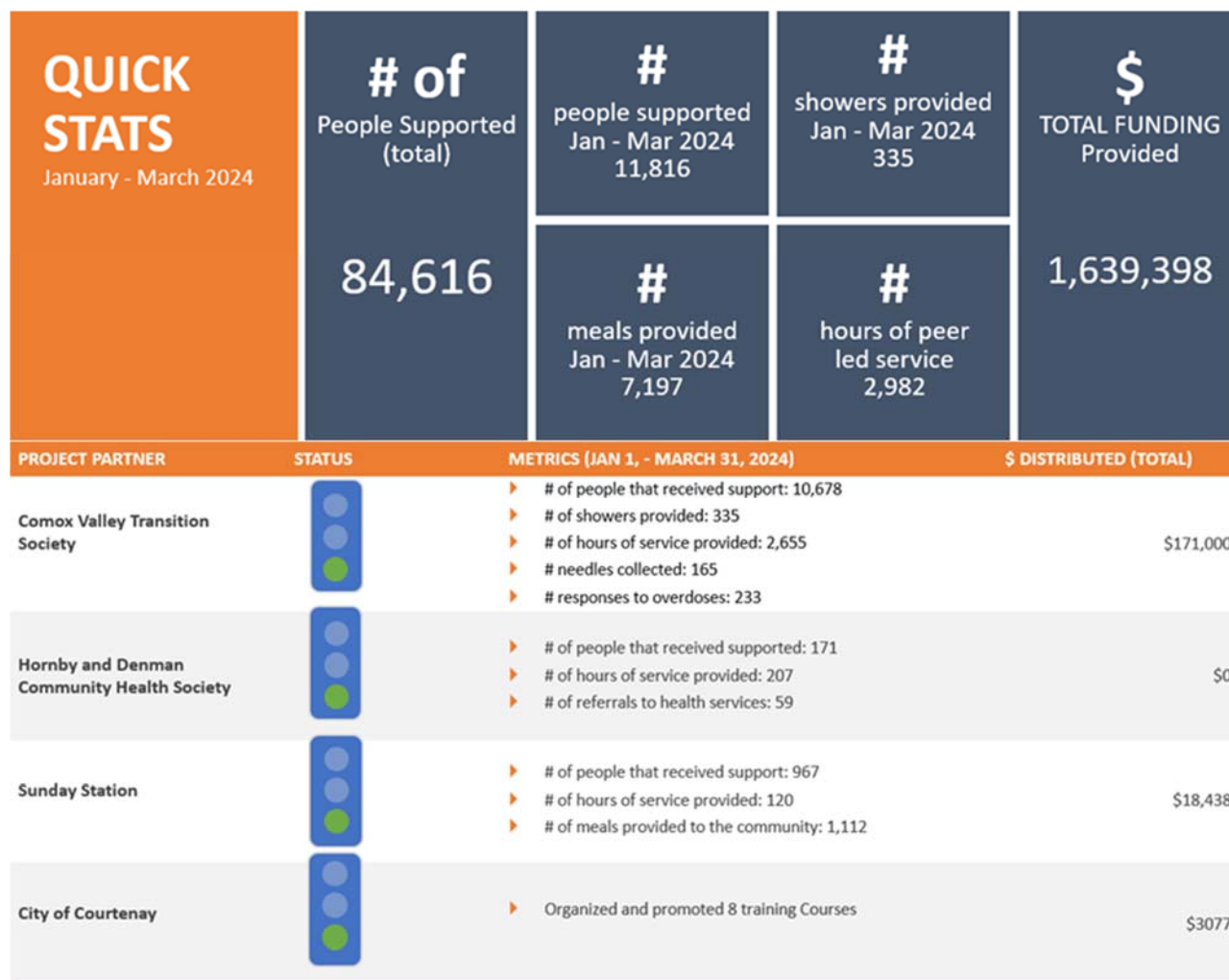
- *“My position is that of a constant liaison between people and services. As a community-based service, I can cast a wide net to reach community members who may not access help through more conventional means. These participants can then begin to access the necessary services, resources, and care providers. I also act as an integral link in connecting one resource provider to another, allowing more wrap-around care for marginalized participants.” - HDCHCS Staff.*
- *“I just need to talk to someone who isn’t in my immediate circle of drama.” - Hornby Island Participant.*
- *“I’m starting to feel joy again for the first time in a long time!”- Denman Island Participant.*

Impact Statements -Sunday Station

- Photos of fresh food donation and hot meals prepared by volunteers.
- *“The effort and grace that Sunday Station volunteers put into each Sunday really shows. Our community relies heavily on this team to provide a nutritious meal to those in need.” - Community Volunteer.*
- *“Thank you for everything you do. Without this program, some of us wouldn’t eat.”- Participant.*
- *“Each Sunday a lot of us look forward to joining together at the train station. It provides a social activity for most of us.” – Participant.*
- *“I watched a short documentary about Sunday Station and it was so well presented to inform the community about Sunday Station. And an awareness about the people who are less fortunate and need community support.” Community Volunteer.*



Quantitative Measures



Sustainability Planning

With funding through the Strengthening Communities Grant set to expire June 30, 2024, staff have been in discussion with partners regarding the impacts of the termination of funding as well as plans for sustaining services following the end of the SCG program.

All principal partners state that they intend to continue to provide the services beyond the June 30, 2024 when SCG funding ends. Each has indicated that they have throughout the project, been able to utilize SCG statistics and funds to leverage additional funds which will allow for some level of service sustainability. That said, each service organization has also stated that the service levels will likely drop off to some extent after the June 30, 2024 date.

The services provided at Connect are likely to be the most significantly impacted by the cessation of funding through SCG. While the SCG funding was a key driver in CVTS securing BC Housing temporary shelter funding, BC Housing funding is limited to the operating costs related to a 24-

hour, 33 bed shelter, and does not contribute to the extensive day services provided to the broader community. Staff and CVTS are in discussions about the potential impacts and strategies for mitigation, however it is expected that the following will be significantly reduced or potentially eliminated completely:

- Peer-led street clean up: CVTS has indicated they may have the capacity to maintain a morning peer led clean-up crew, however they will not have funding to continue with the provision of afternoon and evening street clean up. The City is working with CVTS to explore the extent to which community partners, such as the DCBIA might resource this activity. CVTS is also exploring grant opportunities.
- Food Provision: CVTS currently provides meals to approximately 55 people per day. They will be increasingly dependant upon donations to top up food service for guests above and beyond the 33 shelter guests whose breakfast and dinner meals are covered by BC Housing funding.
- Security: CVTS currently has two security persons for 24 hours everyday of the week. CVTS are in discussion with BC Housing to explore the extent to which this level of service can be maintained. Without additional funding, this service will likely be reduced.

As staff and partners prepare for the end of the Strengthening Communities Grant program, they are planning to come together to discuss project impact, lessons learned, communicating the results, and sustaining momentum where possible.

NEXT STEPS

This SCG update for the period of January 1, 2024 to March 31, 2024 will be sent to the Comox Valley Regional District (CVRD), Town of Comox and the Village of Cumberland as partner municipalities. RCCS staff will continue to support the administration of the SCG on the region's behalf.

The Strengthening Communities' Grant will sunset on June 30, 2024, after which an update to Council and local government partners, including the final report, will be shared. For monthly progress updates, information on our social service partners, contact information and the SCG training and capacity building initiatives please visit: [Strengthening Communities Grant | City of Courtenay](#)

RECOMMENDATION: THAT Council receive the Strengthening Communities' Grant Update briefing note.

Prepared by: Joanne Bays, Community Development Coordinator

Reviewed by: Susie Saunders, Director of Recreation, Culture, and Community Services

Concurrence: Geoff Garbutt, M.PI., MCIP, RPP, City Manager (CAO)

RECEIVED

July 23, 2024

TOWN OF COMOX

LOG: 24-295	REFER:	AGENDA: RCM 07-Aug-24
FILE: 0400-80	ACTION: MR	

From: NIC President <president@nic.bc.ca> Copies: Council
Sent: Tuesday, July 23, 2024 1:36 PM JW/SR/CD
To: Town of Comox – Administration <town@comox.ca>
Subject: NIC's Strategic Plan Year 3 Dashboard Report

Dear Mayor and Council,

I am very pleased to share [BUILD 2026 Strategic Plan and Year 3 Dashboard](#), which provides updates on the itemized, measurable goals of our strategic plan. [BUILD 2026](#) was developed through meaningful consultation with the Nations and communities we have the privilege to serve. Your ongoing input continues to inform this responsive document, as it navigates us toward our mission of working together to build healthy and thriving communities, one student at a time.

BUILD 2026 serves in tandem with NIC’s overarching Indigenization plan, *Working Together*. These plans are further supported by *Journeying Together* (connecting international students and faculty with Indigenous perspectives), *Widening Our Doorways* (ensuring all learning at NIC is relevant and accessible), *Thriving Together* (NIC’s first people plan) and *The CARE² Plan* (supporting the mental health and wellbeing of all members of the college community). The six plans and their dashboard reports (as published) are shared on [Engage NIC](#), where we encourage your feedback.

If you have thoughts about how North Island College can better support the communities in our region through post-secondary education, please contact Emily Haagerup (Emily.Haagerup@nic.bc.ca or 250-334-5270) who will be pleased to coordinate a connection for us.

Thank you for your continued support and guidance.

Sincerely,



Lisa Domae, PhD, RPP
 President and CEO
 North Island College
 2300 Ryan Road, Courtenay, BC V9N 8N6
 250.334.5270
lisa.domae@nic.bc.ca

North Island College is honoured to acknowledge the traditional territories of the combined 35 First Nations of the Nuu-chah-nulth, Kwakwaka’wakw and Coast Salish traditions, on whose traditional and unceded territories the College’s campuses are situated.

RECEIVED

July 17, 2024

TOWN OF COMOX

LOG: 24-298	REFER:	AGENDA: RCM 07-Aug-24
FILE: 0400-90, 4:	ACTION: MR	

Cfile: 0400-90, 4320-01

Copies: Council
JW/EH/SR/CD

From: Tiffany Trownson <tiffany@bccare.ca>

Sent: Wednesday, July 17, 2024 5:13 PM

To: Nicole Minions <NMinions@comox.ca>; Steve Blacklock <sblacklock@comox.ca>; Ken Grant <kgrant@comox.ca>; Chris Haslett <chaslett@comox.ca>; Jonathan Kerr <jkerr@comox.ca>; Jenn Meilleur <jmeilleur@comox.ca>; Maureen Swift <mswift@comox.ca>

Subject: Home Health - Intermunicipal Business Licence

Dear Mayor Minions and Council,

As one of the 12 municipalities included in the **Central Vancouver Island Intermunicipal Mobile Business Licence partnership**, I am reaching out on behalf of our home health members and the seniors they serve across the Tri-Cities.

Last count, there are 300 private home health organizations across BC. Currently, home health operators are not included in the eligibility for Intermunicipal Mobile Business Licences (IMBLs), which requires them to hold a separate business licence in all communities they serve. By nature of their work, home health care service providers are mobile, and therefore should be considered eligible for IMBLs.

Below is a detailed summary of BCCPA’s position as it pertains to this issue.

Re: Intermunicipal Mobile Business Licence for Home Health Care

Recommendation

BCCPA recommends **the members of the Central Vancouver Island Intermunicipal Mobile Business Licence partnership** (City of Campbell River, Town of Comox, City of Courtenay, Village of Cumberland, City of Duncan, Town of Ladysmith, Town of Lake Cowichan, City of Nanaimo, Municipality of North Cowichan, City of Parksville, City of Port Alberni, and Town of Qualicum Beach) **support the inclusion of home health care in the eligibility of intermunicipal mobile business licences**. This licence is currently available for construction, trades, mobile hair salons, and catering.

By acting in this area municipal governments can support the sustainability of the private home health sector, and support seniors to age in their place of choice. Further, reducing costs for operators will lead to more competitive pricing for seniors.

Supporting an aging population:

By the end of the decade, close to one in four Canadians will be a senior, up from 17% now. ¹ RBC has identified Canada’s aging demographic as one of the two most significant existential threats facing the country over the coming decade (the first being climate

change). This will result in broad economic and health system challenges for Canadians and a host of other countries across the world. Action is needed now to support an aging demographic.

A recent (2020) National Institute of Ageing (NIA)/TELUS Health Survey found that 91 percent of Canadians of all ages, and almost 100 percent of Canadians 65 years of age and older, plan on supporting themselves to live safely and independently in their own home for as long as possible.² However, as referenced by NIA, about one-quarter of Canadians aged 75 and over has at least one unmet need with respect to their activities of daily living. Many older adults who live alone also experience issues such as loneliness, falls, malnutrition, and inactivity.

About home health care:

Home care and home support options (respectively medical care, and non-medical support, together referred to as home health care) enable seniors to get the help they need at home. Services range from publicly subsidized care, which is delivered through the health authorities, to customized private-pay options, which can include medical care, transportation, companionship, and home making. **Commonly, family paid home support provides seniors and their families with more flexibility and greater ability to ensure that they can focus on the things that will really help them stay independent for as long as possible.**

Business Licences in Vancouver Island:

Business licences are issued to ensure that local and visiting businesses are operating safely and legally, including complying with health and safety and zoning requirements. Home health businesses provide smaller jobs with less employees than trades and contractors, with less threat of safety hazards such as fires or damage to property, that would require first responders or management of public services. Comparatively, mobile home health services generate less profit margins than trades and contractors; if the cost of the business licence exceeds the cost of revenue, lack of competition in the market will impact service availability in the community.

Under the current system, home health care providers operating in the North Shore areas are required to hold distinct business licences for all locations in which they operate, regardless of the location of their physical office. By the nature of their work, home health providers are mobile, and commonly operate across several municipalities, adjusting their operations to accommodate client needs, and the availability of workers. This means that the vast majority work across multiple jurisdictions. Currently, across the province there are 17 intermunicipal mobile business licence partnerships between 104 communities.

The cost of multiple licences (the cost of which varies considerably across jurisdictions) add up to be significant for smaller organizations, which are already run

on slim margins, and are experiencing increased cost pressures relating to staffing, and inflationary pressures. The administrative cost of applying for and managing multiple licences is also burdensome. Reducing financial and administrative costs for operators will lead to more competitive pricing for seniors.

BCCPA and EngAge BC are recommending that this program be expanded to include home health care service providers.

About BCCPA

BC Care Providers Association (BCCPA) is the leading voice for B.C.'s continuing care sector. Our growing membership base includes over 450 long-term care, assisted living, and commercial members from across British Columbia.

BCCPA members support more than 19,000 seniors annually in long-term care and assisted living settings and 6,500 independent living residents. Additionally, our members deliver almost 2.5 million hours of home care and home support services each year.

Thank you for your consideration, I look forward to hearing from you.

Tiffany Trownson

Director of Public Affairs | EngAge BC
BC Care Providers Association
EngAge BC

1424-4710 Kingsway
Burnaby, BC, V5H4M2
w 604-736-4233 ext #243

www.bccare.ca



Our office is located on the traditional territory of the Musqueam, Squamish and Tsleil-Waututh Nations and we are grateful to live, learn and work on these shared lands.

RECEIVED

July 29, 2024

TOWN OF COMOX

LOG: 24-304	REFER:	AGENDA: RCM 07-Aug-24
FILE: 0220-40	ACTION: MR	

Copies: Council
JW/SR/CD

From: donovanfamily [REDACTED]
 Sent: Sunday, July 28, 2024 11:16 PM
 To: Town of Comox – Administration <town@comox.ca>
 Subject: Suicide Prevention Support for Indigenous Children

Greetings,

My name is Laura Donovan, a member of the Odanak First Nation, living on the territories of the Coast Salish peoples. I want to thank you for your willingness to read this letter, as it may be triggering and difficult to read. I am fully aware of the significant impact suicide has had on every one of our communities.

I have been a Tattooer for 15 years, starting in Selkirk, Manitoba, and now in Surrey, BC. I have tattooed countless memorials for fellow Indigenous community members, using my craft to heal and provide good medicine. I never expected that I would end up tattooing memorials to honor my daughter.

I would like to share information about my 16-year-old daughter Felicity, an Indigenous student and a proud Abenaki member of the Odanak First Nation. Felicity died by suicide in December 2023.

In October 2023, she reached out for suicide prevention support at school, and a call was made to an external agency for help. I did not learn about the details of this call or the support the school had committed to provide until January 2024. At that time, I discovered that the school principal had contacted a different external support agency, informing them that Felicity had not presented as at risk and was not on their radar.

Additionally, I learned that the Surrey School District had a suicide prevention protocol that was not followed and did not have a suicide prevention program or plan to address students presenting with thoughts of suicide, nor support for staff providing this support to students.

On July 22nd, I met with the BC Minister of Education to share Felicity's experience and the experiences of many Indigenous families who have shared their stories with me over the years. In the spirit of Truth and Reconciliation, to honor the lives of all Indigenous children lost to suicide and to ensure Every Child Matters, I have requested the following from the Minister:

1. The Ministry of Education ensures all schools in British Columbia are funded and equipped to provide suicide prevention support and training to staff and students based on

current evidence-based practices. I request this with the understanding that all children and staff will benefit from this honor.

2. The Ministry of Education performs an audit of the suicide prevention support provided to Felicity and our family.

I am reaching out to ask if you municipal leadership will add your voice to my call to provide improved suicide prevention support for children in your municipality. Please consider providing me with a letter addressed to the British Columbia Minister of Education, the Honourable Rachna Singh, in support of my request, with a CC to my email at [REDACTED]. The Minister's email is ECC.Minister@gov.bc.ca.

If you have any questions or would like more information that will enable you to provide a letter of support, please reach out to me, and I will respond promptly.

I have attached supporting information for your reference:

1. A picture of Felicity.
2. A record of the call the School Counsellor made to a suicide prevention program where Felicity asked for support.
3. A record of the call the Principal made to another suicide prevention program, informing them Felicity was not presenting as at risk or on their radar.
4. A letter of support from the Chief and Council of Odanak First Nation.

Thank you for your time and attention,

Laura Donovan (Felicity's Mom)



RECEIVED

July 29, 2024

TOWN OF COMOX

LOG: 24-308	REFER:	AGENDA: RCM 07-Aug-24
FILE: 4010-20	ACTION: MR	

From: Darrell Tomkins [REDACTED] >
 Sent: Monday, July 29, 2024 11:34 AM
 To: council <council@comox.ca>
 Cc: [REDACTED]
 Subject: Vandalism and security in Marina Park

Copies: Council
 JW/SR/RN/CD

Dear Town of Comox Councillors,

I wish to report a problem with security in Marina Park. It is affecting the heavily treed area of the park near the d’Esterre House and a private and Marina Place residences. I live at Marina Place in a unit directly affected.

Every night, barring rain and weather, a group of young people has been gathering under the trees, next to the linden tree where the Story Walk is often installed. The latter is a beautiful display of a children’s book that is changed throughout the summer.

The young people are using the adjacent part of the Park as a hangout, usually starting around 4:00 p.m. and going on till dark. They are loud enough that I don’t want to sit out on my patio. Also they have smoked so much on two occasions that I have had to close all my windows. Two nights ago, before the rain, I could see flames through the hedge. Today I met a lovely Town worker, Emma, who was clearing away the garbage. She says that she and other workers have noted the accumulation of garbage in this area.

There are three safety issues: vandalism in the amount of garbage left around the trees, security for children less than 50 metres away and fire hazard from smoking.

I am respectfully requesting that there is a regular surveillance of this area, either by by-law officers or the RCMP, and signage indicating no smoking or littering would be tolerated.

Respectfully yours
 Darrell J. Tomkins

Darrell Tomkins
 [REDACTED]

From: Debbi Topp [REDACTED]
Sent: Tuesday, July 30, 2024 2:48 PM
To: Town of Comox – Administration <town@comox.ca>
Subject: Giant Digital Screen

Copies: Council
JW/SR/CD/RN/MS/KG

Mayor Minions and Town of Comox Counselors

I must say I was somewhat flabbergasted when I saw the giant digital screen down at the Marina Park. It's visible even a quite a distance. The Town of Comox went out of their way to make the buildings down there blend in with the surrounding scenery and keep them as natural as possible and then you put that overpowering screen there. Most of what I saw were advertisements for the dentists, the weather and town meetings. Do we really need this on display. Personally I think the screen is an eye sore and I'm disappointed with the decision to place it in such a lovely space. Just my opinion I know, but I thought you should know my feelings.

Debbi Topp
[REDACTED]

RECEIVED

August 1, 2024

TOWN OF COMOX

LOG: 24-311	REFER:	AGENDA: RCM 07-Aug-24
FILE: 4010-20	ACTION: MR	

From: Trish Johnson [REDACTED]
Sent: Thursday, August 1, 2024 10:03 AM
To: council <council@comox.ca>
Cc: Trish Johnson [REDACTED]
Subject: COMPLAINT, immediate resolution

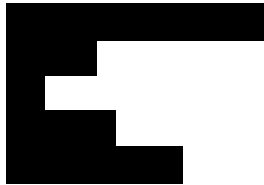
Copies: Council
 JW/SR/CD/TH/PM

Hello to All

I am writing this email to strongly voice my need for help.
 Off and on for years we have put up with continual barking of dogs, a sighting of a small pig on our property from the Reserve four doors from our home.
 Dogs aggressively harassing us as we swim or walk on the beach in front of our home.
 It has gone too far!
 Tuesday my sister and I rolled the garbage up to the top of Comox Hill, 300 feet.
 When walking down our communal driveway the large, aggressive dog that roams the beach freely, as his territory, came out onto our road. Growling, baring his teeth, barking and blocking the way to our home. Video below

This morning my housekeeper walked down the road and there was a giant pig.
 This is completely unacceptable. Completely!
 For us to not be able to go to our beach, walk to get the mail or take the garbage up to Comox Avenue and now the Pig?
 I strongly request something be done immediately.

Sincerely
 Trish Johnson





RECEIVED

July 31, 2024

TOWN OF COMOX

From: Natanya Birkhaven [REDACTED]
 Sent: Wednesday, July 31, 2024 5:07 PM
 To: Town of Comox – Administration <town@comox.ca>
 Subject: This is RIDICULOUSNESS

LOG: 24-310	REFER:	AGENDA: RCM 07-Aug-24
FILE: 0220-20	ACTION: MR	

Copies: Council
 JW/SA/SR/CD

And not very well thought out
 If at all ...

Art gallery ?
 Art classes ?
 Doctors offices ?
 Bridge ?
 Tuesday Taxes ?
 Church services. ?
 Etc
 Never mind dining at many restaurants
 Tourists unaware of attacks

And causing The Bingo Women
 To ATTACK suspects
 As they now say they OWN the parking lot

A CRIMINAL DISASTER WAITING TO HAPPEN

Please forward to mayor and council

Pps: I have repeatedly tried to contact Your by-law men But They don't answer, nor reply to messages

Nattanya Ashala Birkhaven

[REDACTED]

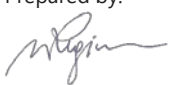

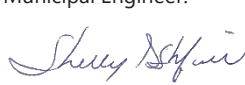

[REDACTED]

I have had 2 attacks from bingo woman
 And I know of 4/5 others that have been attacked



Nattanya Birkhaven

To: Mayor and Council	File: RZ OCP 23-2, PR 24-9
From: Elliot Turnbull, Planner II Regina Bozerocka, Planner II	Date: August 7, 2024
Subject: 2077 Hector Road and 941 Aspen Road: RZ OCP 23-2, PR 24-9 Post-Public Hearing Report	

Prepared by:  Regina Bozerocka, Planner II	Prepared by:  Elliot Turnbull, Planner II	Municipal Engineer:  Shelley Ashfield Director of Operations	Report Approved:  Jordan Wall, CAO
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RECOMMENDATIONS FROM THE CHIEF ADMINISTRATIVE OFFICER:

Amend the bylaws to remove east-west trail and reduce height allowance to 4 storeys

1. That Comox Official Community Plan Amendment Bylaw 1685.11 (**Attachment 2**) be amended at Second Reading by:
 - a) deleting section 2(1) regarding Schedule "A" Part 2: Objectives and Policies, Section 2.1.1.3 Residential Policies;
 - b) deleting the text, "and a new east-west neighbourhood trail" from section 2(2)(d);
 - c) replacing "Schedule 4" in section 2(2)(e) with "Schedule 3";
 - d) replacing "Schedule 5" in section 2(2)(f) with "Schedule 4";
 - e) replacing Schedule 2 with a new "Schedule 2, as included in Attachment 5 of the August 7, 2024 Staff Report from the Planner II, titled "2077 Hector Road and 941 Aspen Road: RZ OCP 23-2, PR 24-9"; and,
 - f) renumbering accordingly.
2. That Comox Zoning Amendment Bylaw 1850.46 (**Attachment 3**) be amended at Second Reading by:
 - a) deleting the text, "or required storey differential" from section 2Ea);
 - b) deleting the text, "or required storey differential" from section 2Ec);
 - c) deleting from Schedule 1A:
 - i) subsections (1) and (2) from section 211.8, and
 - ii) section 211.10;
 - d) deleting from Schedule 1B;
 - i) subsections (a) and (b) from subsection 211.9(1), and
 - ii) section 212.11;

- e) deleting from Schedule 1C;
 - i) subsections (a) and (b) from subsection 310.8(1), and
 - ii) section 310.10; and,
 - f) renumbering accordingly.
3. That Comox Subdivision and Development Servicing Amendment Bylaw 1261.18 (**Attachment 3**) be amended at Second Reading by:
- a) deleting subsection 2(1)(f) and figure C-4.; and,
 - b) renumbering accordingly.
4. That Schedule 1, Outstanding Item 3(c) of Planning Report RZ OCP 23-2, PR 24-9 dated August 7th, 2024 be replaced with the following :
- Long term protection of the environmental areas and provision of a 15.0 metres natural buffer for a wetland, in accordance with RPBio report including a Wetland Management Plan (WMP) by an RPBio specifying measures for the protection of the wetland during land clearing and construction on 2077 Hector which includes oversight and monitoring requirements by a Qualified Environmental Professional during land clearing and construction. The WMP should also include reporting requirements to the Town during and after land clearing and construction is complete, as well as any other measures specified by the RPBio which includes measures already outlined in the Environmental Impact Assessment by Roe Environmental Ltd dated March 13, 2023, and the Environmental Memo by Roe Environmental Ltd dated May 7th 2024.

ALTERNATIVES TO THE RECOMMENDATIONS:

Alternative 1 – *Only amend the bylaws to remove the new east-west trail and proceed with 6 storeys as originally proposed.*

- 1. That Comox Official Community Plan Amendment Bylaw 1685.11 (**Attachment 2**) be amended in accordance with only the blue text shown in **Attachment 5** including renumbering accordingly; and,
- 2. That Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18 (**Attachment 4**) be amended in accordance with the red text shown in **Attachment 7** including renumbering accordingly.

Council can also seek alternative motions from staff to address any of the concerns raised in the Public Hearing.

PROPOSAL

The proposal is for Official Community Plan (OCP) Amendment, Zoning Amendment, Phased Development Agreement Authorization (PDA) and Subdivision and Development Servicing Amendment Bylaws, to facilitate a proposed phased multi-family residential and mixed-use development at 941 Aspen and 2077 Hector Roads.

The multiphase development would result in approximately 750 apartment and townhouse units, and a 600 m² commercial space, including a 28-child daycare. The other amenities proposed by the applicant are reflected in Schedule 1, Outstanding Items, covenants required to be registered prior to adoption of the proposed bylaws.

REPORT SUMMARY

Subject Properties

First Phase

941 Aspen Road (Aspen-West)

approx. 15,800 m² (1.6 ha)

PID 023-020-113

Lot 1 DL 170 Comox District Plan
VIP60685 Except Plan EPP118279



Future Phases

2077 Hector Road

approx. 40,900 m² (4.0 ha)

PID 003-856-704

Lot A, DL 170, Comox District,
Plan 18002

and

941 Aspen Road (Aspen-East)

approx. 34,800 m² (3.5 ha)

PID 023-020-113

Lot 1 DL 170 Comox District Plan
VIP60685 Except Plan EPP118279



Applicant:	Neil Bolton, Highstreet Ventures
Owner:	Highstreet Hector Road Developments Ltd. Inc. No. BC13677778
Subject properties contain	Currently the subject properties are vacant and not serviced
Surrounding Land Uses	Large residential lots within Comox Valley Regional District to the north and east; Townhouses and single-family developments to the south and west; and City of Courtenay single-family properties to the west.
Current land use Current zone	The subject properties have the Comox Valley Regional District land use designation: Settlement Expansion Area; and CR-1 Country Residential – 1 zoning from the time of incorporation into the Town boundaries in 2016.
Development proposal	Over several phases, development of approximately 750 multi-family apartments and townhouse units and a neighbourhood commercial node.

Key Issues	
Public Hearing	<p>Issues raised at the Public Hearing include the following:</p> <ol style="list-style-type: none"> 1. Impact of the development on the surrounding rural properties and a desire for increased setbacks at the perimeter of the subject properties; 2. Environmental: concerns regarding the existing wetlands and desire to see increased buffers around them as well as involvement of additional professionals to oversee development around the wetlands and concerns regarding loss of habitat and trees; 3. Concerns regarding the proposed storm water retention (wet) ponds and a desire for storm water detention (dry) ponds instead; 4. Proposed OCP and Zoning Bylaw permitting up to six storeys while the developers only propose four storeys; 5. Questions regarding the idea of a required trail that was proposed through the site in previous reports which was removed by Council; 6. Infrastructure: increased traffic impacting existing neighbourhoods around the site, particularly on the rural road system, impact on water supply due to increased residents, and sewer capacity in the area; and, 7. Public services: increasing pressures on medical, firefighting, policing and community services (childcare, doctors, etc). 8. Ability for the subject property to be serviced by sanitary services. <p>This report will comment on some of the major issues raised at the Public Hearing.</p>
Bylaw amendments	<p>This report proposes amendments to Comox Official Community Plan Amendment Bylaw 1685.11, Comox Zoning Amendment Bylaw 1850.46, and Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18. These amendments are in accordance with Council's direction to remove the previous east-west trail requirement through 2077 Hector Road at 1st and 2nd Reading. In addition, given community concerns regarding height, amendments to Bylaws 1685.11 and 1850.47 are proposed to reduce the height allowance from six storeys to four storeys.</p>
Servicing Requirements	<p>There are outstanding issues regarding CVRD sewer capacity in this area which need to be clarified prior to Third Reading. Another report will be brought to Council regarding this issue.</p>

Council Decision Options

Council consideration of amending the proposed bylaws

Decision options

Recommended:

Council amends the proposed bylaws to remove trail requirement and reduce height allowance.

Implications

Bylaws amended to remove the administrative error in requiring an east-west trail through 2077 Hector Rd and reduce the height allowance to 4 storeys on all subject properties based on public feedback.

Alternative 1 – Amend the bylaws to only remove trail requirement.

Council may amend the bylaws only to remove the requirement for the new east-west trail through 2077 Hector Rd and keep the six storey height allowance on all subject properties.

Alternative 2 – Council may alter the bylaws further

Council may make other changes to the bylaws provided that the alterations do not do any of the following: alter the use, increase the density, decrease the density without owner's consent, or alter the residential tenure in any area. If alterations to the bylaw do any of these, another Public Hearing will be required.

PROCESSING PROCEDURES

Full background information is available in the June 5th 2024 report to council, available on that day's agenda, page 80¹. The June 7th, 2023 Introductory Report to Council is also available at on the Town website².

¹ https://www.comox.ca/sites/default/files/2024-06/2024_06June_05_Regular%20Council%20Meeting%20Agenda.pdf

² https://www.comox.ca/sites/default/files/2023-06/1_RCM_7%20June%202023_RZ%20OCP%2023-2_941%20Aspen_2077%20Hector_INTRO.pdf

At the June 5, 2024 RCM, Council resolved (2024.216 to 2024.222)

1. *THAT Comox Official Community Plan Amendment Bylaw 1685.11 be given First Reading.*
2. *THAT, having considered the Town of Comox May 2020 Housing Needs Report, and Summary Form; the Comox Strathcona Waste Management 2012 Solid Waste Management Plan and the impact on the current Financial Plan, Comox Official Community Plan Amendment Bylaw 1685.11 be given Second Reading.*
3. *THAT Comox Phased Development Agreement Authorization Bylaw 2024: Aspen-Hector be given First and Second Readings.*
4. *THAT a Public Hearing in respect of Bylaws No: 1685.11, 1850.46 and 2024 be scheduled for July 10, 2024, 6:00 pm, at d'Esterre House, 1801 Beaufort Avenue, with an option for online participation, and notices be published as required in accordance with the Local Government Act.*
5. *THAT Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18 be given First and Second Readings.*
6. *THAT Comox Zoning Amendment Bylaw 1850.46 be given First and Second Readings, and that Item 1.0 in Schedule 1, Outstanding Items, of Planning Report RZ OCP 23-2, PR 24-9 dated June 5, 2024 be deleted*

On July 4th, 2024, a Special Council Meeting was held to correct typos that were present in Bylaw 1850.46.

Attachment 1 lists completed and remaining processing steps aligned with the Chief Administrative Officer's recommendations for Council's concurrent consideration of the Official Community Plan Amendment Bylaw 1685.11, Zoning Amendment Bylaw 1850.46, Phased Development Agreement Authorization Bylaw 2024: Aspen-Hector and Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18.

PUBLIC HEARING AND PUBLIC SUBMISSIONS

A Public Hearing in respect of Bylaws 1685.11, 1850.46, Phased Development Agreement Authorization Bylaw 2024: Aspen-Hector, and Bylaw 1261.18 was held on July 10, 2024. A copy of the *Local Government Act*, Section 465 Public Hearing summary report is included on the August 7, 2024 Regular Council Meeting Agenda which includes written submissions made at the Public Hearing.

Key Issues from Public Hearing

Setbacks

The new zones proposed for the subject properties, RM 6.1 Apartment, RM 6.2 Apartment and Townhouse, and C7.1 Commercial Multi-Family propose the following setbacks:

- Front and exterior side: 3.0 m with 9 x 9 m tree well setback per every 100 metres of frontage³
- Rear and interior side: 5.0 to 9.0 m depending on building size and height (note where a parcel abuts a greenway that is at least 10 metres wide, a reduction to a 5.0 m setback would apply, regardless of building height or length).

The intent of reduced setbacks is to promote a more efficient use of land, increase flexibility for development, and promote a livelier streetscape. To address impacts on neighbouring properties, the proposed zones increase the rear and interior side setback to 9.0 m for apartment buildings greater than 3.0 m.

The applicant's preliminary site plan dated April 2024 shows lower densities (townhomes) and open space (community space, ponds, etc.) adjacent to the existing rural single-family properties and higher densities (apartment buildings) shifted towards Aspen and Hector Roads. This can contribute to lessening the impact on adjacent single-family properties. On the other hand, as this application does not include concurrent processing of a Development Permit, the site plan is subject to change provided it meets the minimums in the Zoning Bylaw.

Council may amend the bylaw to increase the setbacks, however this could result in a loss of units.

Wetland Buffers and Monitoring

Wetlands are protected at the federal and provincial level. The Town does not have any bylaws or regulations which specifically protect wetlands other than indirectly through environmental Development Permit Areas.⁴

The applicant for 941 Aspen and 2077 Hector Road has engaged two professional biologists to assess the wetland on 2077 Hector Road and determine an appropriate buffer area around it. The buffer width

³ In contrast, for the RM7.1 standard townhouse introduced for 458 Anderton a 3.0 m setback with 5 x 5m tree well setbacks per 60 m is proposed reflecting the smaller building size and height permitted.

⁴ DPA #7 Riparian Areas, DPA #8: Waterfront Environment, DPA #9: Upland Environment, DPA #10: Bald Eagle and Great Blue Heron Nesting Sites/Perching Trees, DPA #11: Wildlife Corridor, and DPA 12: Garry Oak Habitat.

proposed by their reports⁵ is 15 metres. Schedule 1 outstanding items contains the following condition: Registration of Section 219 restrictive covenant on titles of all subject properties for the provision of long-term protection of the environmental areas and provision of a 15.0 metres natural buffer for a wetland, in accordance with RPBio report.

The provincial government's Wetland Ways: Interim Guidelines for Wetland Protection and Conservation in British Columbia⁶ states that "A no-development, no-disturbance buffer zone of 30 m or more will benefit a variety of species. For very small wetlands, a 3:1 ratio of undisturbed upland habitat to water is recommended."

In terms of protection of the wetland during construction, the following recommendations are included in Roe's May 7th 2024 Environmental Memo:

- A Stormwater Management Plan (SMP) will be prepared by a qualified professional to ensure the conservation of pre-development surface water flow to the wetland during and after land development activities.
- An Erosion and Sediment Control plan will be prepared by a qualified professional that demonstrates how sedimentation of the wetland will be prevented during development.
- Highstreet and Roe will conduct planning and implement measures to prevent the introduction of invasive plants to the wetland area during development.

Develop with Care: Environmental Guidelines for Urban and Rural Land Development in British Columbia (2014)⁷ states the following as a best practice:

"If Environmentally Valuable Resources (on or near the development site) could be at risk during the land clearing and construction stages, hire an appropriately qualified professional to assist in planning and monitoring. Environmental consultants and monitors should be given the authority to halt all work if they believe on-site conditions could create impacts on sensitive species or habitats. This will demonstrate due diligence should problems arise."

The recommendation of this report is to amend the list of outstanding items to include a Wetland Management Plan (WMP) by an RPBio specifying measures for the protection of the wetland during land clearing and construction on 2077 Hectar which includes oversight and monitoring requirements by a Qualified Environmental Professional during land clearing and construction. The WMP should also include reporting requirements to the Town during and after land clearing and construction is complete, as well as any other measures specified by the RPBio which includes measures already specified in the

⁵ Copies of all professional reports are available by contacting the planning department or reviewing previous reports

⁶ <https://www2.gov.bc.ca/gov/content/environment/air-land-water/water/water-planning-strategies/wetlands-in-bc>

⁷ <https://www2.gov.bc.ca/assets/gov/environment/natural-resource-stewardship/best-management-practices/develop-with-care/dwc-section-3.pdf>

Environmental Impact Assessment by Roe Environmental Ltd dated March 13, 2023, and the Environmental Memo by Roe Environmental Ltd dated May 7th 2024.

Height

Both Official Community Plan Amendment Bylaw 1685.11 and Zoning Amendment Bylaw 1850.46 propose amendments to permit up to six storeys on the subject properties.

In December 2023 and March 2024, Council received staff presentations on the new standard multi-family zones which were developed in consultation with the applicant for this application. The new zones proposed for the subject properties, RM 6.1 Apartment, RM 6.2 Apartment and Townhouse, and C7.1 Commercial Multi-Family are based on the zoning regulations presented to Council and permit up to six storeys.

The intent for a six-storey maximum height was to provide flexibility for the applicant should additional environmental setbacks be required which could result in a loss of units. In addition, as the three zones were intended to be the future standard zones for all greenfield development a six-storey height would provide the greatest flexibility for developers.

Given that comments from both the applicants for the subject properties (Highstreet) and for 2123 Hector Road (Broadstreet), and the public received at the public hearing were not in support of six storeys, and the applicants are not proposing six storeys, the recommendation of this report is to amend Bylaw 1685.11 and Bylaw 1850.46 to reduce the height allowance to four storeys. Attachment 8 contains letters from both developers confirming they wish to proceed with a reduced height allowance.

The creation of a new six storey zone could be considered during the upcoming Official Community Plan and Zoning Bylaw review.

Alternative Recommendation #1 is to proceed with the bylaws with a six-storey maximum height.

Retention (wet) vs Detention (dry) Stormwater Ponds

The Anderton Corridor Servicing Study, Stormwater Management Plan (SWMP) requires that all stormwater management ponds in the area are designed and constructed as wet ponds.

Per the SWMP, wet pond configuration is more efficient in the use of land than a dry pond. Dry ponds require that the bottom of the pond is sloped to allow for the full drainage of the pond. This bottom slope effectively reduces the average depth available for stormwater storage and leads to a larger overall area required to construct the pond. Since wet ponds do not need to be drained, there is no need to have a sloped bottom. Wet ponds will have a larger storage volume than a dry pond constructed with the same footprint.

Wet ponds have the added benefit of improving the quality of the storm water being discharged. Benefits include a reduction in sediment and other pollutants such as heavy metals and unfavourable nutrients.

The pond configuration, design details, and level of maintenance determine the effectiveness of pollutant removal.

Many potential configurations and features for a wet pond are possible. Wet ponds, if designed correctly, can provide the community with recreation and educational opportunities while minimizing the costs associated with maintenance. Recreation can be in the form of non-contact activities such as hiking, and bird watching. Educational opportunities could involve school tours, information boards, and ecosystem observation. Vegetation around wet ponds can provide habitat for mosquito predators and other fauna.

Dry ponds have been designed and included in the Town Subdivision and Development Services Bylaw for NE Comox due to the nautical aviation requirements. Dry ponds can be used for recreational activities as they function as an open field when not being used for stormwater storage. The conditions can change quickly during rain events and drainage times would affect the ability for the ponds to be used for recreation. When used for fields, dry ponds need to be maintained to a high level similar to that of a groomed sports field. This level of grooming is higher than what is done for wet ponds.

The Town currently has 13 existing retention ponds. One pond has invasive plants that are being maintained on a regular basis to attempt to remove the species and control the spread. We are not aware of any infestation of invasive animals, such as bullfrogs, in any pond. In fact, wet ponds provide habitat for species of native wetland reptiles, insects, and animals. Mosquitoes are not an issue around the existing wet ponds. Mosquito breeding typically occurs in stagnant water bodies with a depth of less than one metre. 5 of the 6 types of ponds contained within the SWMP include a permanent water depth of between 2 and 3 metres and include mosquito predator habitat.

Administration will work with a professional biologist and the Coastal Invasive Species Committee to ensure design of wet ponds includes mitigation factors to deter invasive animals such as the bullfrog, as well as create a proper management plan.

There are a number of benefits to a neighborhood retention pond (wet pond) verses a detention pond (dry pond) these include, potential educational value, efficiency in land use, recreational benefit, improvement in water quality, decrease operation and maintenance costs as such Administration recommends retention ponds in the Anderton Corridor Area per the Anderton Corridor Servicing Study, SWMP.

New East-West Trail

At the June 5, 2024 Regular Council Meeting, Council removed the requirement for a new east-west trail through 2077 Hector Road. Attachment 5-7 shows the amendments to Bylaws 1685.11, 1850.46, and 1261.18 in accordance with Council's direction at 1st and 2nd reading. In addition, the proposed amendments correct minor typos that were present in Bylaw 1685.11.

Next Steps

The Town is currently working with the Comox Valley Regional District regarding sewer capacity in this area. These issues need to be clarified prior to Third Reading as they may have implications for outstanding items. Once the issues are better understood, another report will be brought to Council.

ET/RB

Schedule 1, Outstanding Items

Attachments

1. List of steps for concurrent processing of proposed bylaws
2. Comox Official Community Plan Amendment Bylaw 1685.11
3. Comox Zoning Amendment Bylaw 1850.46
4. Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18
5. Comox Official Community Plan Amendment Bylaw 1685.11 Markup
6. Comox Zoning Amendment Bylaw 1850.46 Markup
7. Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18 Markup
8. Letters from Highstreet and Broadstreet confirming they support 4 storeys

SCHEDULE 1

OUTSTANDING ITEMS

- Affordable housing contribution in accordance with CCL-069 (\$728,000 for the First Phase, per PDA Bylaw,);
- Contribution of \$100,000 toward construction of a tot-lot playground within Hector Greenway (per PDA Bylaw,), based on the 2021.060 Council Resolution to seek cash contributions from developers of adjacent properties in proportion of proposed residential units);
- Grant to the Town an option to purchase approximately 250 m² commercial unit with 220 m² outdoor space suitable for child care facility at construction cost (per PDA Bylaw,);
- 10% of all residential units, distributed equally throughout the development phases, meeting the Town adaptable requirements;
- Sound attenuation for residential units within at least 35 metres from Aspen Road and alternative means for those units ventilation;
- Conduits and electrical capacity to accommodate one Electrical Vehicle charger in each townhouse garage and one for every unit in apartment buildings (approximately 80% of parking stalls would have the conduits and ability to install a charger);
- Energy Step Code Level 4 for Part 3 buildings (current Town Building Bylaw requires minimum Energy Step Code Level 3);
- Road dedication, 10.0 metres wide for new Hector Greenway section on the south side of Hector Road;
- Tree retention and replanting in accordance with Council Policy CCL-067, seeking 30% retention or replanting;
- Long term protection of the environmental area and provision of a 15.0 metres natural buffer;
- Oil/ grit separators for all surface parking and driveways; and
- Road dedication for and construction of a stormwater retention pond, infiltration trenches and associated facilities servicing all phases of the proposed development.

Outstanding Items to be resolved prior to adoption of proposed bylaws

- 1.0 Registration of development covenant in accordance with Phased Development Agreement to grant to the Town an option to purchase approximately 250 m² commercial unit with 220 m² outdoor space suitable for child care facility at construction cost (per PDA Bylaw);

2.0 Registration of covenant for the provision of road dedication and construction of 10 m wide greenway along north lot line of 2077 Hector Road property

3.0 Registration of Section 219 restrictive covenants on titles of all subject properties for the provision of the following:

- a. establishment of a Local Area Service for all phases of the proposed development, for the operation, maintenance and replacement of Storm Water Management System (retention pond and infiltration trenches) to service all phases of the proposed development. This will require the submission of a petition, in accordance with section 212 of the *Community Charter* and Council adoption of a local service area establishment bylaw
- b. Long term existing tree protection or replanting and long term protection in accordance with CCL-067, seeking minimum 30% retention or replanting
- c. Long term protection of the environmental areas and provision of a 15.0 metres natural buffer for a wetland, in accordance with RPBio report
- d. Construction of minimum 10% of all proposed dwelling units meeting Comox Zoning Bylaw 1850, Section 5.20 Special Needs Housing Standards – Adaptable Housing, distributed equally throughout the development phases
- e. Construction to meet Energy Step Code Level 4 for apartment and townhouse buildings
- f. Provision of conduits and electrical capacity to accommodate one electrical vehicle charger in each townhouse garage and one for every unit in a multi-unit development, approximately 80% of all parking stalls
- g. Covenant to ensure residential units within 35 metres from Aspen Road have sound attenuation that meets Canada Mortgage and Housing Corporation Standards, in accordance with *Town of Comox, Acoustical Standards for Residential Developments*, a report prepared by Wakefield Acoustics, dated October 30, 2014 and having alternative means to window opening for ventilation
- h. provision of oil/ grit separators and their future maintenance for all parking areas
- i. provision of required road dedication for the stormwater pond and other storm facilities

- j. provision of off-site works: road upgrades for all adjacent sections of Aspen Road and Hector Road; greenway construction; stormwater management system construction complete with landscaping and 3.0 metres wide asphalt maintenance path around the pond, and otherwise in accordance with Town specifications.

ATTACHMENT 1

PROCESSING STEPS

The following processing steps would allow the following bylaws to be considered concurrently:

Comox Official Community Plan Amendment Bylaw 1685.11

Comox Zoning Amendment Bylaw 1850.46

Comox Phased Development Agreement Authorization Bylaw 2024: Aspen-Hector

Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18

COMPLETE STEPS:

1. Council consideration of proposed OCP amendment public consultation opportunities in accordance with *Local Government Act*, section 475;
2. Publication of proposed OCP amendment notice on Town's bulletin board and website and the Introductory Planning Report on Town's website;
3. First Reading of proposed OCP Amendment Bylaw;
4. Council consideration of the Housing Needs Report dated May 2020, the Comox Strathcona Solid Waste Management Plan, and the current Financial Plan;
5. Second Reading of proposed OCP Amendment Bylaw;
6. First and Second Reading of proposed Zoning Amendment and Phased Development Agreement Bylaws;
7. First and Second Reading of proposed SD Bylaw
8. Notification of Public Hearing on proposed bylaws;
9. Public Hearing;

REMAINING PROCESSING STEPS:

10. Third Reading of proposed bylaws;
11. Resolution of any outstanding items; and
12. Adoption of proposed bylaws.

Comox Official Community Plan Amendment Bylaw 1685.11

TOWN OF COMOX

BYLAW 1685.11

A BYLAW TO AMEND COMOX OFFICIAL COMMUNITY PLAN BYLAW 1685

WHEREAS Council has the authority under the provisions of the Local Government Act to amend the Official Community Plan Bylaw;

AND WHEREAS Council has considered the amendments in conjunction with its financial plan, any waste management plan that is applicable in the municipality or regional district and Town of Comox May 2020 Housing Needs Report Data Results including Summary Form;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. Title

This Bylaw may be cited for all purposes as "Comox Official Community Plan Amendment Bylaw 1685. 11".

2. Amendments

Comox Official Community Plan Bylaw 1685 is hereby amended as follows:

(1) Schedule "A" Part 2: Objectives and Policies, Section 2.1.1.3 Residential Policies, as follows:

(a) By replacing Section 2.1.1.3(g) with the following text:

Land designated Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill as shown on *Map 1 - Land Use Designations* is intended to accommodate the following land uses low rise apartments up to a maximum of 4 storeys, townhouses, single detached dwellings, secondary suites, coach houses, duplexes, triplexes, patio homes and child care facilities. Excluded from the 4 storey apartment maximum are the following:

- i. Subject to the provision of satisfactory amenities, 1700 Balmoral Avenue – Lot A, Section 56, Comox District, Plan VIP81758;
- ii. 941 Aspen Road - Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279;
- iii. 2077 Hector Road - Lot A, DL 170, Comox District, Plan 18002; and
- iv. 2123 Hector Road – Lot 4 District Lot 170 Comox District Plan VIP60685.

- (2) Schedule "A" Part 5: Maps is hereby amended by:
- (a) Designating Western portion of the parcel legally described **Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279** (941 Aspen West), shown shaded in Schedule "1A", which is attached to and forms part of this bylaw, as within the following:
 - (i) Map 1 – Land Use Designations, as Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill;
and
 - (ii) Development Permit Areas (DPAs), as shown in their respective maps:
 - # 3 General Multi-Family
 - # 11 Wildlife Corridor
 - (b) Designating Eastern portion of the parcel legally described **Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279** (941 Aspen East), shown shaded in Schedule "1B", which is attached to and forms part of this bylaw, as within the following:
 - (i) Map 1 – Land Use Designations, as Mixed Use: Commercial – Residential;
and
 - (ii) Development Permit Areas (DPAs), as shown in their respective maps:
 - #4 Mixed Use: Commercial - Residential
 - # 11 Wildlife Corridor
 - (c) Designating parcel legally described **Lot A, District Lot 170, Comox District Plan 18002** (2077 Hectar), shown shaded in Schedule "1C", which is attached to and forms part of this bylaw, as within the following:
 - (j) Map 1 – Land Use Designations, as Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill;
and
 - (ii) Development Permit Areas (DPAs), as shown in their respective maps:
 - # 3 General Multi-Family
 - # 7 Riparian Areas
 - # 11 Wildlife Corridor

- (d) Adding to Map 3, Parks, Trails and Open Space, an extension of Hector Greenway and new east-west neighbourhood trail shown in red line in Schedule "2", which is attached to and forms part of this bylaw, as "Off-Street Trails (existing and proposed)"
 - (e) Adding to Map 5, Road Network, an extension of Hector Road shown in bright blue line in Schedule "4", which is attached to and forms part of this bylaw, as "Minor Collector"
 - (f) Adding to Map 7, Bicycle Network, an extension of Hector Road shown in bright green line in Schedule "5", which is attached to and forms part of this bylaw, as "Proposed Major Bike Route"
- (3) Comox Official Community Plan Bylaw 1685 is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw.

3. Repeal Comox Valley Regional District OCP Bylaw

- (1) Rural Comox Valley Official Community Plan Bylaw No. 337, 2014 (as at September 19, 2016), is hereby amended by replacing section 1(3) in its entirety with the following text:
- This bylaw applies to all lands as noted in section 1(1) except in relation to those properties legally described as:
- (a) PID 023-020-113, Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279; and
 - (b) PID 003-856-704, Lot A, District Lot 170, Comox District Plan 18002
- (2) Rural Comox Valley Official Community Plan Bylaw No. 337, 2014 (as at September 19, 2016) is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw.

4. Adoption

- (1) READ A FIRST time this 5th day of June, 2024
- (2) READ A SECOND time this 5th day of June, 2024
- (3) ADVERTISED A FIRST time this 26th day of June, 2024
- (4) ADVERTISED A SECOND time this 3rd day of July, 2024
- (5) PUBLIC HEARING HELD this 10th day of July, 2024
- (6) READ A THIRD time this day of , 2024
- (7) ADOPTED this day of , 2024

Mayor

Chief Administrative Officer

BYLAW 1685.11

SCHEDULE "1A"

941 Aspen Road (Aspen-West)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



BYLAW 1685.11

SCHEDULE "1B"

941 Aspen Road (Aspen-East)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



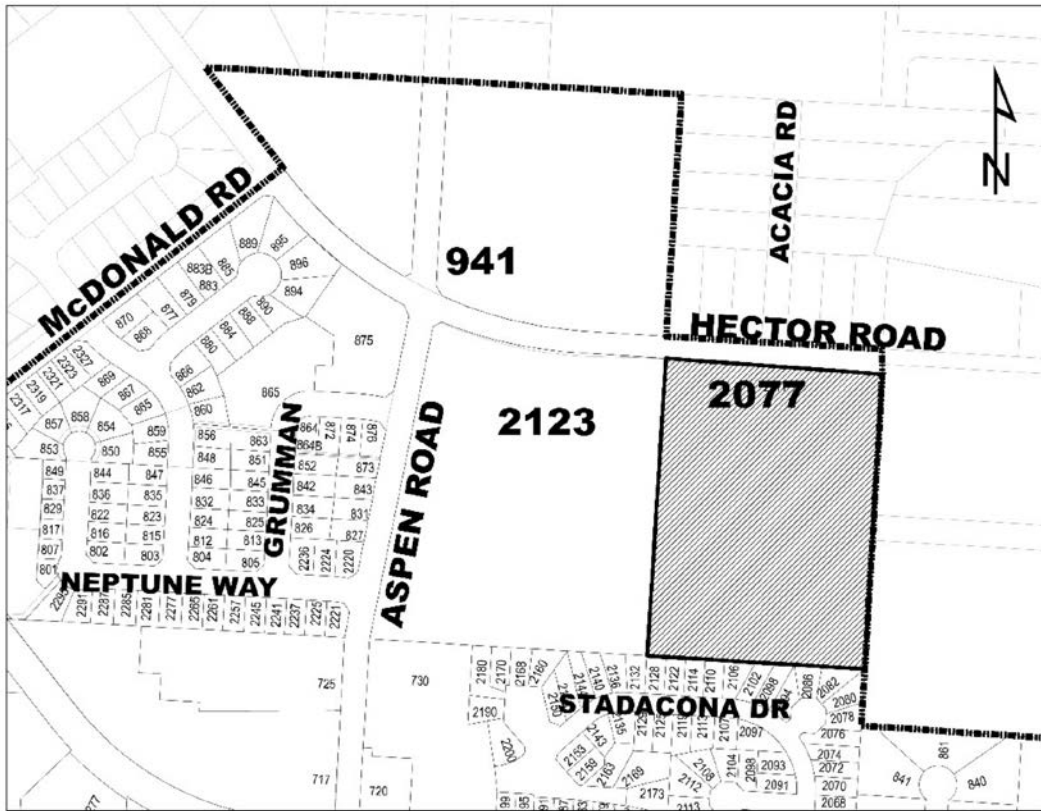
BYLAW 1685.11

SCHEDULE "1C"

2077 Hector Road

PID 003-856-704

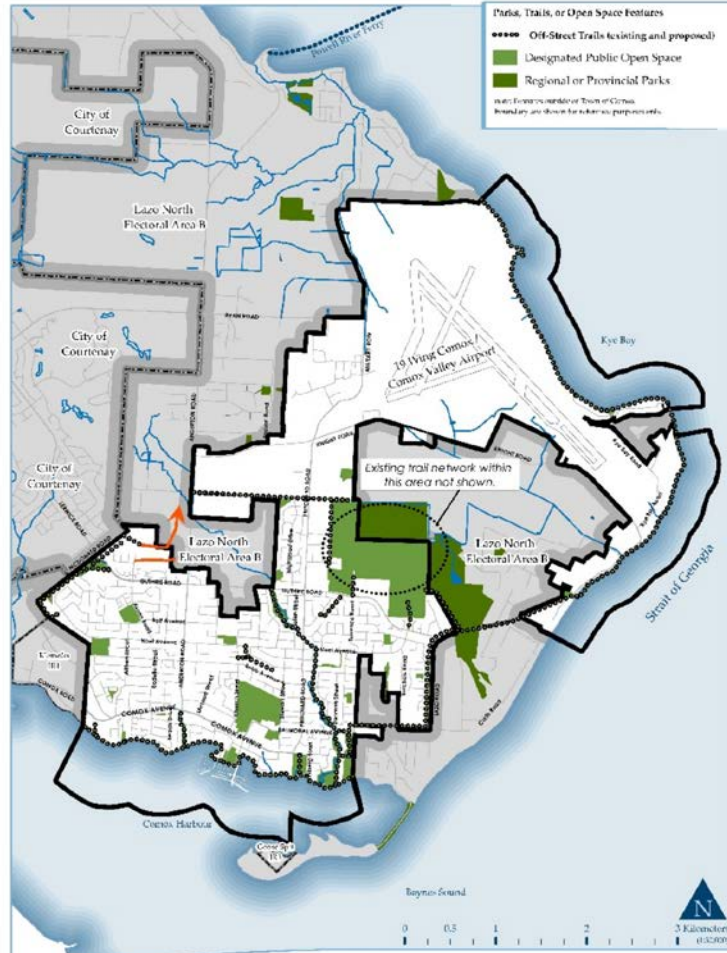
Lot A, DL 170, Comox District, Plan 18002



BYLAW 1685.11

SCHEDULE "2"

OFF-STREET TRAILS (EXISTING AND PROPOSED)



**MAP 3
Parks, Trails, and Open Space**



June 2011,
Bylaw #1771, May 7, 2014
Bylaw #1954, February 17, 2021
map created by the Arlington Group for the Town of Comox
data provided by:
Town of Courtenay, Comox Valley Regional District, Province of British Columbia,
Department of Natural Resources, Canada © All rights reserved
WFS (Wildlife Time Stewardship Program) 2005
© International Community Mapping Network

BYLAW 1685.11

SCHEDULE "3"

MINOR COLLECTOR



**MAP 5
Road Network**

July, 2011
Bylaw # 1954, February 17, 2021

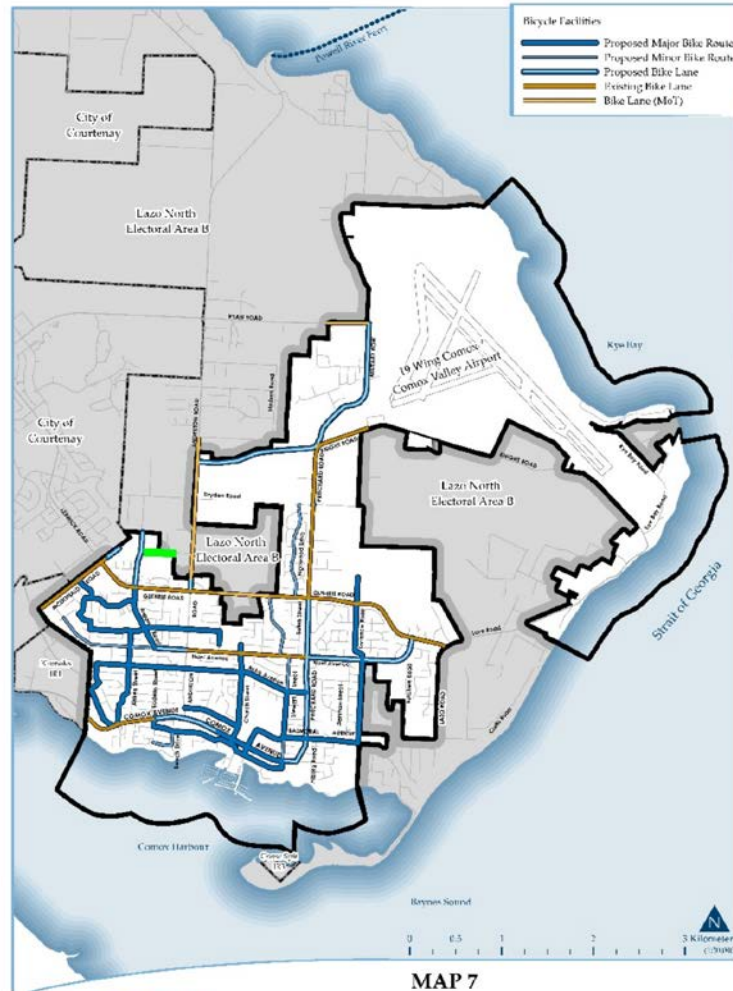
map created by the Artisan Group and Town of Comox
data provided by:
Town of Comox, Comox Valley Regional District, Province of British Columbia,
Department of Natural Resources Canada © All rights reserved.

MAP 5 - ROAD NETWORK

BYLAW 1685.11

SCHEDULE "4"

PROPOSED MAJOR BIKE ROUTE



**MAP 7
Bicycle Network**

July, 2011
Bylaw # 1954, February 17, 2021

map created by the A'Gingbo Group and Town of Comox

data provided by:
Town of Comox, Comox Valley Regional District, Province of British Columbia,
Department of Natural Resources Canada © All rights reserved

MAP 7 BICYCLE NETWORK

ATTACHMENT 3

Comox Zoning Amendment Bylaw 1850.46

**TOWN OF COMOX
BYLAW 1850.46**

A BYLAW TO AMEND COMOX ZONING BYLAW 1850

WHEREAS Council has the authority under the provisions of the *Local Government Act* to amend the Zoning Bylaw;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. Title

This bylaw may be cited for all purposes as the “Comox Zoning Amendment Bylaw 1850.46”

2. Amendments

Comox Zoning Bylaw 1850 is hereby amended as follows:

A. Section 2.7(2) Penalties is amended by adding the following:

Column 1	Column 2	Column 3
Offence	Bylaw Section	Fine Amount
Unlawful use – RM6.1 zone	211.1	\$250.00
Failure to comply with conditions – RM6.1 zone	211.2	\$250.00
Unlawful projection into setback – RM6.1 zone	211.9	\$250.00
Unlawful accessory buildings – RM6.1 zone	211.11	\$250.00
Unlawful use – RM6.2 zone	212.1	\$250.00
Failure to comply with conditions – RM6.2 zone	212.2	\$250.00
Unlawful projection into setback – RM6.2 zone	212.9	\$250.00
Unlawful projections into storey differentials – RM6.2 zone	212.11	\$250.00
Unlawful accessory buildings – RM6.2 zone	212.12	\$250.00
Unlawful use – C7.1 zone	310.1	\$250.00
Failure to comply with conditions – C7.1 zone	310.2	\$250.00
Unlawful projection into setback – C7.1 zone	310.9	\$250.00
Unlawful projections into storey differentials – C7.1 zone	310.11	\$250.00
Unlawful accessory buildings – C7.1 zone	310.12	\$250.00

B. Section 3.2, Definitions, is amended by

(1) Adding alphabetically the following text:

Greenway

A street or portion thereof that is:

- (a) 6.0 metres or more in width, restricted to pedestrian or bicycle use, or
- (b) used for stormwater retention or detention pond maintenance access.

(2) Replacing the following text:

i. Exterior Side Lot line

A lot line other than a front or rear lot line, which abuts a street; does not include a lot line which abuts a lane.

with

Exterior Side Lot line

A lot line other than a front or rear lot line, which abuts a street; does not include a lot line which abuts a lane or greenway.

ii. Exterior Side Yard

A side yard which abuts a street other than a lane

With

Exterior Side Yard

A side yard which abuts a street other than a lane or greenway.

iii. Front Lot Line

A lot line common to a parcel and a street other than a lane. Where a parcel abuts two or more streets, only the street with the shortest length along the parcel shall be used to determine front lot lines. Where a parcel does not abut a street, lot lines from which common vehicle access is provided shall be deemed to be common to a street. Notwithstanding the above, for parcels next to the sea, *front lot line* is defined as a lot line next to the sea.

With

Front Lot Line

A lot line common to a parcel and a street other than a lane or greenway. Where a parcel abuts two or more streets, only the street with the shortest length along the parcel shall be used to determine front lot lines. Where a parcel does not abut a street, lot lines from which common vehicle access is provided shall be deemed to be common to a street. Notwithstanding the above, for parcels next to the sea, *front lot line* is defined as a lot line next to the sea.

C. Section 4.1 Classification of Zones is amended by:

(1) adding the following text under the Multi-Family Residential Zones heading:

RM 6.1 Apartment;
RM 6.2 Apartment and Townhouse

(2) adding the following text under the Commercial Zones heading:

C 7.1 Commercial – Residential

D. Section 5 General regulations is amended by:

Adding as Section 5.3(5) the following text:

In RM6.1; RM6.2 and C7.1 zones, no fences are permitted within a front and exterior side yard, as shown in Figure 5-2.

E. Section 5.12 Projections into Required Setbacks is amended by adding as subsection 5(14) the following text:

In the RM6.1; RM6.2 and C7.1 zones,

- a) awnings, canopies, cornices, eaves, gutters, leaders, ornamental features, pilasters, sills, or sunshades may project up to 0.6 m into a required setback or required storey differential;
- b) decks, patios, stairs, and landings less than 0.6 m above immediately adjacent finished grade, may project up to 1.5 metres into a required setback; and
- c) balconies may project up to 1.0 metre into a required setback or required storey differential.

- F. Section 7, Class I and Class II Bicycle Parking Spaces is amended by replacing Section 7.3 with the following text:

Required Bicycle Parking Spaces for Downtown and RM6.1; RM6.2 and C7.1 zones

The following applies to all lands shown shaded in Appendix “S” and all lands zoned RM6.1; RM6.2 and C7.1:

- (1) Class I and Class II bicycle parking spaces shall be provided in accordance with the requirements in Sections 7.6(1) and 7.8(1). In the case of a use not specifically mentioned, the required Class I and Class II bicycle parking spaces shall be the same as for the most comparable use.
- (2) At the option of the owner or occupier of a parcel, structure, or part thereof, rather than provide the Class I bicycle parking spaces in accordance with Section 7.3(1), the owner or occupier may pay to the Town the sum of \$2,000.00 for each bicycle parking space not provided.
- (3) The monies referred to in Section 7.3(2) are payable in accordance with the provisions of the *Local Government Act* of British Columbia, for the purpose of providing transportation infrastructure that supports walking, bicycling, public transit or other alternative forms of transportation.
- (4) Transportation infrastructure provided under Section 7.3(3) shall not be available on a reserved or dedicated basis.

- G. Schedule “A” is amended by

- i. adding as Section 211 the RM 6.1 Apartment zone, as shown in Schedule “1A”, which is attached to and forms part of this Bylaw;
- ii. adding as Section 212 the RM 6.2 Apartment and Townhouse zone, as shown in Schedule “1B”, which is attached to and forms part of this Bylaw;
- iii. adding as Section 310 the C 7.1 Commercial – residential zone, as shown in Schedule “1C”, which is attached to and forms part of this Bylaw;

- H. Schedule “B” (the Zoning Map) is amended by rezoning

- i. The Western portion of the property legally described as **Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279** (941 Aspen West), shown shaded on **Schedule “2A”** which is attached to and forms part of this Bylaw,

from Country Residential CR-1
to RM 6.1 Apartment zone

- ii. The Eastern portion of the property legally described as **Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279** (941 Aspen East), shown shaded on **Schedule “2B”** which is attached to and forms part of this Bylaw,

from Country Residential CR-1
to C 7.1 Commercial – Residential zone; and

- iii. That property legally described **Lot A, District Lot 170, Comox District Plan 18002** (2077 Hector), shown shaded on **Schedule “2C”** which is attached to and forms part of this Bylaw,

from Country Residential CR-1
to RM 6.2 Apartment and Townhouse zone

- I. Comox Zoning Bylaw 1850 is further amended by making such consequential changes as are required to reflect the foregoing amendments, including without limitation changes in the numbering and order of the sections of the bylaw.

3. Repeal CVRD Zoning Bylaw

- (1) Section Two, subsection (1) of Comox Valley Zoning Bylaw, 2005 (as at September 19, 2016) is hereby amended by inserting, immediately after the text "This bylaw shall be applicable to Electoral Areas ‘A’, ‘B’ and ‘C’ (Comox Valley) of the Regional District of Comox-Strathcona" the following text:

with the exception of the properties legally described as
Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279;
Lot A, District Lot 170, Comox District Plan 18002

- (2) Comox Valley Zoning Bylaw, 2005 (as at September 19, 2016) is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw.

4. Repeal CVRD Floodplain Management Bylaw

- (1) Part B, Section 1, Bylaw No. 2782, being the “Floodplain Management Bylaw, 2005” (as at September 19, 2016), is hereby amended by inserting, immediately after the text “this bylaw shall be applicable to all electoral areas within the Regional District of Comox Strathcona with the exception of”:

the properties legally described as
Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279;
Lot A, District Lot 170, Comox District Plan 18002

- (2) Bylaw No. 2782, being the “Floodplain Management Bylaw, 2005” (as at September 19, 2016) is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw

3. Adoption

- | | |
|--|------------------------------------|
| (1) READ A FIRST time this | 5 th day of June , 2024 |
| (2) READ A SECOND time as amended this | 4 th day of July , 2024 |
| (3) ADVERTISED A FIRST time this | 26 th day of July, 2024 |
| (4) ADVERTISED A SECOND time this | 3 rd day of July, 2024 |
| (5) PUBLIC HEARING HELD this | 10th day of July, 2024 |
| (6) READ A THIRD time this | ___ day of _____, 2024 |
| (7) ADOPTED this | ___ day of _____, 2024 |

Mayor

Corporate Officer

BYLAW 1850.46

SCHEDULE "1A"

211. RM 6.1 APARTMENT

211.1 Permitted Uses:

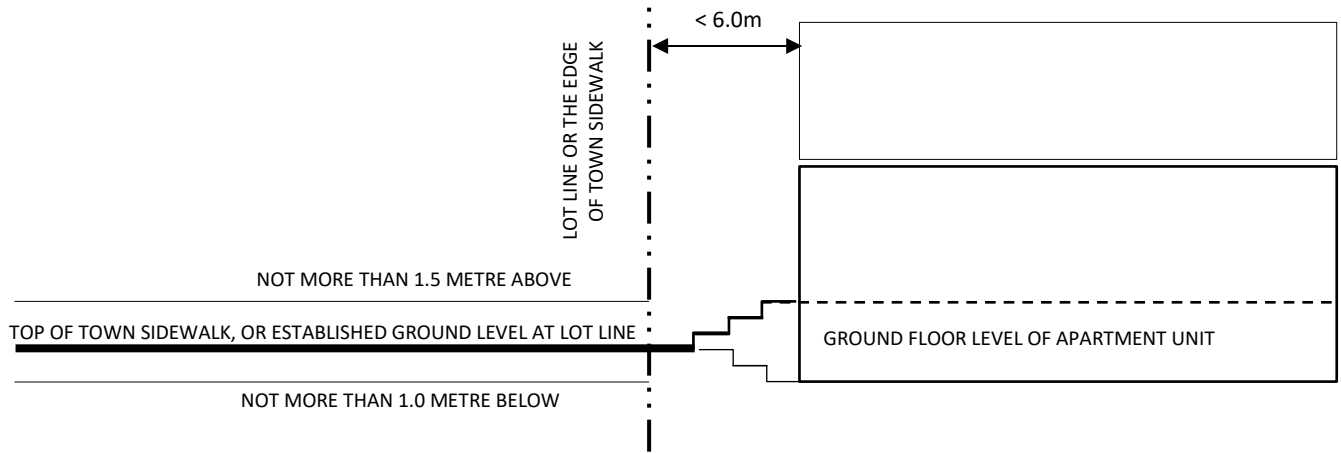
In the RM 6.1 zone, the following uses are permitted, and all other uses are prohibited:

- (1) Accessory structures and uses, excluding:
 - (i) Buildings other than those used for dwelling unit accessory or child care facility uses; and
 - (ii) Outside storage
- (2) Apartment dwellings
- (3) Child care facilities
- (4) Home occupations
- (5) Townhouse dwellings

211.2 Conditions of Use:

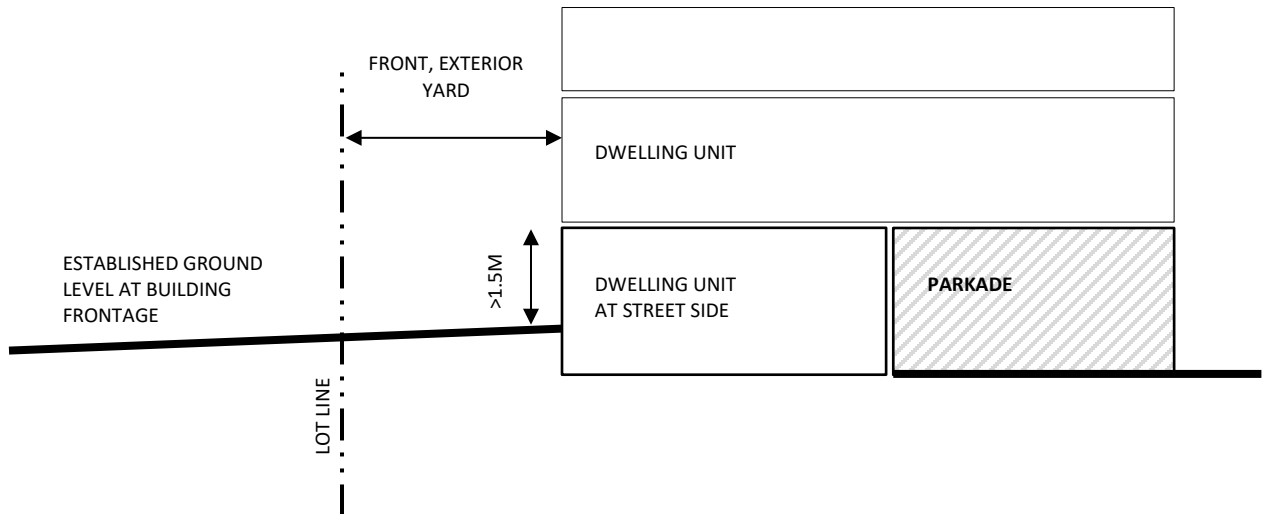
- (1) All permitted uses shall be located within a portion of a building, completely enclosed by exterior walls, except for landscape material, childcare facilities, accessory structures and accessory uses.
- (2) Child care facilities shall be located on the ground floor of an apartment building or accessory building.
- (3) Townhouse dwelling units shall:
 - (a) only be located within an apartment building;
 - (b) not be located above or below another townhouse dwelling unit; and
 - (c) not be located above a second storey of an apartment building.
- (4) Not less than 25% of all dwellings units on a parcel shall include 2 or more bedrooms. Where the calculation of 25% of dwelling units results in a fractional number, the nearest whole number shall be used.
- (5) Apartment and townhouse dwelling units shall:
 - (a) have a ground floor located not more than 1.5 metres above or 1.0 metre below the top of an adjacent Town sidewalk in accordance with Figure 211-1 when located along a front or exterior side yard, and in the absence of a Town sidewalk, as measured from established ground level at the front or exterior side lot line; and
 - (b) section 211.2(5)(a) does not apply to dwelling units located 6.0 metres or more from an edge of a Town sidewalk, and in the absence of a Town sidewalk, from a front or exterior side lot line.

Figure 211-1.



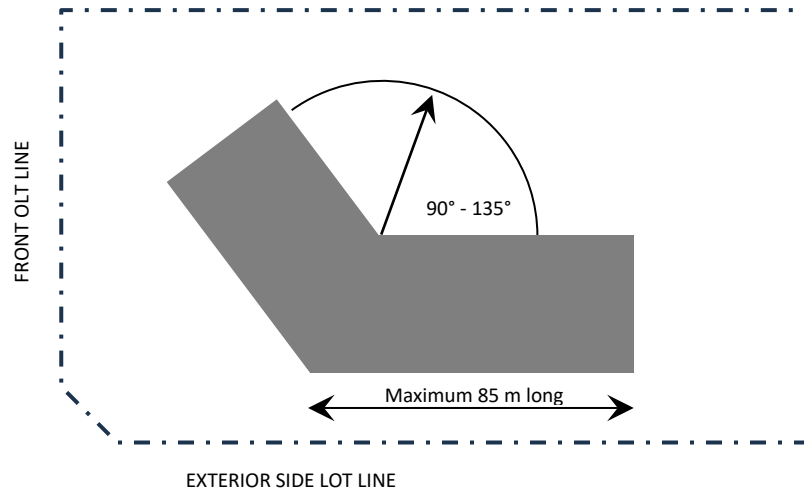
- (6) Along a front or exterior side yard, parking located within a building at or more than 1.50 metres above the established ground level shall be screened from the street by dwelling units in accordance with Figure 211-2.

Figure 211-2.



- (7) Residential building length shall:
 - (a) not exceed 85 metres in length, including projections; and
 - (b) where a directional turn of 90° to 135° degrees is provided; no section of a residential building shall exceed 85 metres in length, as shown in Figure 211-3.

Figure 211-3.



- (8) Garbage and recycling storage, and accessory uses such as dog runs and community gardens shall not be located within front or exterior side yard.

211.3 Density:

- (1) Density shall not be less than 55 units per hectare;
- (2) Density shall not exceed 80 units per hectare;
- (3) Notwithstanding 211.3(2), where not less than 40 % of total on-site parking is provided underground or within a residential building footprint, density shall not exceed 150 units per hectare; and
- (4) Notwithstanding 211.3(2), where in excess of 80 % of total on-site parking is underground parking or provided within a building footprint – density N/A.

211.4 Parcel Area:

Parcel area shall not be less than 3,500 square metres.

211.5 Parcel Frontage:

Parcel frontage shall not be less than 50.0 metres.

211.6 Parcel Depth:

Parcel depth shall not be less than 50.0 metres.

211.7 Parcel Coverage:

- (1) Parcel coverage shall not exceed 40%; and
- (2) Parcel coverage including parking areas, loading areas and driveways that are open sided and roofless shall not exceed 75%.

211.8 Height and Storeys:

Height shall not exceed:

- (1) 24.0 m for buildings up to 6 storeys;
- (2) 20.0 m for buildings up to 5 storeys;
- (3) 16.0 m for buildings up to 4 storeys; and
- (4) 12.0 m for buildings up to 3 storeys.

211.9 Required Setbacks

(1) Front

- (a) For 9.0 metres as measured parallel to and at the front setback – Front setback shall not be less than 9.0 metres in accordance with Figure 211- 4;
- (b) Additional 211.9(1)(a) setback area shall be provided for every 100 metres of front lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
- (c) All other situations – Front setback shall not be less than 3.0 metres.

(2) Rear

- (a) Rear setback shall not be less than 9.0 metres; and
- (b) Notwithstanding 211.9(2)(a), for building elevations measuring 25.0 metres or less in width along the rear yard – Rear setback shall not be less than 5.0 metres, in accordance with Figure 211- 5.

(3) Side -interior

- (a) Interior side setback shall not be less than 9.0 metres; and
- (b) Notwithstanding 211.9(3)(a), for building elevations measuring 25.0 metres or less in width along the interior side yard – Interior side setback shall not be less than 5.0 metres, in accordance with Figure 211- 5.

(4) Side - exterior

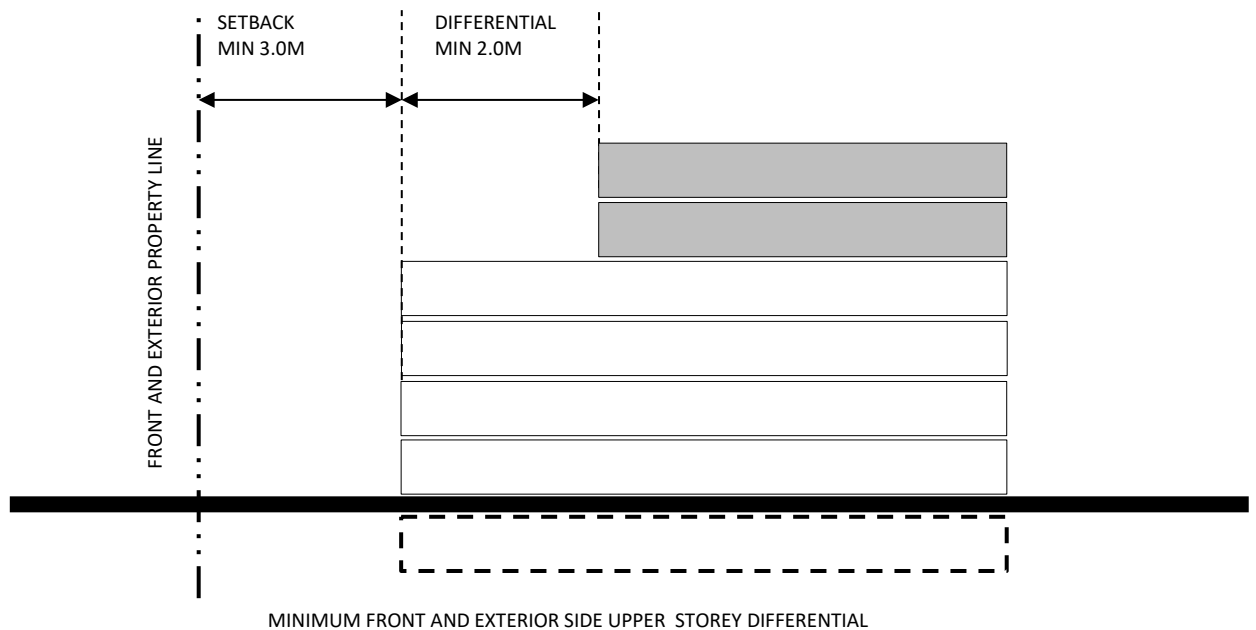
- (a) For 9.0 metres as measured parallel to and at the exterior side setback – Exterior side setback shall not be less than 9.0 metres in accordance with Figure 211- 4;
- (b) Additional 211.9(4)(a) setback area shall be provided for every 100 metres of exterior side lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
- (c) All other situations – Exterior side setback shall not be less than 3.0 metres.

- (6) Notwithstanding section 211.9, building setback from any lot lines shared with the Agricultural Land Reserve shall be not less than 15.0 metres.

211.10 Required Storey Differentials

For buildings over 4 storeys in height, along a front and exterior side elevations, at the 5th and 6th storeys, a minimum differential setback of 2.0 metres shall be provided from the storeys below, in accordance with Figure 211 – 7

Figure 211-7.



211.11 Accessory Buildings and Structures

Accessory Buildings shall:

- (1) Not be located within front or exterior side yard;
- (2) have a parcel coverage not exceeding 5%;
- (3) Not exceed 9.0 metres and 2 storeys in height, for accessory buildings used for multi-family amenity and recreation; and
- (4) Not exceed 4.5 metres in height, for other accessory buildings.

211.12 Screening

- (1) Along a front or exterior side yard, parking located within a building more than 0.6 metres but less than 1.50 metres above finished grade at building frontage shall be screened from streets by vegetation and landscaping.
- (2) The following shall be screened in accordance with Section 8:
 - (a) Off-street parking and loading areas;
 - (b) Above ground utility boxes and utility transformers;
 - (c) Garbage or recycling compounds and collection areas, unless enclosed in a building;
 - (d) RM6.1 zoned parcels from abutting Residential zoned parcels; and
 - (e) Parcels abutting land within the Agricultural Land Reserve, in accordance with specifications in Appendix B1.

211.13 Off-Street Vehicle Parking and Loading

- (1) Off-street vehicles parking and loading shall be provided in accordance with Section 6;
- (2) No more than one driveway per street frontage shall be permitted on a parcel;
- (3) Internal driveways shall not exceed 6.0 metres in width.

211.14 Off-Street Bicycle Parking

- (1) Off-street parking for bicycles shall be provided in accordance with Section 7; and
- (2) Notwithstanding Section 7, Class II bicycle rooms may be provided within individual storage units located on either the ground level or underground parkade level of an apartment building with direct access to the outdoors.

211.15 Other Requirements:

- (1) Overhead wiring shall not be permitted on a parcel. All new services on a parcel shall be placed underground.
- (2) Unoccupied open spaces shall be fully and suitably landscaped with landscape material; this does not include environmentally sensitive areas and required buffers where native planting is to be left undisturbed, including watercourse setbacks as specified in Section 5.19.
- (3) All buildings shall conform to Section 5.19, Watercourse Regulations.

BYLAW 1850.46

SCHEDULE "1B"

212. RM 6.2 APARTMENT AND TOWNHOUSE

212.1 Permitted Uses:

In the RM 6.2 zone, the following uses are permitted, and all other uses are prohibited:

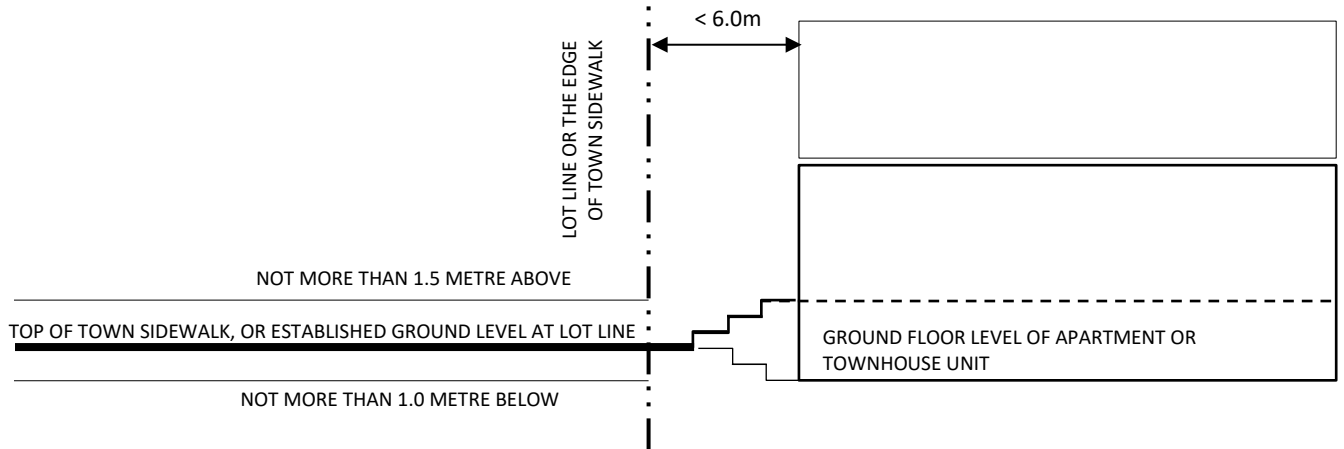
- (1) Accessory structures and uses, excluding:
 - (i) Buildings other than those used for dwelling unit accessory or child care facility uses; and
 - (ii) Outside storage
- (2) Apartment dwellings
- (3) Child care facilities
- (4) Home occupations
- (5) Townhouse dwellings
- (6) Two-family dwellings

212.2 Conditions of Use:

- (1) All permitted uses shall be located within a portion of a building, completely enclosed by exterior walls, except for landscape material, childcare facilities, accessory structures and accessory uses.
- (2) Child care facilities shall be located on the ground floor of an apartment building or accessory building.
- (3) The combined number of two-family and townhouse dwelling units on a parcel shall not exceed 20% of all dwelling units on the parcel. Where the calculation of 20% dwelling units results in a fractional number, the nearest whole number shall be used.
- (4) Not less than 25% of all dwellings units on a parcel shall include 2 or more bedrooms. Where the calculation of 25% of dwelling units results in a fractional number, the nearest whole number shall be used.
- (5) Townhouse dwelling units may be located within an apartment building;
- (6) Townhouse dwelling units located within an apartment building shall:
 - (a) not be located above or below another townhouse dwelling unit; and
 - (b) not be located above a second storey of an apartment building.
- (7) Apartment, townhouse and two-family dwelling units shall:
 - (a) have a ground floor located not more than 1.5 metres above or 1.0 metre below the top of an adjacent Town sidewalk in accordance with Figure 212-1 when located along a front or exterior side yard, and in the absence of a Town sidewalk, as measured from established ground level at the front or exterior side lot line; and

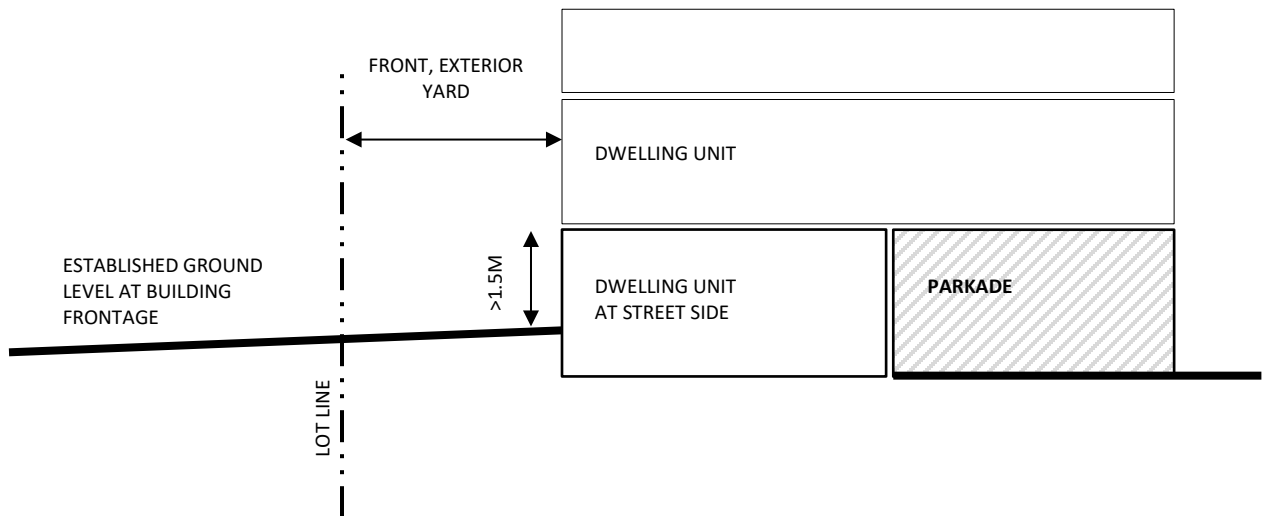
- (b) section 212.2(7)(a) does not apply to dwelling units located 6.0 metres or more from an edge of a Town sidewalk, and in the absence of a Town sidewalk, from a front or exterior side lot line.

Figure 212-1.



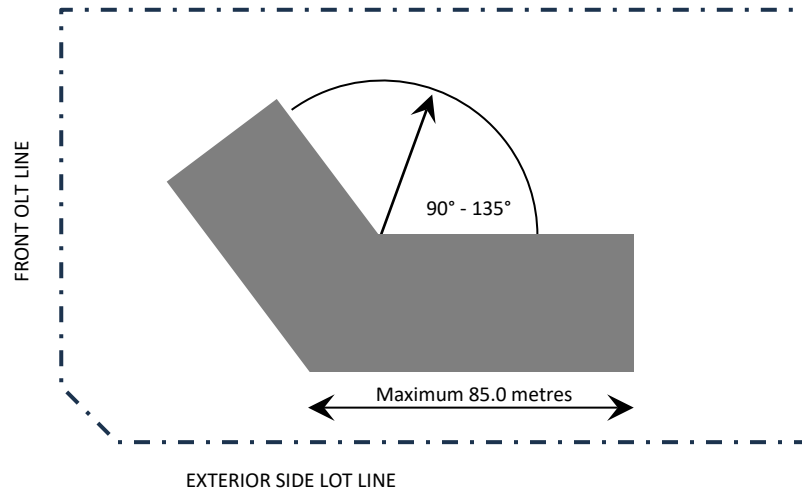
- (8) Along a front or exterior side yard, parking located within a building at or more than 1.50 metres above the established ground level shall be screened from the street by dwelling units in accordance with Figure 212-2.

Figure 212-2.



- (9) Residential building length shall:
- (a) not exceed 85.0 metres in length, including projections; and
 - (b) where a directional turn of 90° to 135° degrees is provided; no section of a residential building shall exceed 85.0 metres in length, as shown in Figure 212-3.

Figure 212-3.



- (10) Garbage and recycling storage, and accessory uses such as dog runs and community gardens shall not be located within front or exterior side yard.

212.3 Density:

- (1) Density shall not be less than 55 units per hectare;
- (2) Density shall not exceed 80 units per hectare;
- (3) Notwithstanding 212.3(2), where not less than 40 % of total on-site parking is provided underground or within a residential building footprint, density shall not exceed 150 units per hectare; and
- (4) Notwithstanding 212.3(2), where in excess of 80 % of total on-site parking is underground parking or provided within a building footprint – density N/A.

212.4 Parcel Area:

Parcel area shall not be less than 3,500 square metres.

212.5 Parcel Frontage:

Parcel frontage shall not be less than 50.0 metres.

212.6 Parcel Depth:

Parcel depth shall not be less than 50.0 metres.

211.8 Parcel Coverage:

- (1) Parcel coverage shall not exceed 40%; and
- (2) Parcel coverage including parking areas, loading areas and driveways that are open sided and roofless shall not exceed 75%.

211.9 Height and Storeys:

- (1) Apartment building height shall not exceed:
 - (a) 24.0 m for buildings up to 6 storeys;
 - (b) 20.0 m for buildings up to 5 storeys;
 - (c) 16.0 m for buildings up to 4 storeys; and
 - (d) 12.0 m for buildings up to 3 storeys.
- (2) Townhouse and two-family dwelling building height shall not exceed 12.0 metres and 3 storeys.

212.9 Required Setbacks

- (1) Front
 - (d) For 9.0 metres as measured parallel to and at the front setback – Front setback shall not be less than 9.0 metres in accordance with Figure 212- 4;
 - (e) Additional 212.9(1)(a) setback area shall be provided for every 100 metres of front lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
 - (f) All other situations – Front setback shall not be less than 3.0 metres.
- (2) Rear
 - (c) Rear setback shall not be less than 9.0 metres; and
 - (d) Notwithstanding 212.9(2)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the rear yard – Rear setback shall not be less than 5.0 metres, in accordance with Figure 212- 5.
- (3) Side-interior
 - (c) Interior side setback shall not be less than 9.0 metres; and
 - (d) Notwithstanding 212.9(3)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the interior side yard – Interior side setback shall not be less than 5.0 metres, in accordance with Figure 212- 5.
- (4) Side- exterior
 - (d) For 9.0 metres as measured parallel to and at the exterior side setback – Exterior side setback shall not be less than 9.0 metres in accordance with Figure 212- 4;
 - (e) Additional 212.9(4)(a) setback area shall be provided for every 100 metres of exterior side lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and

minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;

- (f) All other situations – Exterior side setback shall not be less than 3.0 metres.

Figure 212-4.

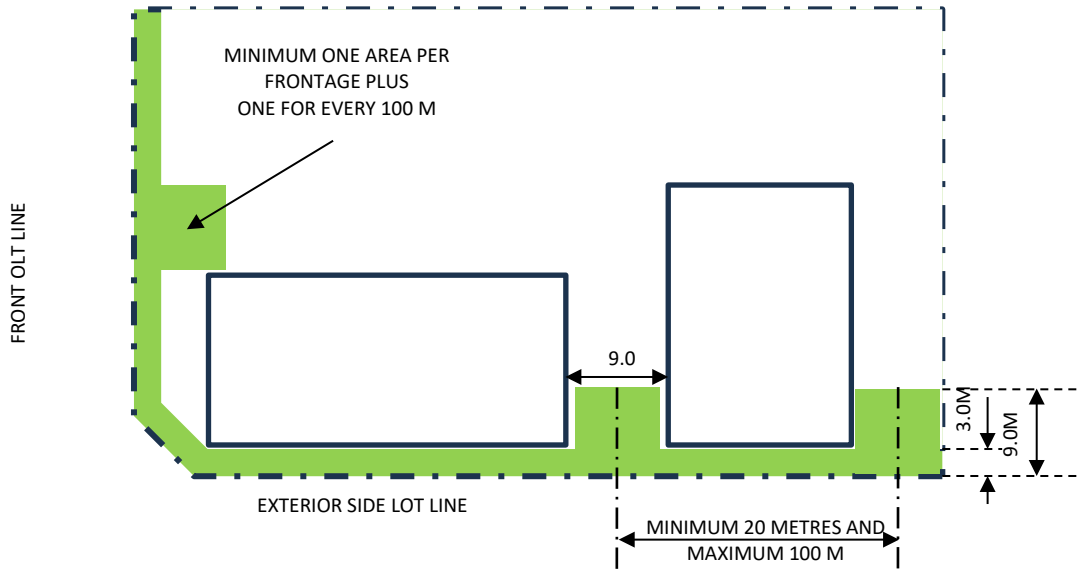
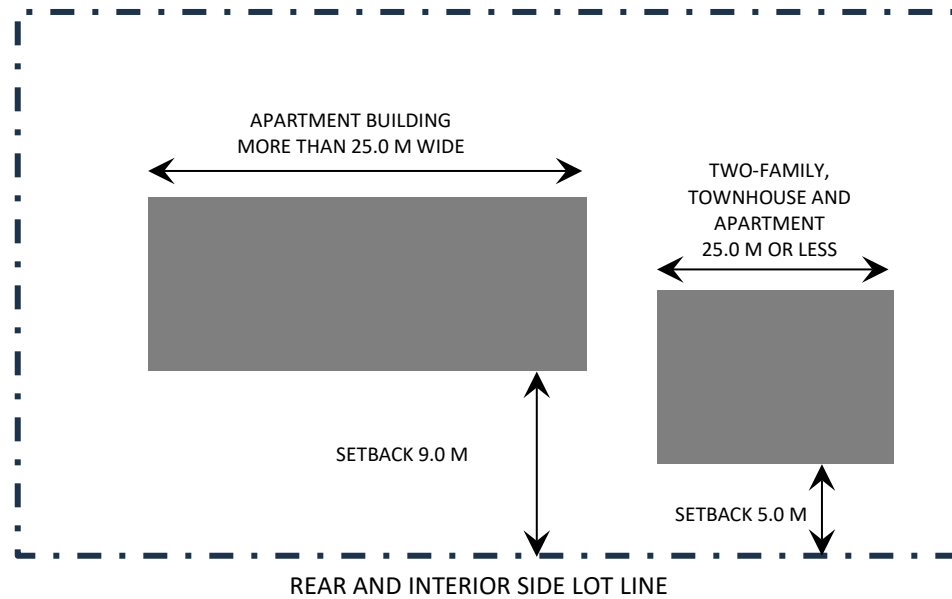


Figure 212-5

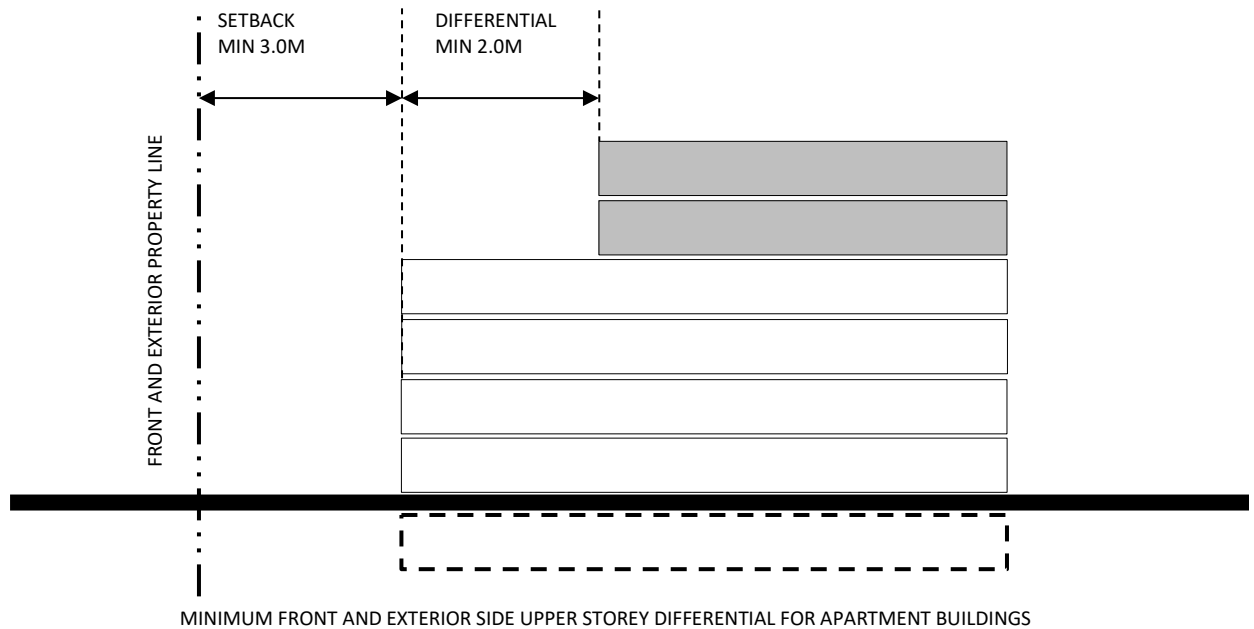


- (5) Notwithstanding sections 212.9 (1) to (4), where a parcel abuts a greenway that is not less than 10.0 metres in width, the setback from a lot line abutting a greenway shall be not less than 5.0 metres; and
- (6) Notwithstanding sections 212.9, building setback from any lot lines shared with the Agricultural Land Reserve shall be not less than 15.0 metres.

212.11 Required Storey Differentials

For apartment buildings over 4 storeys in height, along front and exterior side elevations at the 5th and 6th storeys, a minimum differential setback of 2.0 metres shall be provided from the storeys below, in accordance with Figure 212 –6.

Figure 212-6.



212.12 Accessory Buildings and Structures

Accessory Buildings shall:

- (1) Not be located within front or exterior side yard;
- (2) Have a parcel coverage not exceeding 5%;
- (3) Not exceed 9.0 metres and 2 storeys in height, for accessory buildings used for multi-family amenity and recreation;
- (4) Not exceed 4.5 metres in height, for other accessory buildings;
- (5) Notwithstanding section 21.12(4) accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit: not exceed 6.5 m² in gross floor area per dwelling unit and not exceed 2.5 metres in height; and

- (6) Accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit be excluded from required rear and interior side setbacks, provided that no accessory building is located closer than 2.0 metres to a rear or interior side lot line;

212.13 Screening

- (1) Along a front or exterior side yard, parking located within an apartment building more than 0.6 metres but less than 1.50 metres above finished grade at building frontage shall be screened from streets by vegetation and landscaping.
- (2) The following shall be screened in accordance with Section 8:
 - (f) Off-street parking and loading areas;
 - (g) Above ground utility boxes and utility transformers;
 - (h) Garbage or recycling compounds and collection areas, unless enclosed in a building;
 - (i) RM6.2 zoned parcels from abutting Residential zoned parcels; and
 - (j) Parcels abutting land within the Agricultural Land Reserve, in accordance with specifications in Appendix B1.

212.14 Off-Street Vehicle Parking and Loading

- (1) Off-street vehicles parking and loading shall be provided in accordance with Section 6;
- (2) No more than one driveway per street frontage shall be permitted on a parcel;
- (3) Internal driveways shall not exceed 6.0 metres in width.

212.15 Off-Street Bicycle Parking

- (1) Off-street parking for bicycles shall be provided in accordance with Section 7; and
- (2) Notwithstanding Section 7, Class II bicycle rooms may be provided within individual storage units located on either the ground level or underground parkade level of an apartment building with direct access to the outdoors.

212.15 Other Requirements:

- (1) Overhead wiring shall not be permitted on a parcel. All new services on a parcel shall be placed underground.
- (2) Unoccupied open spaces shall be fully and suitably landscaped with landscape material; this does not include environmentally sensitive areas and required buffers where native planting is to be left undisturbed, including watercourse setbacks as specified in Section 5.19.
- (3) All buildings shall conform to Section 5.19, Watercourse Regulations.

BYLAW 1850.46

SCHEDULE "1C"

310. C 7.1 COMMERCIAL RESIDENTIAL

310.1 Permitted Uses:

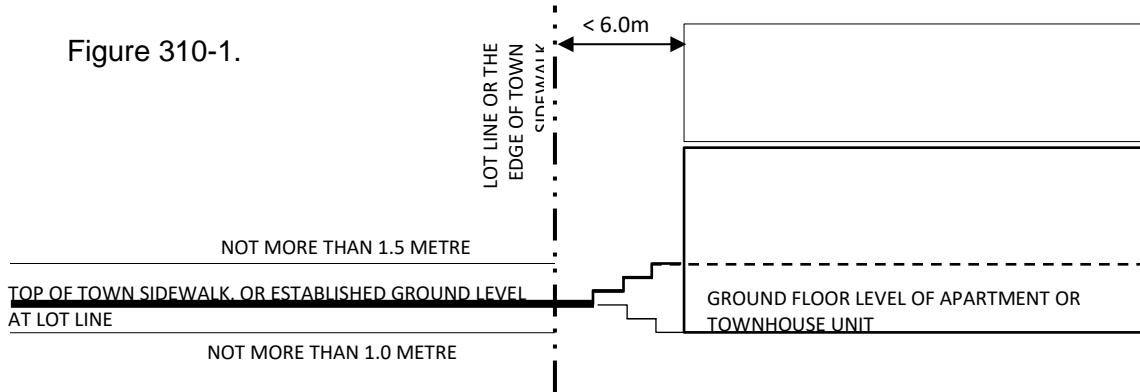
In the C 7.1 zone, the following uses are permitted, and all other uses are prohibited:

- (1) Accessory structures and uses, excluding:
 - (i) Buildings other than those used for dwelling unit accessory or child care facility uses; and
 - (ii) Outside storage
- (2) Apartment dwellings
- (3) Townhouse dwellings
- (4) Two-family dwellings
- (5) Following Commercial Uses
 - (a) Artist studios
 - (b) Art galleries
 - (c) Child care facilities
 - (d) Dental clinics
 - (e) Denturist labs
 - (f) Home occupations
 - (g) Libraries
 - (h) Locksmiths
 - (i) Medical clinics
 - (j) Offices
 - (k) Personal service establishments
 - (l) Pet grooming
 - (m) Restaurants
 - (n) Restaurants – coffee shops
 - (o) Retail stores, which have a gross floor area not in excess of 120 square metres
 - (p) Small appliance repair shops, processing or packaging of food or beverage products, or establishments that repair or assemble electronic equipment which:
 - (i) have a total non-retail floor area not in excess of 120 square metres,
 - (ii) retail directly from the premises, and
 - (iii) have the retail area extending the full width of the premises and located adjacent to the primary pedestrian entrance.
 - (q) Tailor shops, which:
 - (i) have a gross floor area not in excess of 120 square metres,
 - (ii) retail directly from the premises, and
 - (iii) have the retail area extending the full width of the width of the premises and located adjacent to the primary pedestrian entrance
 - (r) Veterinary Clinic – Small Animal Practice, excluding the boarding or kenneling of animals.

310.2 Conditions of Use:

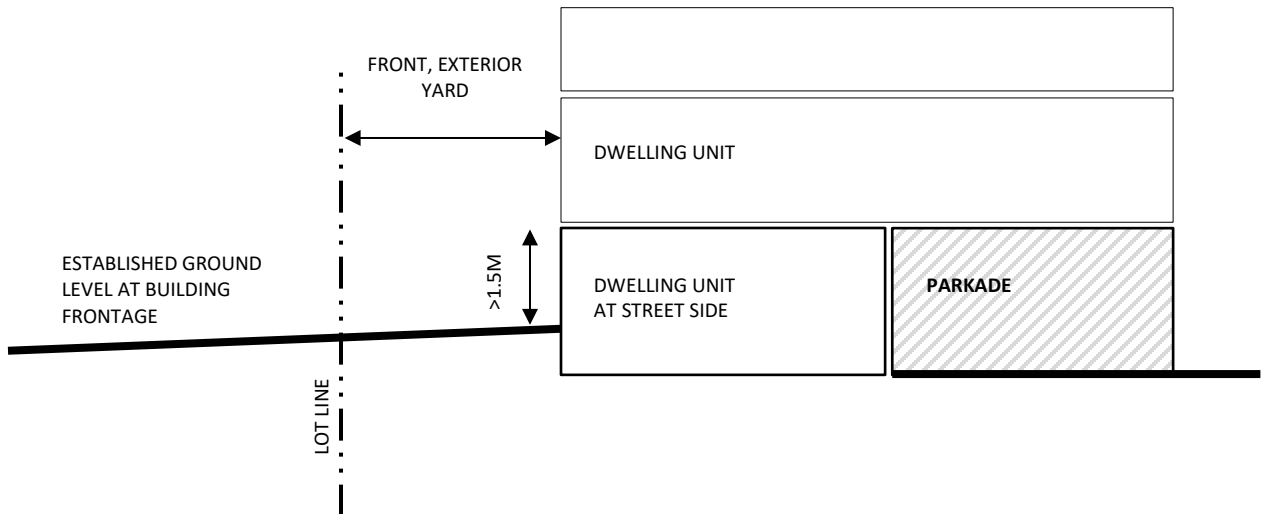
- (1) All permitted uses shall be located within a portion of a building, completely enclosed by exterior walls, except for landscape material, child care facilities, restaurants, accessory structures and accessory uses.
- (2) Child care facilities shall be located on the ground floor of an apartment building or accessory building.
- (3) Permitted uses other than dwelling units, dwelling unit accessory uses, and child care facilities shall
 - (a) be located within an apartment building, and
 - (b) not be located above the ground floor.
- (4) For a distance of not less than 60.0 m measured parallel to a road list in section 310.2(3)(a), permitted use shall be limited to apartment buildings with ground floor commercial use for a depth of not less than 7.5 m, measured perpendicularly from the exterior of the building elevation facing the road:
 - (a) Aspen Road.
- (5) Permitted uses other than dwelling units and child care facilities shall:
 - (a) Have a ground floor located not more than 0.50 metres above and not more than 0.50 metres below the top of adjacent Town sidewalk; and
 - (b) Section 310.2(5)(a) does not apply to commercial units located not less than 6.0 metres from a front and exterior side lot line.
- (6) Townhouse dwelling units may be located within an apartment building;
- (7) Townhouse dwelling units located within an apartment building shall:
 - (c) not be located above or below another townhouse dwelling unit; and
 - (d) not be located above a second storey of an apartment building.
- (8) The combined number of two-family and townhouse dwelling units on a parcel shall not exceed 20% of all dwelling units on the parcel. Where the calculation of 20% dwelling units results in a fractional number, the nearest whole number shall be used.
- (8) Not less than 25% of all dwellings units on a parcel shall include 2 or more bedrooms. Where the calculation of 25% of dwelling units results in a fractional number, the nearest whole number shall be used.
- (9) Apartment, townhouse and two-family dwelling units shall:
 - (c) Have a ground floor located not more than 1.5 metres above or 1.0 metre below the top of an adjacent Town sidewalk in accordance with Figure 310-1 when located along a front or exterior side yard, and in the absence of a Town sidewalk, as measured from established ground level at the front or exterior side lot line; and
 - (d) Section 310.2(9)(a) does not apply to dwelling units located 6.0 metres or more from an edge of a Town sidewalk, and in the absence of a Town sidewalk, from a front or exterior side lot line.

Figure 310-1.



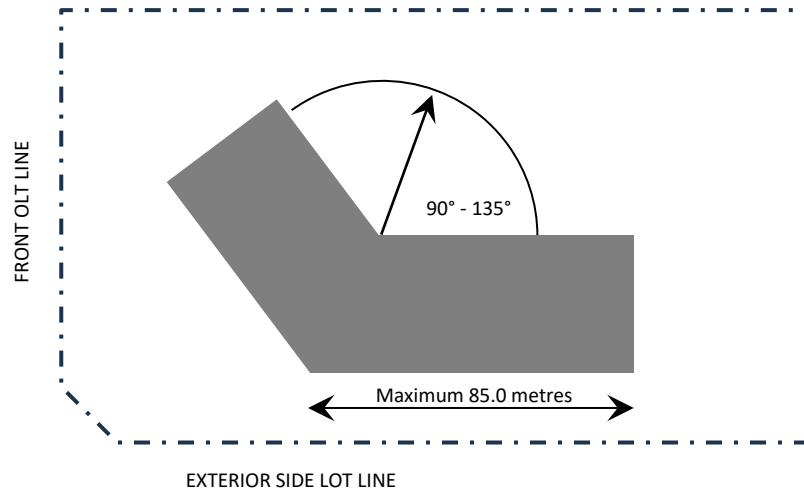
- (10) Along a front or exterior side yard, parking located within an apartment building at or more than 1.50 metres above the established ground level shall be screened from the street by dwelling units in accordance with Figure 310-2.

Figure 310-2.



- (11) Residential building length shall:
 - (c) not exceed 85.0 metres in length, including projections; and
 - (d) where a directional turn of 90° to 135° degrees is provided; no section of a residential building shall exceed 85.0 metres in length, as shown in Figure 310-3.

Figure 310-3.



- (12) Garbage and recycling storage, and accessory uses such as dog runs and community gardens shall not be located within front or exterior side yard.

310.3 Density:

- (1) Density shall not be less than 55 units per hectare;
- (2) Density shall not exceed 80 units per hectare;
- (3) Notwithstanding 310.3(2), where not less than 40 % of total on-site parking is provided underground or within a residential building footprint, density shall not exceed 150 units per hectare; and
- (4) Notwithstanding 310.3(2), where in excess of 80 % of total on-site parking is underground parking or provided within a building footprint – density N/A.

310.4 Parcel Area:

Parcel area shall not be less than 3,500 square metres.

310.5 Parcel Frontage:

Parcel frontage shall not be less than 50.0 metres.

310.6 Parcel Depth:

Parcel depth shall not be less than 50.0 metres.

310.7 Parcel Coverage:

- (1) Parcel coverage shall not exceed 40%; and
- (2) Parcel coverage including parking areas, loading areas and driveways that are open sided and roofless shall not exceed 75%.

310.8 Height and Storeys:

- (1) Apartment building height shall not exceed:
 - (e) 24.0 m for buildings up to 6 storeys;
 - (f) 20.0 m for buildings up to 5 storeys;
 - (g) 16.0 m for buildings up to 4 storeys; and
 - (h) 12.0 m for buildings up to 3 storeys.
- (2) Townhouse and two-family dwelling building height shall not exceed 12.0 metres and 3 storeys.

310.9 Required Setbacks

- (1) Front
 - (g) For 9.0 metres as measured parallel to and at the front setback – Front setback shall not be less than 9.0 metres in accordance with Figure 310- 4;
 - (h) Additional 310.9(1)(a) setback area shall be provided for every 100 metres of front lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
 - (i) All other situations – Front setback shall not be less than 3.0 metres.
- (2) Rear
 - (e) Rear setback shall not be less than 9.0 metres; and
 - (f) Notwithstanding 310.9(2)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the rear yard – Rear setback shall not be less than 5.0 metres, in accordance with Figure 310- 5.
- (3) Side-interior
 - (e) Interior side setback shall not be less than 9.0 metres; and
 - (f) Notwithstanding 310.9(3)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the interior side yard – Interior side setback shall not be less than 5.0 metres, in accordance with Figure 310- 5.
- (4) Side- exterior
 - (g) For 9.0 metres as measured parallel to and at the exterior side setback – Exterior side setback shall not be less than 9.0 metres in accordance with Figure 310- 4;
 - (h) Additional 310.9(4)(a) setback area shall be provided for every 100 metres of exterior side lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
 - (i) All other situations – Exterior side setback shall not be less than 3.0 metres.

Figure 310-4.

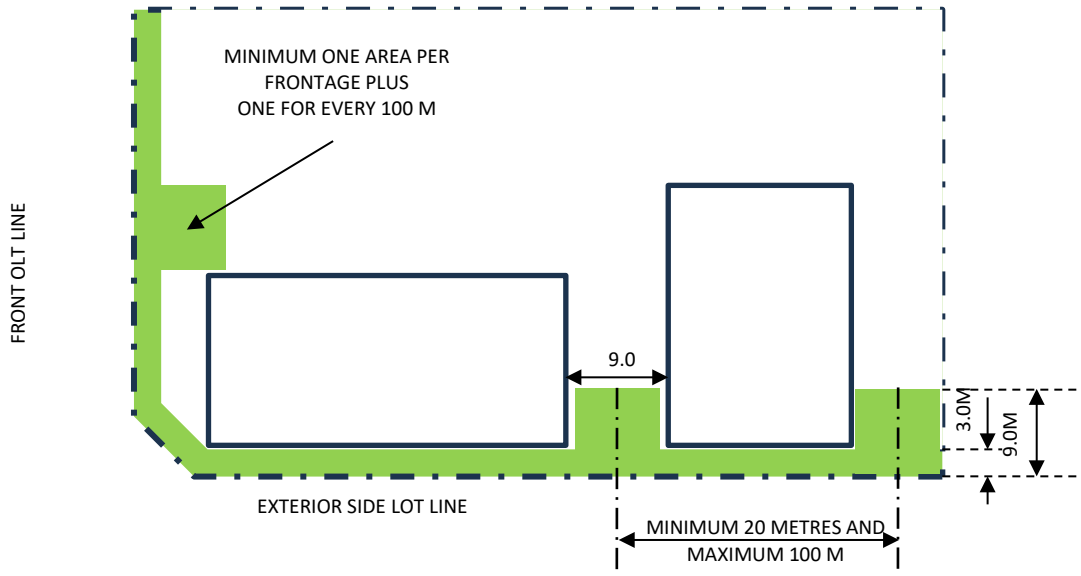
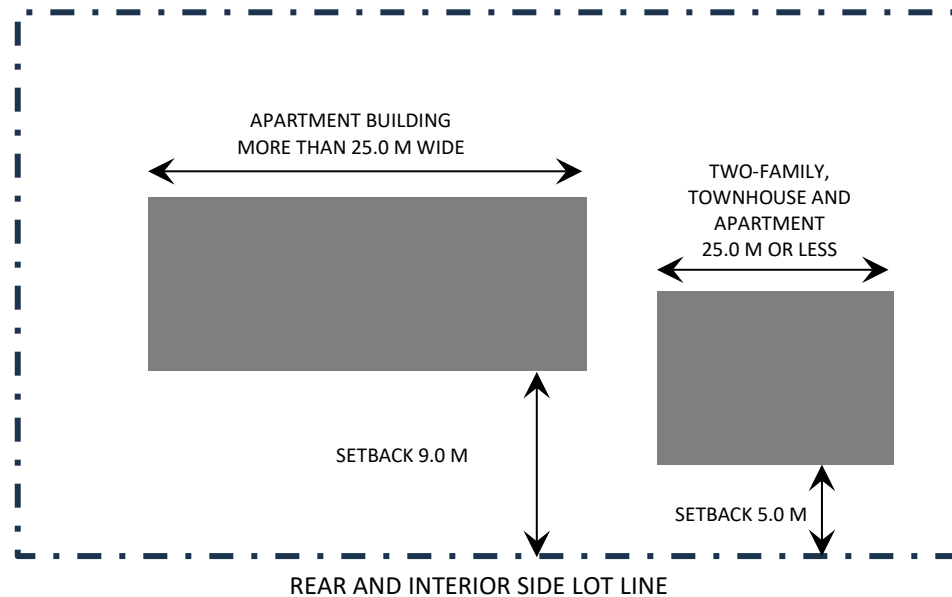


Figure 310-5

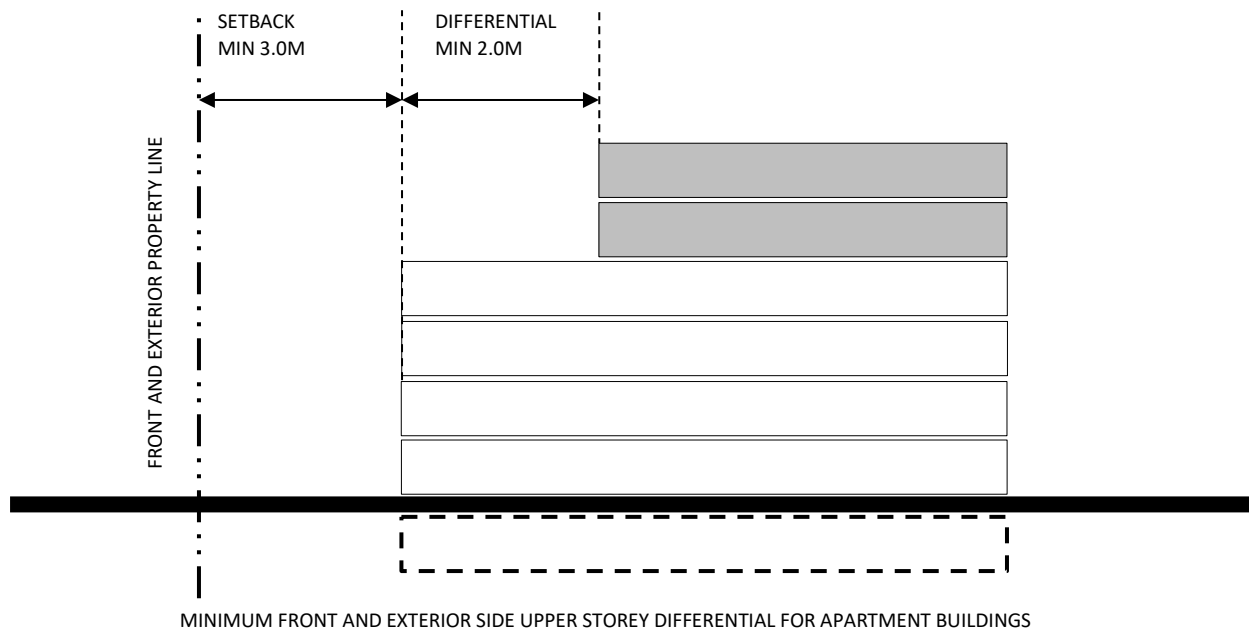


- (5) Notwithstanding sections 310.9 (1) to (4), where parcel abuts a greenway that is not less than 10.0 metres in width, the setback from a lot line abutting a greenway shall be not less than 5.0 metres; and
- (6) Notwithstanding sections 310.9, building setback from any lot lines shared with the Agricultural Land Reserve shall be not less than 15.0 metres.

310.10 Required Storey Differentials

For apartment buildings over 4 storeys in height, along front and exterior side elevations at the 5th and 6th storeys, a minimum differential setback of 2.0 metres shall be provided from the storeys below, in accordance with Figure 310 – 6.

Figure 310-6.



310.11 Accessory Buildings and Structures

Accessory Buildings shall:

- (1) Not be located within front or exterior side yard;
- (2) Have a parcel coverage not exceeding 5%;
- (3) Not exceed 9.0 metres and 2 storeys in height, for accessory buildings used for multi-family amenity and recreation;
- (4) Not exceed 4.5 metres in height, for other accessory buildings;
- (5) Notwithstanding section 21.12(4) accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit: not exceed 6.5 m² in gross floor area per dwelling unit and not exceed 2.5 metres in height; and
- (6) Accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit be excluded from required rear and interior side setbacks, provided that no accessory building is located closer than 2.0 metres to a rear or interior side lot line;

310.12 Screening

- (1) Along a front or exterior side yard, parking located within an apartment building more than 0.6 metres but less than 1.50 metres above finished grade at building frontage shall be screened from streets by vegetation and landscaping.
- (2) The following shall be screened in accordance with Section 8:
 - (k) Off-street parking and loading areas;
 - (l) Above ground utility boxes and utility transformers;
 - (m) Garbage or recycling compounds and collection areas, unless enclosed in a building;
 - (n) C 7.1 zoned parcels from abutting Residential zoned parcels; and
 - (o) Parcels abutting land within the Agricultural Land Reserve, in accordance with specifications in Appendix B1.

310.13 Off-Street Vehicle Parking and Loading

- (1) Off-street vehicles parking and loading shall be provided in accordance with Section 6;
- (2) No more than one driveway per street frontage shall be permitted on a parcel;
- (3) Internal driveways shall not exceed 6.0 metres in width.

310.14 Off-Street Bicycle Parking

- (1) Off-street parking for bicycles shall be provided in accordance with Section 7; and
- (2) Notwithstanding Section 7, Class II bicycle rooms may be provided within individual storage units located on either the ground level or underground parkade level of an apartment building with direct access to the outdoors.

310.15 Other Requirements:

- (1) Overhead wiring shall not be permitted on a parcel. All new services on a parcel shall be placed underground.
- (2) Unoccupied open spaces shall be fully and suitably landscaped with landscape material; this does not include environmentally sensitive areas and required buffers where native planting is to be left undisturbed, including watercourse setbacks as specified in Section 5.19.
- (3) All buildings shall conform to Section 5.19, Watercourse Regulations.

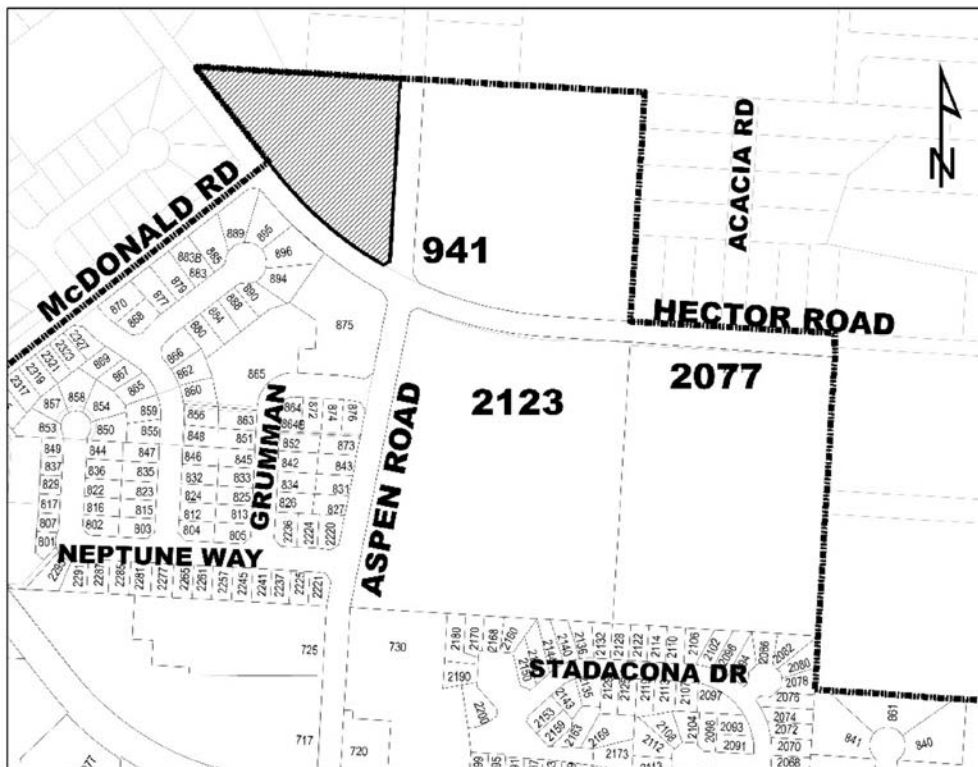
BYLAW 1850.46

SCHEDULE "2A"

941 Aspen Road (Aspen-West)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



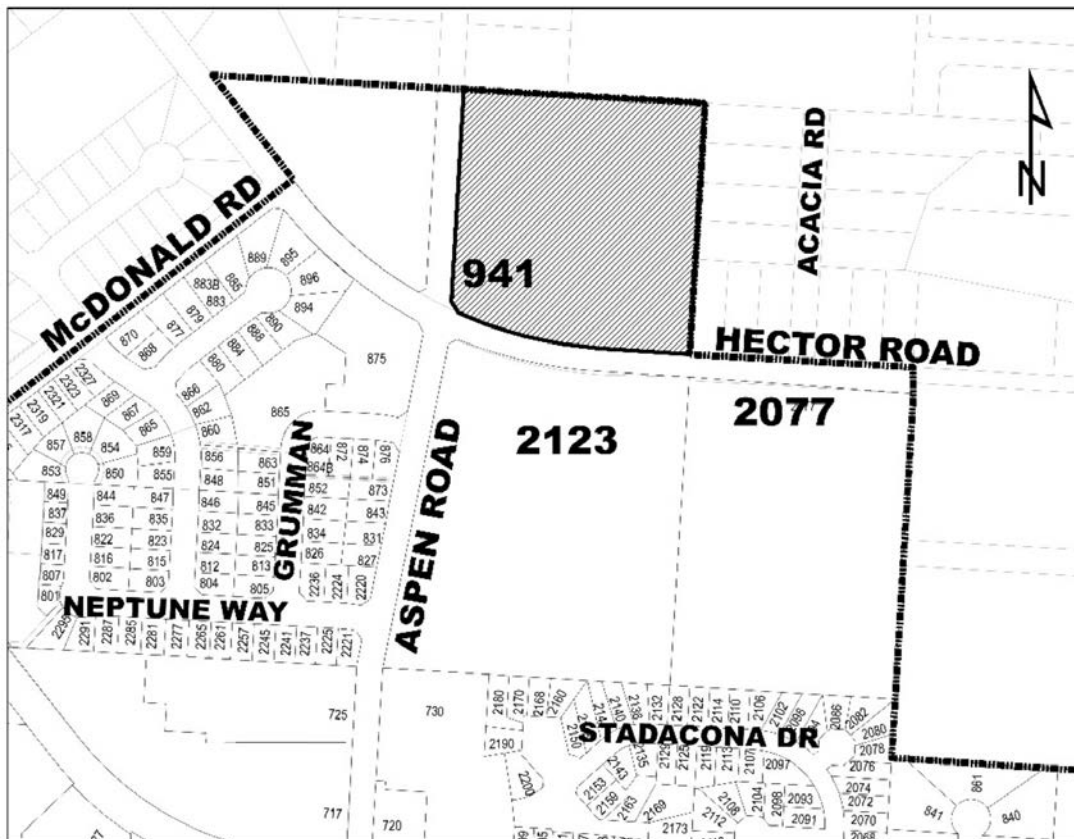
BYLAW 1850.46

SCHEDULE "2B"

941 Aspen Road (Aspen-East)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



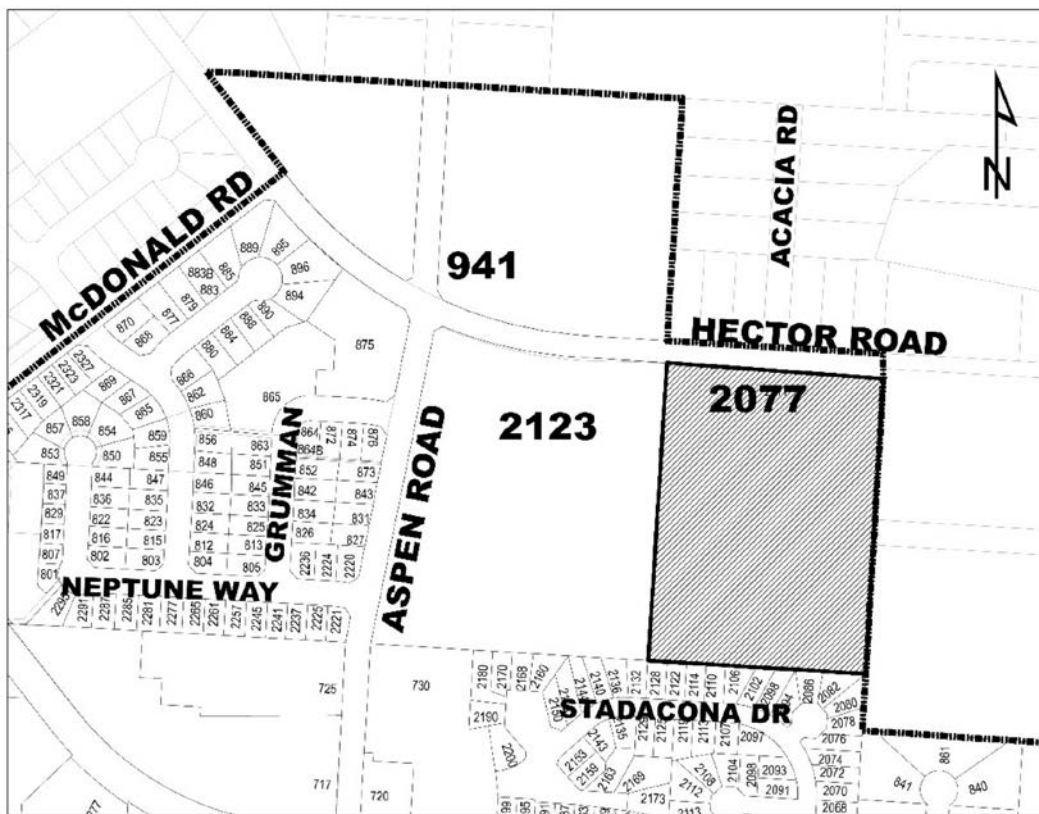
BYLAW 1850.46

SCHEDULE "2C"

2077 Hector Road

PID 003-856-704

Lot A, DL 170, Comox District, Plan 18002



Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18

TOWN OF COMOX

BYLAW 1261.18

A BYLAW TO AMEND TOWN OF COMOX SUBDIVISION AND DEVELOPMENT SERVICING BYLAW, 1261

WHEREAS Council has adopted a Subdivision And Servicing Bylaw and has the authority under the provisions of the *Community Charter* to amend the Subdivision And Development Servicing Bylaw, 1261;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. TITLE

This Bylaw may be cited for all purposes as "Comox Subdivision And Development Servicing Amendment Bylaw 1261.18".

2. AMENDMENTS

(1) Town of Comox Subdivision And Development Servicing Bylaw, 1261 is amended as follows:

(a) By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 1.4, the following text:

Notwithstanding Section 1.1 above, for sections of Aspen Road abutting parcels legally described Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279 (941 Aspen) and Lot 4 District Lot 170 Comox District Plan VIP60685 (2123 Hector) the minimum widths of rights-of-way and pavement and the requirements for sidewalks and curbs shall be those shown in Standard Drawing SC-17.

(b) By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 1.5, the following text:

Notwithstanding Section 1.1 above, for sections of Hector Road abutting Lot A, District Lot 170, Comox District Plan 18002 (2077 Hector) and Lot 4 District Lot 170 Comox District Plan VIP60685 (2123 Hector) the minimum widths of rights-of-way and pavement and the requirements for sidewalks and curbs shall be those shown in Standard Drawing SC-18.

- (c) By adding to Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, as Standard Drawing SC-17 figure in Schedule "1", which is attached to and forms part of this bylaw.

- (d) By adding to Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, as Standard Drawing SC-18 figure in Schedule "2", which is attached to and forms part of this bylaw .

- (e) By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 8.3, the following text and figure:

Hector Greenway section shown in black bold line in Figure C-3 below shall:

- (i) Be not less than 10.0 metres wide;
- (ii) Align with south side of Hector road dedication and connect to 3.0 metres wide stormwater facility maintenance roads;
- (iii) Be constructed to the following specifications:
 - minimum 3.0 metres wide multi-use asphalt path aligned so as to not be in conflict with significant trees;
 - minimum 10 significant trees for every 50 metres of greenway length, retained existing or replanted in groups to create groves;
 - irrigation 1.5" water meter on Town property with separate zones for turf and tree drip.

Figure C-3



- (f) By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 8.4, the following text and figure:

Neighbourhood trail shown in black dotted line in Figure C-4 below shall:

- (i) Be not less than 10.0 metres wide;
- (ii) Align approximately East-West so as to not be in conflict with wetlands and environmental areas, and connect to 3.0 metres wide stormwater facility maintenance roads;
- (iii) Be constructed to the following specifications:
 - minimum 4.0 metres wide multi-use asphalt path aligned so as to not be in conflict with significant trees;
 - minimum 10 significant trees for every 50 metres of greenway length, retained existing or replanted in groups to create groves;
 - irrigation 1.5" water meter on Town property with separate zones for turf and tree drip.

Figure C-4



- (2) "Town of Comox Subdivision And Development Servicing Bylaw, 1261" is hereby further amended by making such consequential changes as are required to reflect the foregoing amendments, including without limitation changes in the numbering and order of the sections of this bylaw.

3. DEFINITIONS

(1) In this Bylaw, unless the context otherwise requires

- (a) "Council" means the Council of the Town of Comox;

4. Adoption

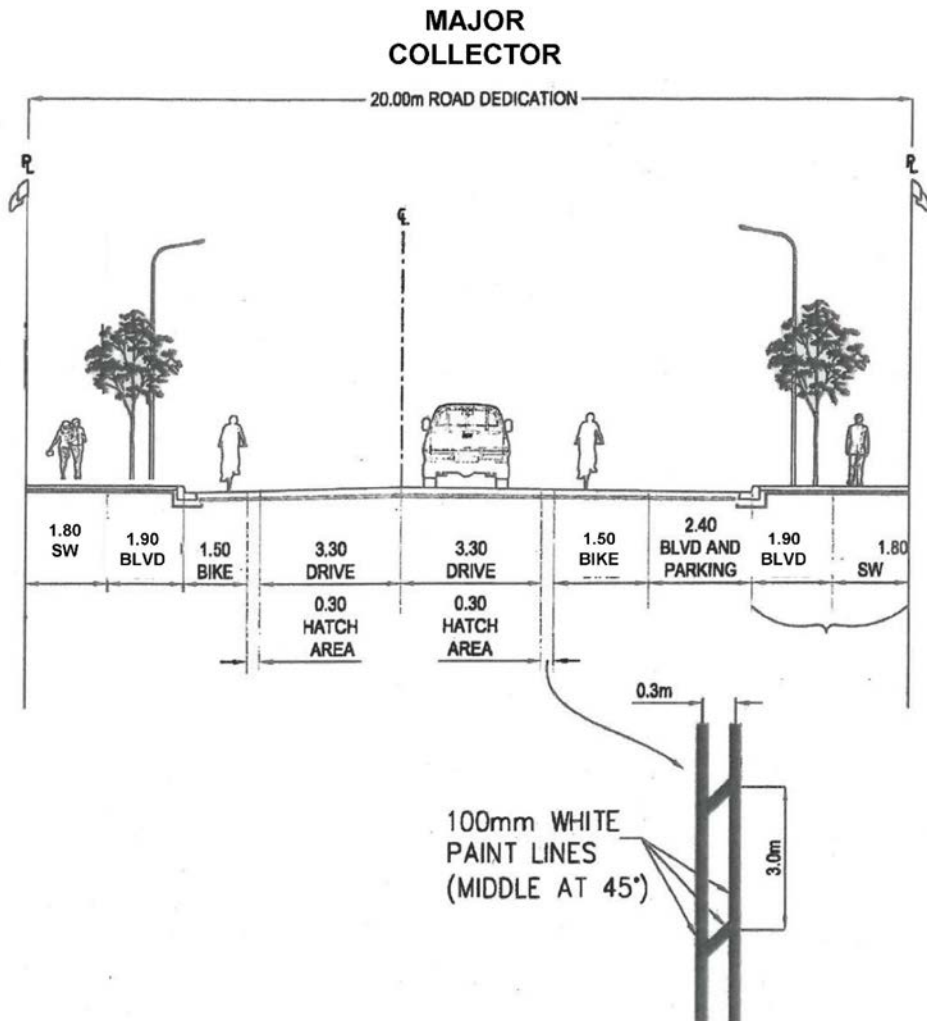
- (1) READ A FIRST and SECOND time this 5th day of June, 2024
- (2) READ A THIRD time this day of, 2024
- (3) ADOPTED this ____ day of _____, 2024

Mayor

Corporate Officer

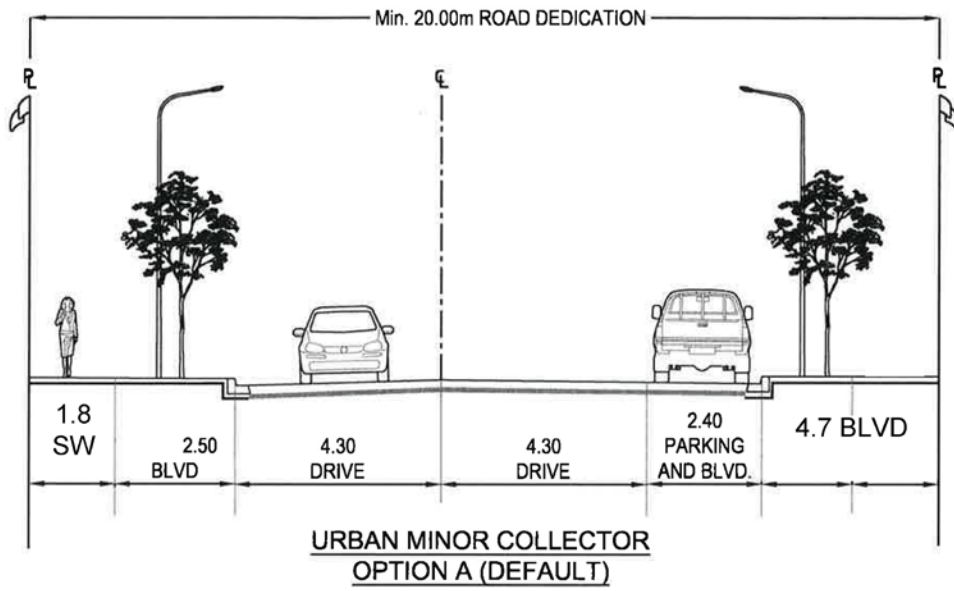
BYLAW 1261.18

SCHEDULE "1"



BYLAW 1261.18

SCHEDULE "2"



Comox Official Community Plan Amendment Bylaw 1685.11 Markup

TOWN OF COMOX

BYLAW 1685.11

A BYLAW TO AMEND COMOX OFFICIAL COMMUNITY PLAN BYLAW 1685

WHEREAS Council has the authority under the provisions of the Local Government Act to amend the Official Community Plan Bylaw;

AND WHEREAS Council has considered the amendments in conjunction with its financial plan, any waste management plan that is applicable in the municipality or regional district and Town of Comox May 2020 Housing Needs Report Data Results including Summary Form;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. Title

This Bylaw may be cited for all purposes as "Comox Official Community Plan Amendment Bylaw 1685. 11".

2. Amendments

Comox Official Community Plan Bylaw 1685 is hereby amended as follows:

(1) ~~Schedule "A" Part 2: Objectives and Policies, Section 2.1.1.3 Residential Policies, as follows:~~

~~(a) By replacing Section 2.1.1.3(g) with the following text:~~

~~Land designated Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill as shown on Map 1 – Land Use Designations is intended to accommodate the following land uses low rise apartments up to a maximum of 4 storeys, townhouses, single detached dwellings, secondary suites, coach houses, duplexes, triplexes, patio homes and child care facilities. Excluded from the 4 storey apartment maximum are the following:~~

- ~~i. Subject to the provision of satisfactory amenities, 1700 Balmoral Avenue – Lot A, Section 56, Comox District, Plan VIP81758;~~
- ~~ii. 941 Aspen Road – Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279;~~
- ~~iii. 2077 Hector Road – Lot A, DL 170, Comox District, Plan 18002; and~~
- ~~iv. 2123 Hector Road – Lot 4 District Lot 170 Comox District Plan VIP60685.~~

(2) Schedule "A" Part 5: Maps is hereby amended by:

(a) Designating Western portion of the parcel legally described

Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279 (941 Aspen West), shown shaded in Schedule "1A", which is attached to and forms part of this bylaw, as within the following:

- (i) Map 1 – Land Use Designations, as Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill;
and
- (ii) Development Permit Areas (DPAs), as shown in their respective maps:
3 General Multi-Family
11 Wildlife Corridor

(b) Designating Eastern portion of the parcel legally described

Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279 (941 Aspen East), shown shaded in Schedule "1B", which is attached to and forms part of this bylaw, as within the following:

- (i) Map 1 – Land Use Designations, as Mixed Use: Commercial – Residential;
and
- (ii) Development Permit Areas (DPAs), as shown in their respective maps:
#4 Mixed Use: Commercial - Residential
11 Wildlife Corridor

(c) Designating parcel legally described

Lot A, District Lot 170, Comox District Plan 18002 (2077 Hector), shown shaded in Schedule "1C", which is attached to and forms part of this bylaw, as within the following:

- (j) Map 1 – Land Use Designations, as Residential: Low Rise Apartments, Townhouses and Ground Oriented Infill;
and
- (ii) Development Permit Areas (DPAs), as shown in their respective maps:
3 General Multi-Family
7 Riparian Areas
11 Wildlife Corridor

- (d) Adding to Map 3, Parks, Trails and Open Space, an extension of Hector Greenway ~~and new east-west neighbourhood trail~~ shown in red line in Schedule "2", which is attached to and forms part of this bylaw, as "Off-Street Trails (existing and proposed)"
 - (e) Adding to Map 5, Road Network, an extension of Hector Road shown in bright blue line in Schedule "3-4", which is attached to and forms part of this bylaw, as "Minor Collector"
 - (f) Adding to Map 7, Bicycle Network, an extension of Hector Road shown in bright green line in Schedule "4-5", which is attached to and forms part of this bylaw, as "Proposed Major Bike Route"
- (3) Comox Official Community Plan Bylaw 1685 is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw.

3. Repeal Comox Valley Regional District OCP Bylaw

- (1) Rural Comox Valley Official Community Plan Bylaw No. 337, 2014 (as at September 19, 2016), is hereby amended by replacing section 1(3) in its entirety with the following text:
- This bylaw applies to all lands as noted in section 1(1) except in relation to those properties legally described as:
- (a) PID 023-020-113, Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279; and
 - (b) PID 003-856-704, Lot A, District Lot 170, Comox District Plan 18002
- (2) Rural Comox Valley Official Community Plan Bylaw No. 337, 2014 (as at September 19, 2016) is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw.

4. Adoption

- | | |
|-----------------------------------|------------------------------------|
| (1) READ A FIRST time this | 5 th day of June, 2024 |
| (2) READ A SECOND time this | 5 th day of June, 2024 |
| (3) ADVERTISED A FIRST time this | 26 th day of June, 2024 |
| (4) ADVERTISED A SECOND time this | 3 rd day of July, 2024 |
| (5) PUBLIC HEARING HELD this | 10 th day of July, 2024 |
| (6) READ A THIRD time this | day of , 2024 |
| (7) ADOPTED this | day of , 2024 |

Mayor

Chief Administrative Officer

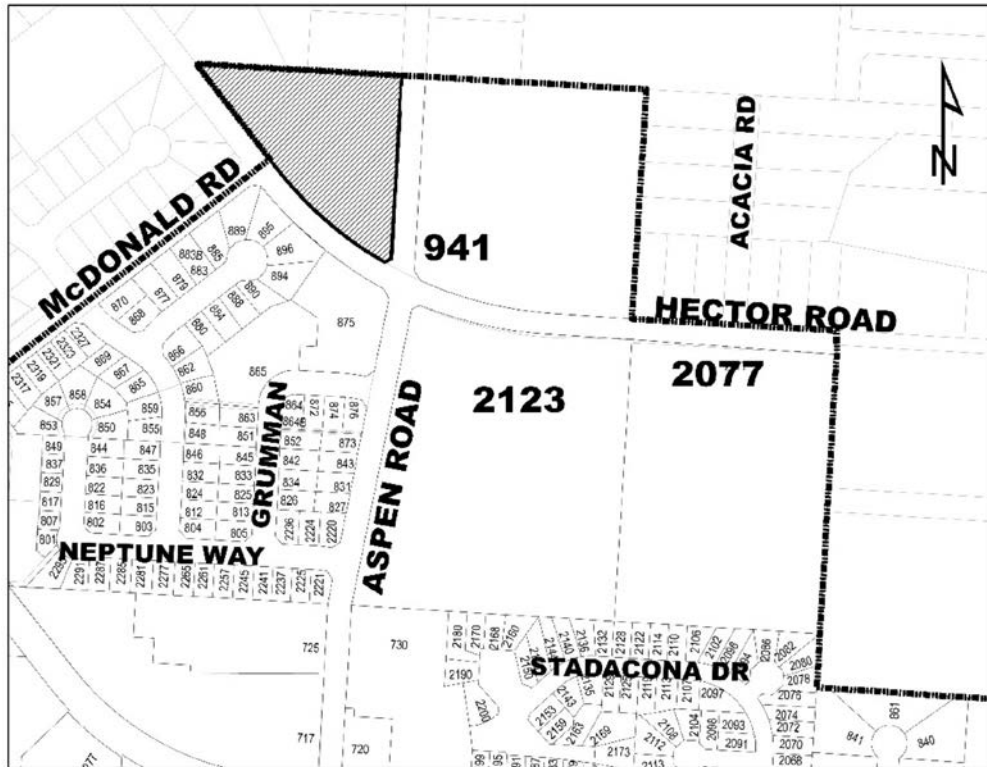
BYLAW 1685.11

SCHEDULE "1A"

941 Aspen Road (Aspen-West)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



BYLAW 1685.11

SCHEDULE "1B"

941 Aspen Road (Aspen-East)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



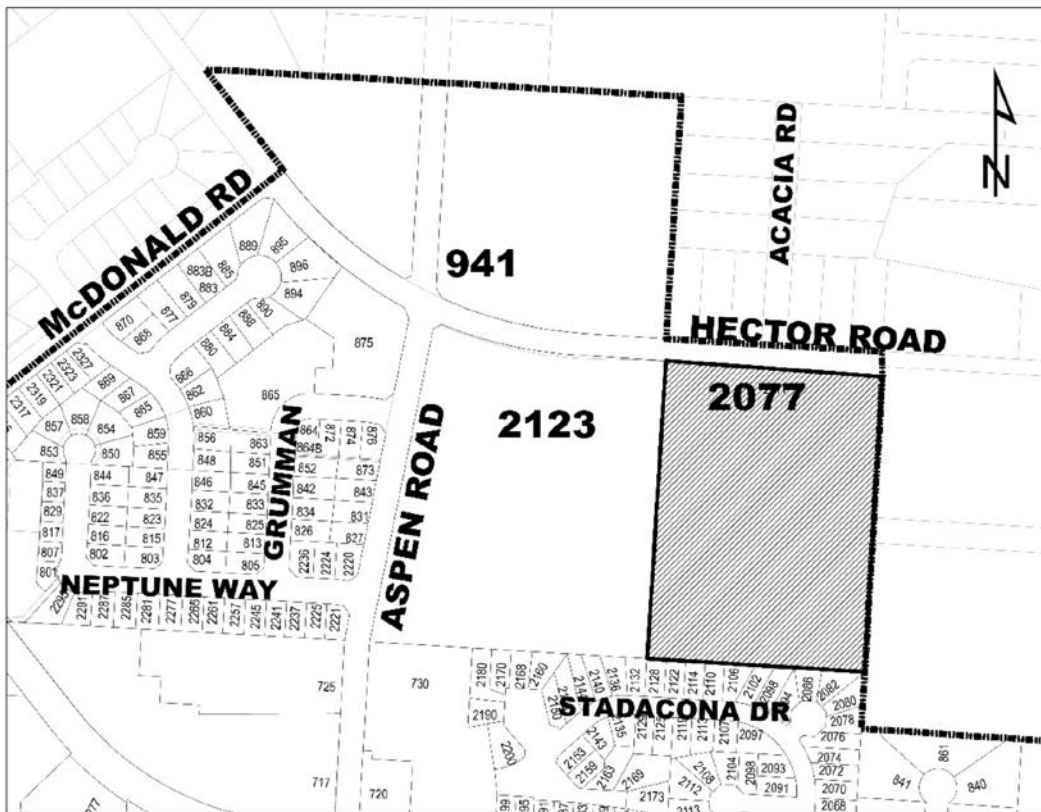
BYLAW 1685.11

SCHEDULE "1C"

2077 Hector Road

PID 003-856-704

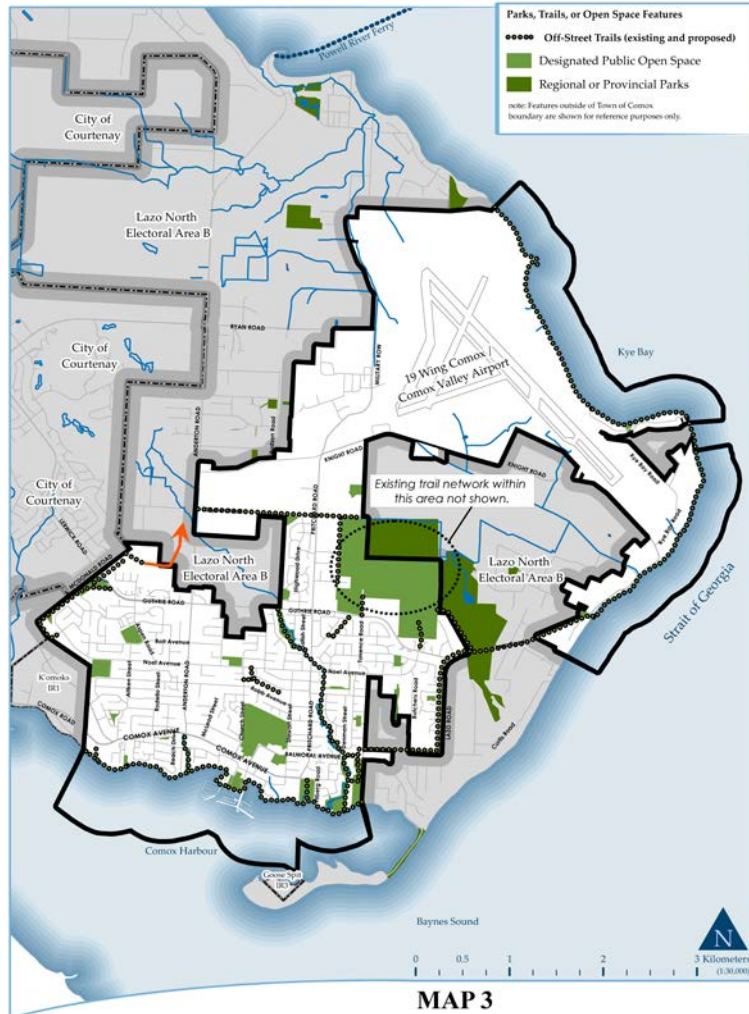
Lot A, DL 170, Comox District, Plan 18002



BYLAW 1685.11

SCHEDULE "2"

OFF-STREET TRAILS (EXISTING AND PROPOSED)



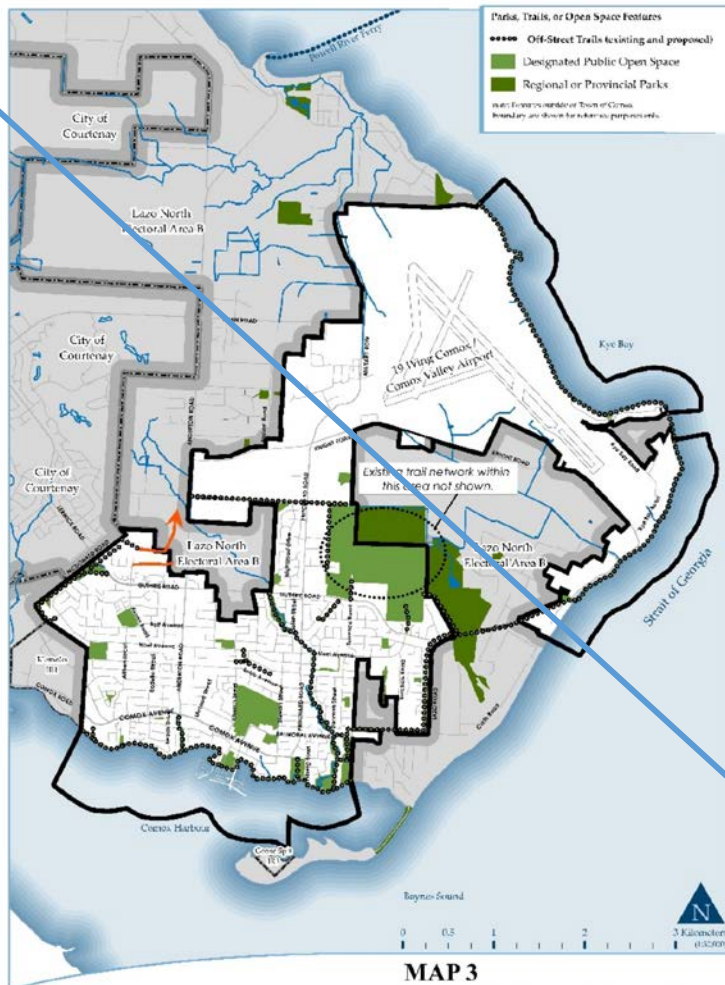
**MAP 3
Parks, Trails, and Open Space**

June 2011,
Bylaw #1771, May 7, 2014
Bylaw #1954, February 17, 2021

map created by the Arlington Group for the Town of Comox
data provided by:
Town of Comox, Comox Valley Regional District, Province of British Columbia,
Department of Natural Resources Canada © All rights reserved
WITS (Wildlife Free Sewardship Program) 2009,
BC Nature and Community Mapping Network.

MAP 3: PARKS, TRAILS, AND OPEN SPACE





**MAP 3
Parks, Trails, and Open Space**



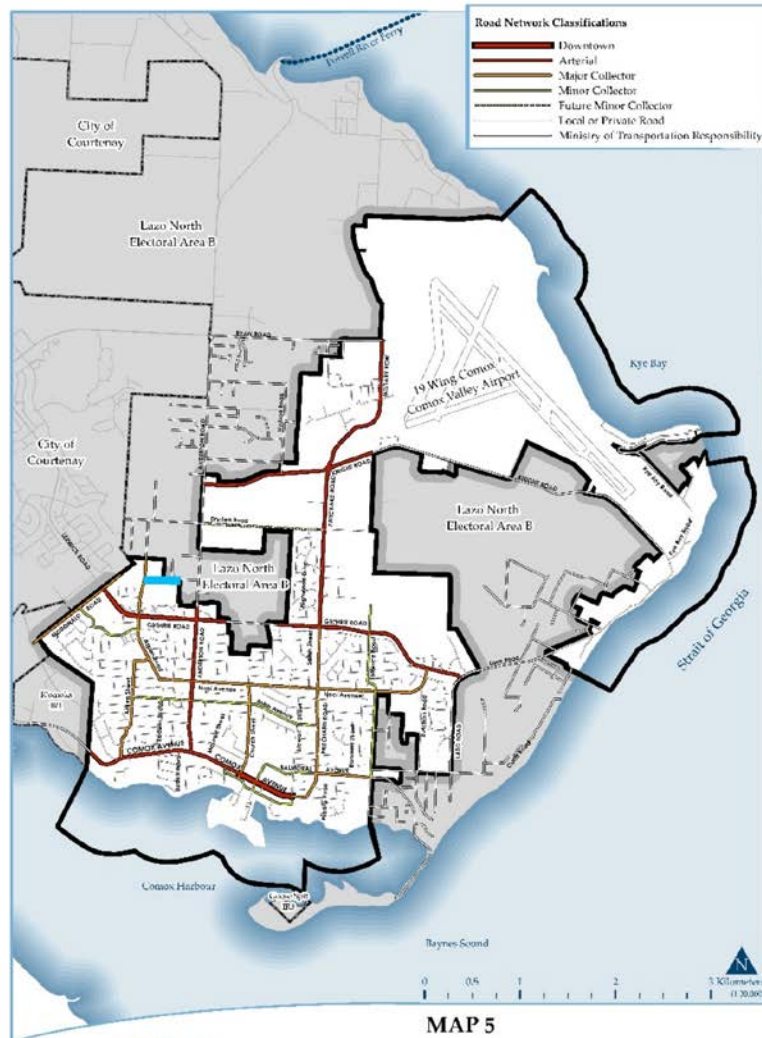
June 2011,
Bylaw #1771, May 7, 2014
Bylaw #1956, February 17, 2021
re-created by the Arlington Group for the Town of Comox
data provided by:
Town of Comox, Comox Valley Regional District, Province of British Columbia,
Department of Parks, Recreation & Culture © All rights reserved
WTF (Wildlife Trust Stewardship Program) 2005
© Kelowna Community Mapping Network

MAP 3: PARKS, TRAILS, AND OPEN SPACE

BYLAW 1685.11

SCHEDULE "3"

MINOR COLLECTOR



**MAP 5
Road Network**

July 2011
Bylaw # 1954, February 17, 2021

map created by the Arbitrage Group and Town of Comox

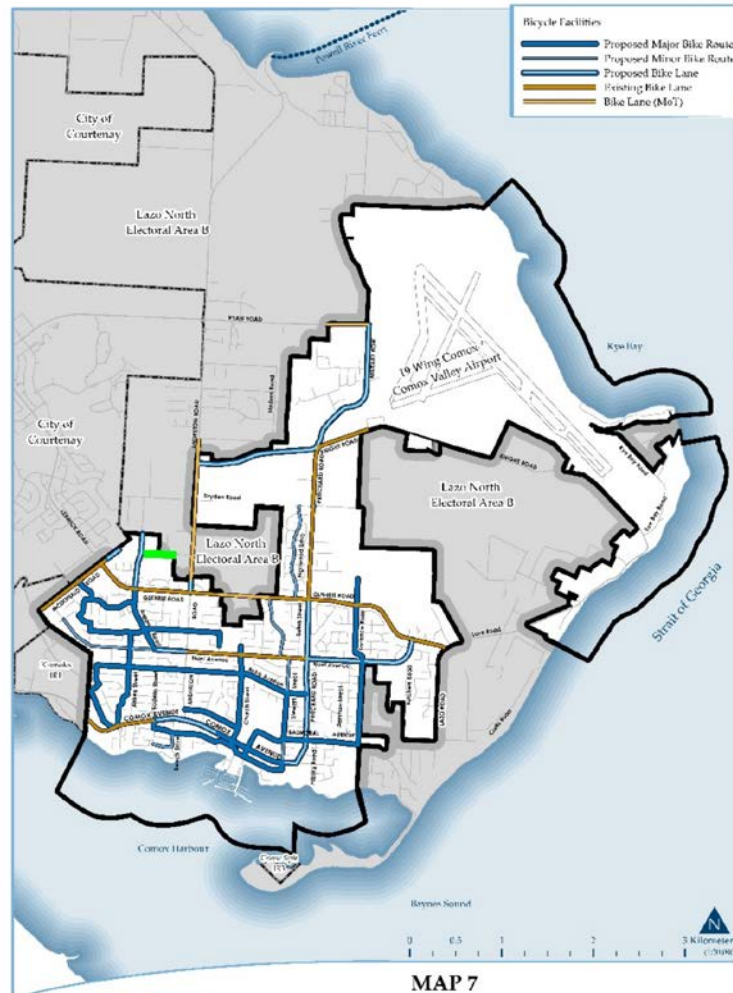
data provided by:
Town of Comox, Comox Valley Regional District, Province of British Columbia,
Department of Natural Resources Canada © All rights reserved.

MAP 5 ROAD NETWORK

BYLAW 1685.11

SCHEDULE "4"

PROPOSED MAJOR BIKE ROUTE



**MAP 7
Bicycle Network**

July, 2011
Bylaw # 1954, February 17, 2021

map created by the A/C Group and Town of Comox.
data provided by:
Town of Comox, Comox Valley Regional District, Province of British Columbia,
Department of Natural Resources Canada © All rights reserved

MAP 7 - BICYCLE NETWORK

Comox Zoning Amendment Bylaw 1850.46 Markup

**TOWN OF COMOX
BYLAW 1850.46**

A BYLAW TO AMEND COMOX ZONING BYLAW 1850

WHEREAS Council has the authority under the provisions of the *Local Government Act* to amend the Zoning Bylaw;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. Title

This bylaw may be cited for all purposes as the “Comox Zoning Amendment Bylaw 1850.46”

2. Amendments

Comox Zoning Bylaw 1850 is hereby amended as follows:

A. Section 2.7(2) Penalties is amended by adding the following:

Column 1	Column 2	Column 3
Offence	Bylaw Section	Fine Amount
Unlawful use – RM6.1 zone	211.1	\$250.00
Failure to comply with conditions – RM6.1 zone	211.2	\$250.00
Unlawful projection into setback – RM6.1 zone	211.9	\$250.00
Unlawful accessory buildings – RM6.1 zone	211.11	\$250.00
Unlawful use – RM6.2 zone	212.1	\$250.00
Failure to comply with conditions – RM6.2 zone	212.2	\$250.00
Unlawful projection into setback – RM6.2 zone	212.9	\$250.00
Unlawful projections into storey differentials – RM6.2 zone	212.11	\$250.00
Unlawful accessory buildings – RM6.2 zone	212.12	\$250.00
Unlawful use – C7.1 zone	310.1	\$250.00
Failure to comply with conditions – C7.1 zone	310.2	\$250.00
Unlawful projection into setback – C7.1 zone	310.9	\$250.00
Unlawful projections into storey differentials – C7.1 zone	310.11	\$250.00
Unlawful accessory buildings – C7.1 zone	310.12	\$250.00

B. Section 3.2, Definitions, is amended by

(1) Adding alphabetically the following text:

Greenway

A street or portion thereof that is:

- (a) 6.0 metres or more in width, restricted to pedestrian or bicycle use, or
- (b) used for stormwater retention or detention pond maintenance access.

(2) Replacing the following text:

i. Exterior Side Lot line

A lot line other than a front or rear lot line, which abuts a street; does not include a lot line which abuts a lane.

with

Exterior Side Lot line

A lot line other than a front or rear lot line, which abuts a street; does not include a lot line which abuts a lane or greenway.

ii. Exterior Side Yard

A side yard which abuts a street other than a lane

With

Exterior Side Yard

A side yard which abuts a street other than a lane or greenway.

iii. Front Lot Line

A lot line common to a parcel and a street other than a lane. Where a parcel abuts two or more streets, only the street with the shortest length along the parcel shall be used to determine front lot lines. Where a parcel does not abut a street, lot lines from which common vehicle access is provided shall be deemed to be common to a street. Notwithstanding the above, for parcels next to the sea, *front lot line* is defined as a lot line next to the sea.

With

Front Lot Line

A lot line common to a parcel and a street other than a lane or greenway. Where a parcel abuts two or more streets, only the street with the shortest length along the parcel shall be used to determine front lot lines. Where a parcel does not abut a street, lot lines from which common vehicle access is provided shall be deemed to be common to a street. Notwithstanding the above, for parcels next to the sea, *front lot line* is defined as a lot line next to the sea.

C. Section 4.1 Classification of Zones is amended by:

(1) adding the following text under the Multi-Family Residential Zones heading:

RM 6.1 Apartment;
RM 6.2 Apartment and Townhouse

(2) adding the following text under the Commercial Zones heading:

C 7.1 Commercial – Residential

D. Section 5 General regulations is amended by:

Adding as Section 5.3(5) the following text:

In RM6.1; RM6.2 and C7.1 zones, no fences are permitted within a front and exterior side yard, as shown in Figure 5-2.

E. Section 5.12 Projections into Required Setbacks is amended by adding as subsection 5(14) the following text:

In the RM6.1; RM6.2 and C7.1 zones,

- a) awnings, canopies, cornices, eaves, gutters, leaders, ornamental features, pilasters, sills, or sunshades may project up to 0.6 m into a required setback ~~or required storey differential~~;
- b) decks, patios, stairs, and landings less than 0.6 m above immediately adjacent finished grade, may project up to 1.5 metres into a required setback; and
- c) balconies may project up to 1.0 metre into a required setback ~~or required storey differential~~.

- F. Section 7, Class I and Class II Bicycle Parking Spaces is amended by replacing Section 7.3 with the following text:

Required Bicycle Parking Spaces for Downtown and RM6.1; RM6.2 and C7.1 zones

The following applies to all lands shown shaded in Appendix “S” and all lands zoned RM6.1; RM6.2 and C7.1:

- (1) Class I and Class II bicycle parking spaces shall be provided in accordance with the requirements in Sections 7.6(1) and 7.8(1). In the case of a use not specifically mentioned, the required Class I and Class II bicycle parking spaces shall be the same as for the most comparable use.
- (2) At the option of the owner or occupier of a parcel, structure, or part thereof, rather than provide the Class I bicycle parking spaces in accordance with Section 7.3(1), the owner or occupier may pay to the Town the sum of \$2,000.00 for each bicycle parking space not provided.
- (3) The monies referred to in Section 7.3(2) are payable in accordance with the provisions of the *Local Government Act* of British Columbia, for the purpose of providing transportation infrastructure that supports walking, bicycling, public transit or other alternative forms of transportation.
- (4) Transportation infrastructure provided under Section 7.3(3) shall not be available on a reserved or dedicated basis.

- G. Schedule “A” is amended by

- i. adding as Section 211 the RM 6.1 Apartment zone, as shown in Schedule “1A”, which is attached to and forms part of this Bylaw;
- ii. adding as Section 212 the RM 6.2 Apartment and Townhouse zone, as shown in Schedule “1B”, which is attached to and forms part of this Bylaw;
- iii. adding as Section 310 the C 7.1 Commercial – residential zone, as shown in Schedule “1C”, which is attached to and forms part of this Bylaw;

- H. Schedule “B” (the Zoning Map) is amended by rezoning

- i. The Western portion of the property legally described as **Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279** (941 Aspen West), shown shaded on **Schedule “2A”** which is attached to and forms part of this Bylaw, from Country Residential CR-1 to RM 6.1 Apartment zone
- ii. The Eastern portion of the property legally described as **Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279** (941 Aspen East),

shown shaded on **Schedule “2B”** which is attached to and forms part of this Bylaw,

from Country Residential CR-1
to C 7.1 Commercial - Residential; and

- iii. That property legally described **Lot A, District Lot 170, Comox District Plan 18002** (2077 Hectar), shown shaded on **Schedule “2C”** which is attached to and forms part of this Bylaw,

from Country Residential CR-1
to RM 6.2 Apartment and Townhouse zone

- I. Comox Zoning Bylaw 1850 is further amended by making such consequential changes as are required to reflect the foregoing amendments, including without limitation changes in the numbering and order of the sections of the bylaw.

3. Repeal CVRD Zoning Bylaw

- (1) Section Two, subsection (1) of Comox Valley Zoning Bylaw, 2005 (as at September 19, 2016) is hereby amended by inserting, immediately after the text "This bylaw shall be applicable to Electoral Areas ‘A’, ‘B’ and ‘C’ (Comox Valley) of the Regional District of Comox-Strathcona" the following text:

with the exception of the properties legally described as
Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279;
Lot A, District Lot 170, Comox District Plan 18002

- (2) Comox Valley Zoning Bylaw, 2005 (as at September 19, 2016) is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw.

4. Repeal CVRD Floodplain Management Bylaw

- (1) Part B, Section 1, Bylaw No. 2782, being the “Floodplain Management Bylaw, 2005” (as at September 19, 2016), is hereby amended by inserting, immediately after the text “this bylaw shall be applicable to all electoral areas within the Regional District of Comox Strathcona with the exception of”:

the properties legally described as
Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279;
Lot A, District Lot 170, Comox District Plan 18002

- (2) Bylaw No. 2782, being the “Floodplain Management Bylaw, 2005” (as at September 19, 2016) is further amended by making such consequential changes as are required to reflect the foregoing amendments, including, without limitation, changes in the numbering and order of the sections of the bylaw

3. Adoption

- | | |
|-----------------------------------|-----------------------------------|
| (1) READ A FIRST time this | 5 th day of June, 2024 |
| (2) READ A SECOND time this | 5 th day of June, 2024 |
| (3) ADVERTISED A FIRST time this | ___ day of _____, 2024 |
| (4) ADVERTISED A SECOND time this | ___ day of _____, 2024 |
| (5) PUBLIC HEARING HELD this | ___ day of _____, 2024 |
| (6) READ A THIRD time this | ___ day of _____, 2024 |
| (7) ADOPTED this | ___ day of _____, 2024 |

Mayor

Corporate Officer

BYLAW 1850.46

SCHEDULE "1A"

211. RM 6.1 APARTMENT

211.1 Permitted Uses:

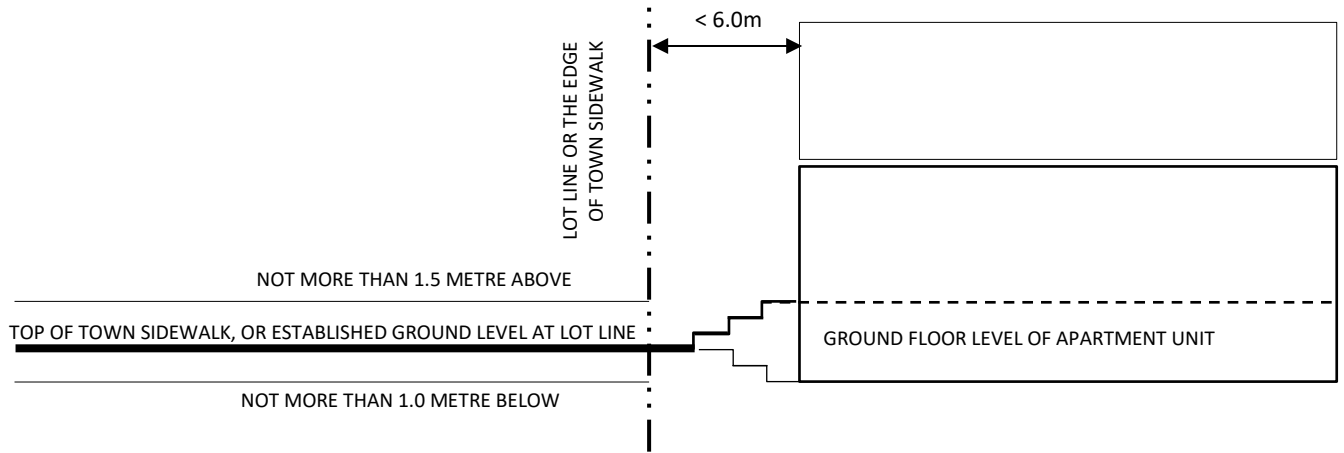
In the RM 6.1 zone, the following uses are permitted, and all other uses are prohibited:

- (1) Accessory structures and uses, excluding:
 - (i) Buildings other than those used for dwelling unit accessory or child care facility uses; and
 - (ii) Outside storage
- (2) Apartment dwellings
- (3) Child care facilities
- (4) Home occupations
- (5) Townhouse dwellings

211.2 Conditions of Use:

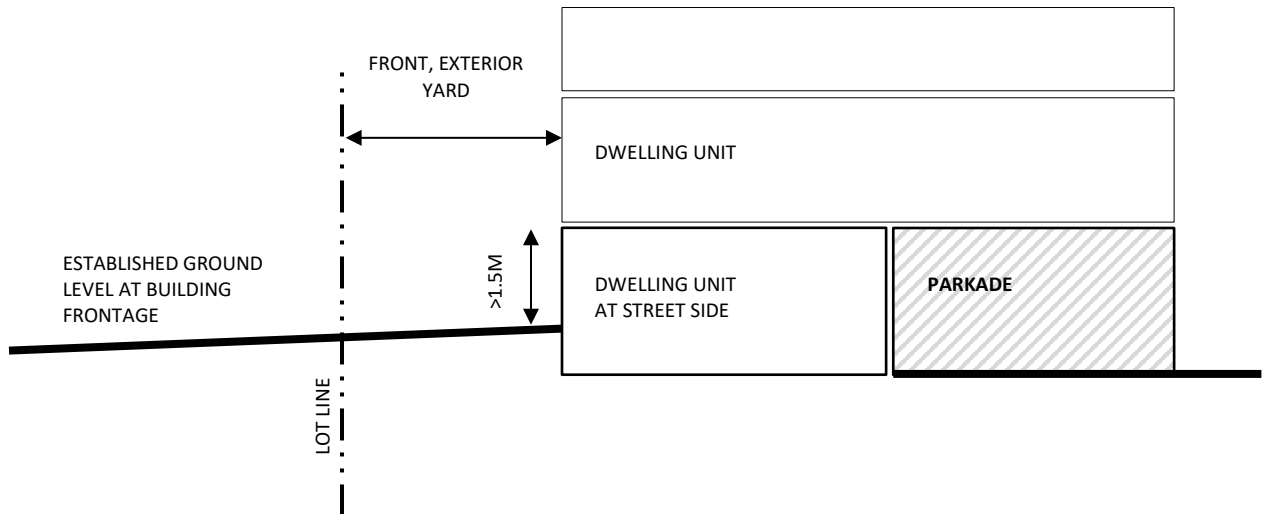
- (1) All permitted uses shall be located within a portion of a building, completely enclosed by exterior walls, except for landscape material, childcare facilities, accessory structures and accessory uses.
- (2) Child care facilities shall be located on the ground floor of an apartment building or accessory building.
- (3) Townhouse dwelling units shall:
 - (a) only be located within an apartment building;
 - (b) not be located above or below another townhouse dwelling unit; and
 - (c) not be located above a second storey of an apartment building.
- (4) Not less than 25% of all dwellings units on a parcel shall include 2 or more bedrooms. Where the calculation of 25% of dwelling units results in a fractional number, the nearest whole number shall be used.
- (5) Apartment and townhouse dwelling units shall:
 - (a) have a ground floor located not more than 1.5 metres above or 1.0 metre below the top of an adjacent Town sidewalk in accordance with Figure 211-1 when located along a front or exterior side yard, and in the absence of a Town sidewalk, as measured from established ground level at the front or exterior side lot line; and
 - (b) section 211.2(5)(a) does not apply to dwelling units located 6.0 metres or more from an edge of a Town sidewalk, and in the absence of a Town sidewalk, from a front or exterior side lot line.

Figure 211-1.



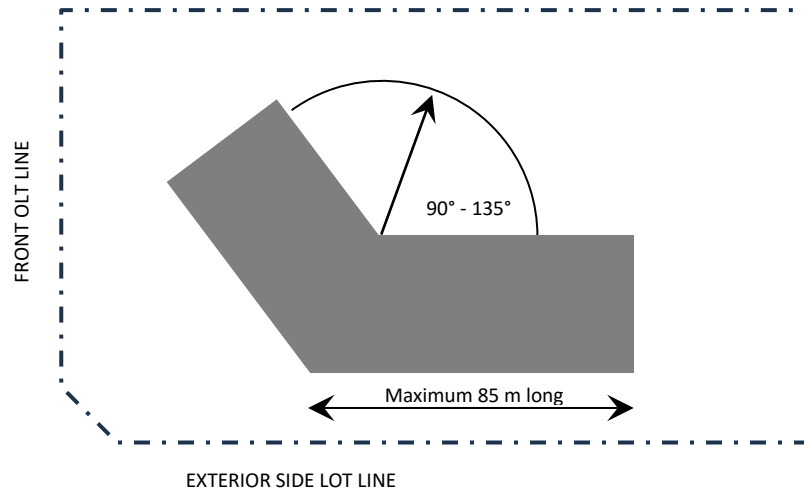
- (6) Along a front or exterior side yard, parking located within a building at or more than 1.50 metres above the established ground level shall be screened from the street by dwelling units in accordance with Figure 211-2.

Figure 211-2.



- (7) Residential building length shall:
 - (a) not exceed 85 metres in length, including projections; and
 - (b) where a directional turn of 90⁰ to 135⁰ degrees is provided; no section of a residential building shall exceed 85 metres in length, as shown in Figure 211-3.

Figure 211-3.



- (8) Garbage and recycling storage, and accessory uses such as dog runs and community gardens shall not be located within front or exterior side yard.

211.3 Density:

- (1) Density shall not be less than 55 units per hectare;
- (2) Density shall not exceed 80 units per hectare;
- (3) Notwithstanding 211.3(2), where not less than 40 % of total on-site parking is provided underground or within a residential building footprint, density shall not exceed 150 units per hectare; and
- (4) Notwithstanding 211.3(2), where in excess of 80 % of total on-site parking is underground parking or provided within a building footprint – density N/A.

211.4 Parcel Area:

Parcel area shall not be less than 3,500 square metres.

211.5 Parcel Frontage:

Parcel frontage shall not be less than 50.0 metres.

211.6 Parcel Depth:

Parcel depth shall not be less than 50.0 metres.

211.7 Parcel Coverage:

- (1) Parcel coverage shall not exceed 40%; and
- (2) Parcel coverage including parking areas, loading areas and driveways that are open sided and roofless shall not exceed 75%.

211.8 Height and Storeys:

Height shall not exceed:

~~(1) 24.0 m for buildings up to 6 storeys;~~

~~(2) 20.0 m for buildings up to 5 storeys;~~

(3) 16.0 m for buildings up to 4 storeys; and

(4) 12.0 m for buildings up to 3 storeys.

211.9 Required Setbacks

(1) Front

- (a) For 9.0 metres as measured parallel to and at the front setback – Front setback shall not be less than 9.0 metres in accordance with Figure 211- 4;
- (b) Additional 211.9(1)(a) setback area shall be provided for every 100 metres of front lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
- (c) All other situations – Front setback shall not be less than 3.0 metres.

(2) Rear

- (a) Rear setback shall not be less than 9.0 metres; and
- (b) Notwithstanding 211.9(2)(a), for building elevations measuring 25.0 metres or less in width along the rear yard – Rear setback shall not be less than 5.0 metres, in accordance with Figure 211- 5.

(3) Side -interior

- (a) Interior side setback shall not be less than 9.0 metres; and
- (b) Notwithstanding 211.9(3)(a), for building elevations measuring 25.0 metres or less in width along the interior side yard – Interior side setback shall not be less than 5.0 metres, in accordance with Figure 211- 5.

(4) Side - exterior

- (a) For 9.0 metres as measured parallel to and at the exterior side setback – Exterior side setback shall not be less than 9.0 metres in accordance with Figure 211- 4;
- (b) Additional 211.9(4)(a) setback area shall be provided for every 100 metres of exterior side lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
- (c) All other situations – Exterior side setback shall not be less than 3.0 metres.

Figure 211-4.

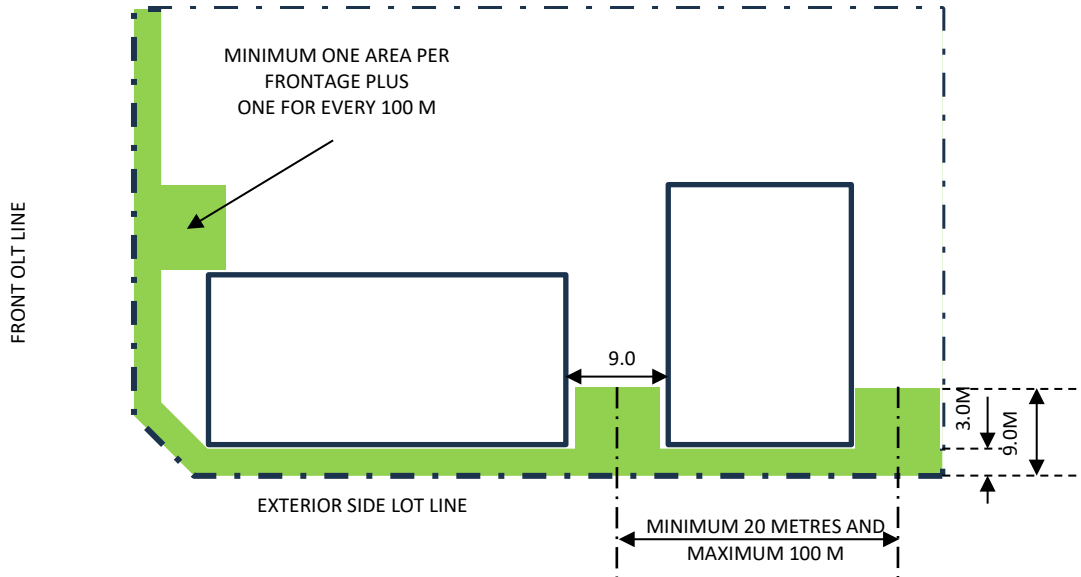
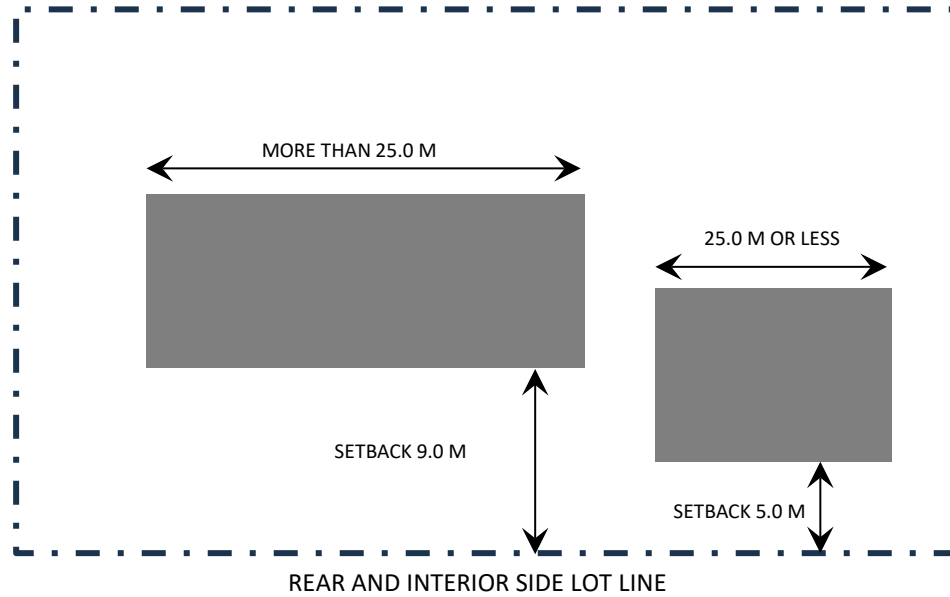


Figure 211-5



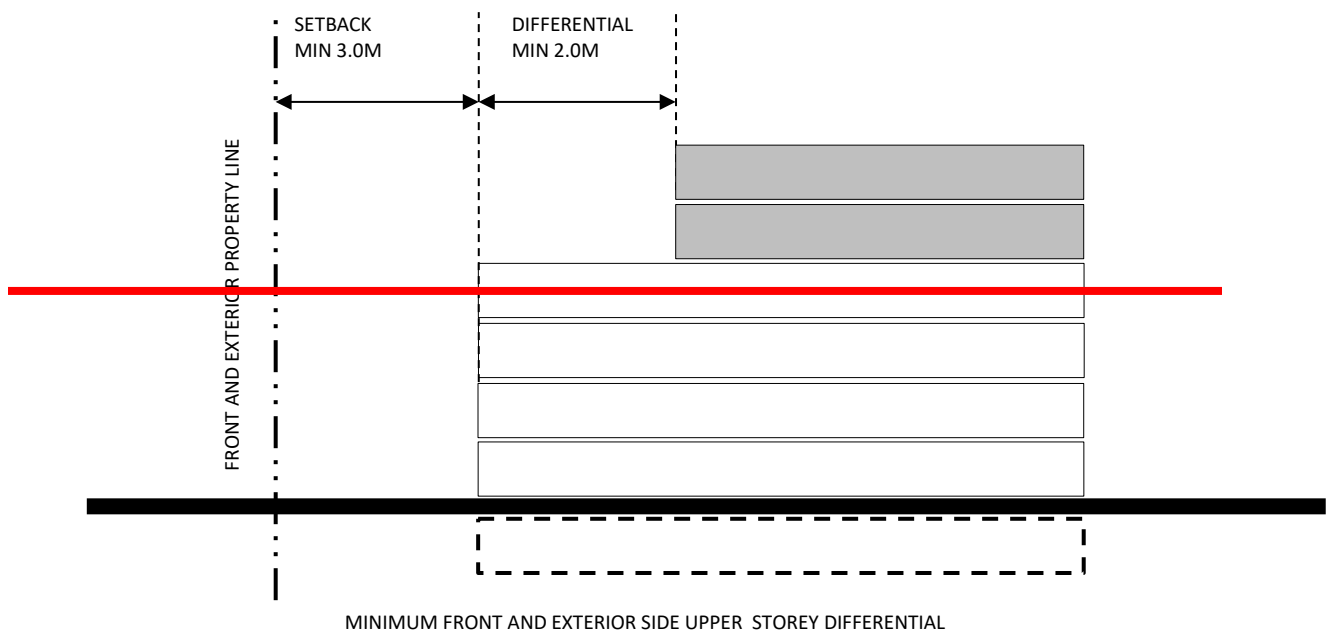
- (5) Notwithstanding sections 211.9 (1) to (4), where a parcel abuts a greenway that is not less than 10.0 metres in width, the setback from a lot line abutting a greenway shall be not less than 5.0 metres.

- (6) Notwithstanding section 211.9, building setback from any lot lines shared with the Agricultural Land Reserve shall be not less than 15.0 metres.

~~211.10 Required Storey Differentials~~

~~For buildings over 4 storeys in height, along a front and exterior side elevations, at the 5th and 6th storeys, a minimum differential setback of 2.0 metres shall be provided from the storeys below, in accordance with Figure 211-7~~

~~Figure 211-7.~~



211.11 Accessory Buildings and Structures

Accessory Buildings shall:

- (1) Not be located within front or exterior side yard;
- (2) have a parcel coverage not exceeding 5%;
- (3) Not exceed 9.0 metres and 2 storeys in height, for accessory buildings used for multi-family amenity and recreation; and
- (4) Not exceed 4.5 metres in height, for other accessory buildings.

211.12 Screening

- (1) Along a front or exterior side yard, parking located within a building more than 0.6 metres but less than 1.50 metres above finished grade at building frontage shall be screened from streets by vegetation and landscaping.
- (2) The following shall be screened in accordance with Section 8:
 - (a) Off-street parking and loading areas;
 - (b) Above ground utility boxes and utility transformers;
 - (c) Garbage or recycling compounds and collection areas, unless enclosed in a building;
 - (d) RM6.1 zoned parcels from abutting Residential zoned parcels; and
 - (e) Parcels abutting land within the Agricultural Land Reserve, in accordance with specifications in Appendix B1.

211.13 Off-Street Vehicle Parking and Loading

- (1) Off-street vehicles parking and loading shall be provided in accordance with Section 6;
- (2) No more than one driveway per street frontage shall be permitted on a parcel;
- (3) Internal driveways shall not exceed 6.0 metres in width.

211.14 Off-Street Bicycle Parking

- (1) Off-street parking for bicycles shall be provided in accordance with Section 7; and
- (2) Notwithstanding Section 7, Class II bicycle rooms may be provided within individual storage units located on either the ground level or underground parkade level of an apartment building with direct access to the outdoors.

211.15 Other Requirements:

- (1) Overhead wiring shall not be permitted on a parcel. All new services on a parcel shall be placed underground.
- (2) Unoccupied open spaces shall be fully and suitably landscaped with landscape material; this does not include environmentally sensitive areas and required buffers where native planting is to be left undisturbed, including watercourse setbacks as specified in Section 5.19.
- (3) All buildings shall conform to Section 5.19, Watercourse Regulations.

BYLAW 1850.46

SCHEDULE "1B"

212. RM 6.2 APARTMENT AND TOWNHOUSE

212.1 Permitted Uses:

In the RM 6.2 zone, the following uses are permitted, and all other uses are prohibited:

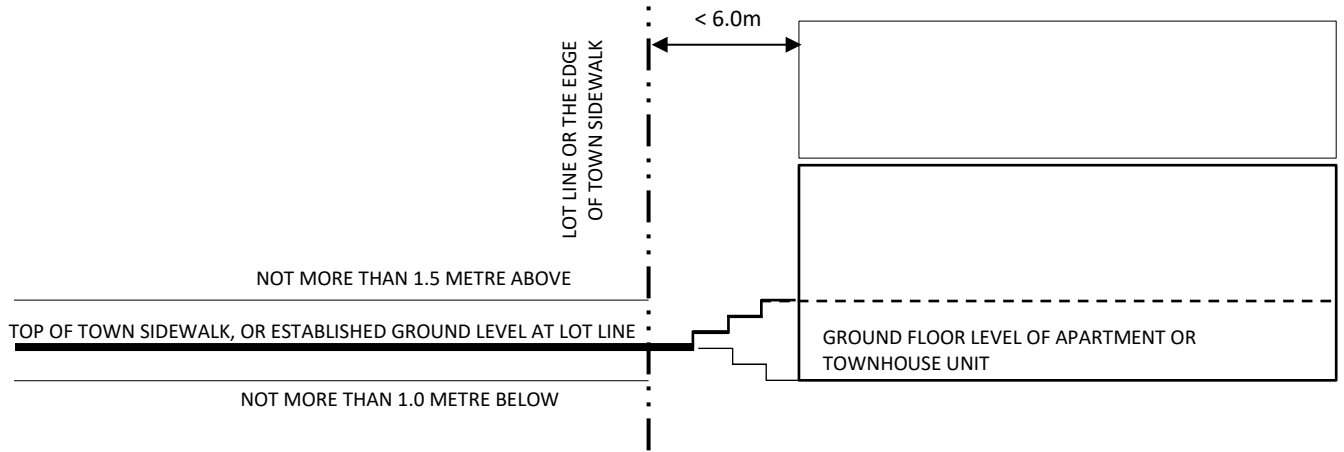
- (1) Accessory structures and uses, excluding:
 - (i) Buildings other than those used for dwelling unit accessory or child care facility uses; and
 - (ii) Outside storage
- (2) Apartment dwellings
- (3) Child care facilities
- (4) Home occupations
- (5) Townhouse dwellings
- (6) Two-family dwellings

212.2 Conditions of Use:

- (1) All permitted uses shall be located within a portion of a building, completely enclosed by exterior walls, except for landscape material, childcare facilities, accessory structures and accessory uses.
- (2) Child care facilities shall be located on the ground floor of an apartment building or accessory building.
- (3) The combined number of two-family and townhouse dwelling units on a parcel shall not exceed 20% of all dwelling units on the parcel. Where the calculation of 20% dwelling units results in a fractional number, the nearest whole number shall be used.
- (4) Not less than 25% of all dwellings units on a parcel shall include 2 or more bedrooms. Where the calculation of 25% of dwelling units results in a fractional number, the nearest whole number shall be used.
- (5) Townhouse dwelling units may be located within an apartment building;
- (6) Townhouse dwelling units located within an apartment building shall:
 - (a) not be located above or below another townhouse dwelling unit; and
 - (b) not be located above a second storey of an apartment building.
- (7) Apartment, townhouse and two-family dwelling units shall:
 - (a) have a ground floor located not more than 1.5 metres above or 1.0 metre below the top of an adjacent Town sidewalk in accordance with Figure 212-1 when located along a front or exterior side yard, and in the absence of a Town sidewalk, as measured from established ground level at the front or exterior side lot line; and

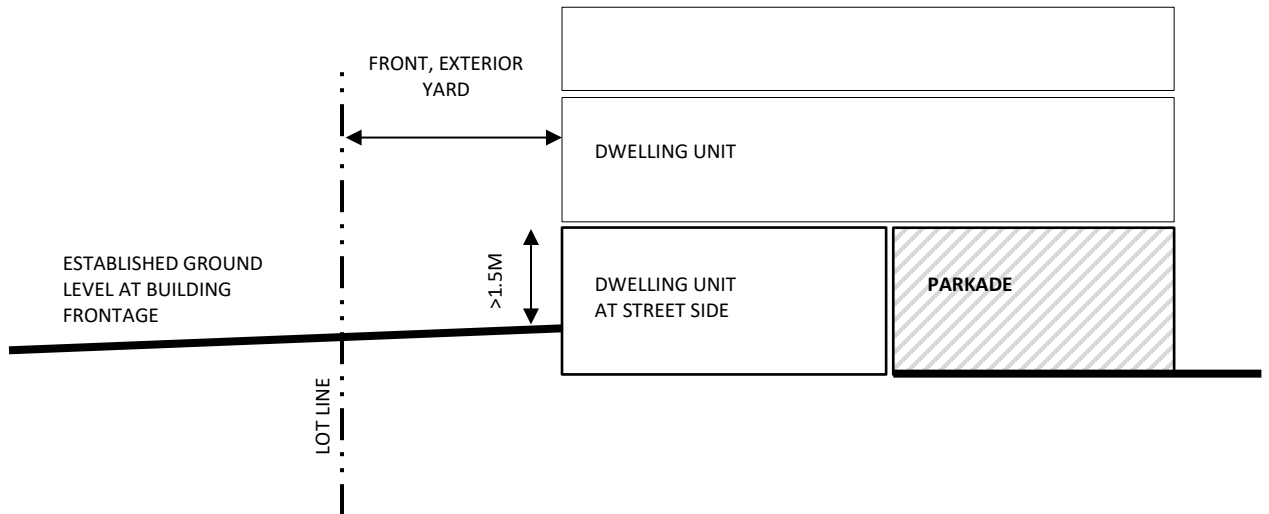
- (b) section 212.2(7)(a) does not apply to dwelling units located 6.0 metres or more from an edge of a Town sidewalk, and in the absence of a Town sidewalk, from a front or exterior side lot line.

Figure 212-1.



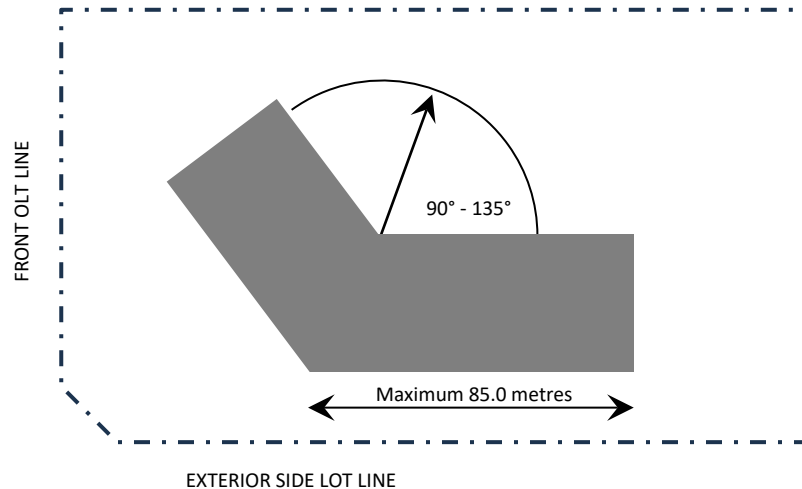
- (8) Along a front or exterior side yard, parking located within a building at or more than 1.50 metres above the established ground level shall be screened from the street by dwelling units in accordance with Figure 212-2.

Figure 212-2.



- (9) Residential building length shall:
- (a) not exceed 85.0 metres in length, including projections; and
 - (b) where a directional turn of 90° to 135° degrees is provided; no section of a residential building shall exceed 85.0 metres in length, as shown in Figure 212-3.

Figure 212-3.



- (10) Garbage and recycling storage, and accessory uses such as dog runs and community gardens shall not be located within front or exterior side yard.

212.3 Density:

- (1) Density shall not be less than 55 units per hectare;
- (2) Density shall not exceed 80 units per hectare;
- (3) Notwithstanding 212.3(2), where not less than 40 % of total on-site parking is provided underground or within a residential building footprint, density shall not exceed 150 units per hectare; and
- (4) Notwithstanding 212.3(2), where in excess of 80 % of total on-site parking is underground parking or provided within a building footprint – density N/A.

212.4 Parcel Area:

Parcel area shall not be less than 3,500 square metres.

212.5 Parcel Frontage:

Parcel frontage shall not be less than 50.0 metres.

212.6 Parcel Depth:

Parcel depth shall not be less than 50.0 metres.

211.8 Parcel Coverage:

- (1) Parcel coverage shall not exceed 40%; and
- (2) Parcel coverage including parking areas, loading areas and driveways that are open sided and roofless shall not exceed 75%.

211.9 Height and Storeys:

- (1) Apartment building height shall not exceed:
 - ~~(a) 24.0 m for buildings up to 6 storeys;~~
 - ~~(b) 20.0 m for buildings up to 5 storeys;~~
 - (c) 16.0 m for buildings up to 4 storeys; and
 - (d) 12.0 m for buildings up to 3 storeys.
- (2) Townhouse and two-family dwelling building height shall not exceed 12.0 metres and 3 storeys.

212.9 Required Setbacks

- (1) Front
 - (d) For 9.0 metres as measured parallel to and at the front setback – Front setback shall not be less than 9.0 metres in accordance with Figure 212- 4;
 - (e) Additional 212.9(1)(a) setback area shall be provided for every 100 metres of front lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
 - (f) All other situations – Front setback shall not be less than 3.0 metres.
- (2) Rear
 - (c) Rear setback shall not be less than 9.0 metres; and
 - (d) Notwithstanding 212.9(2)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the rear yard – Rear setback shall not be less than 5.0 metres, in accordance with Figure 212- 5.
- (3) Side-interior
 - (c) Interior side setback shall not be less than 9.0 metres; and
 - (d) Notwithstanding 212.9(3)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the interior side yard – Interior side setback shall not be less than 5.0 metres, in accordance with Figure 212- 5.
- (4) Side- exterior
 - (d) For 9.0 metres as measured parallel to and at the exterior side setback – Exterior side setback shall not be less than 9.0 metres in accordance with Figure 212- 4;
 - (e) Additional 212.9(4)(a) setback area shall be provided for every 100 metres of exterior side lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and

minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;

- (f) All other situations – Exterior side setback shall not be less than 3.0 metres.

Figure 212-4.

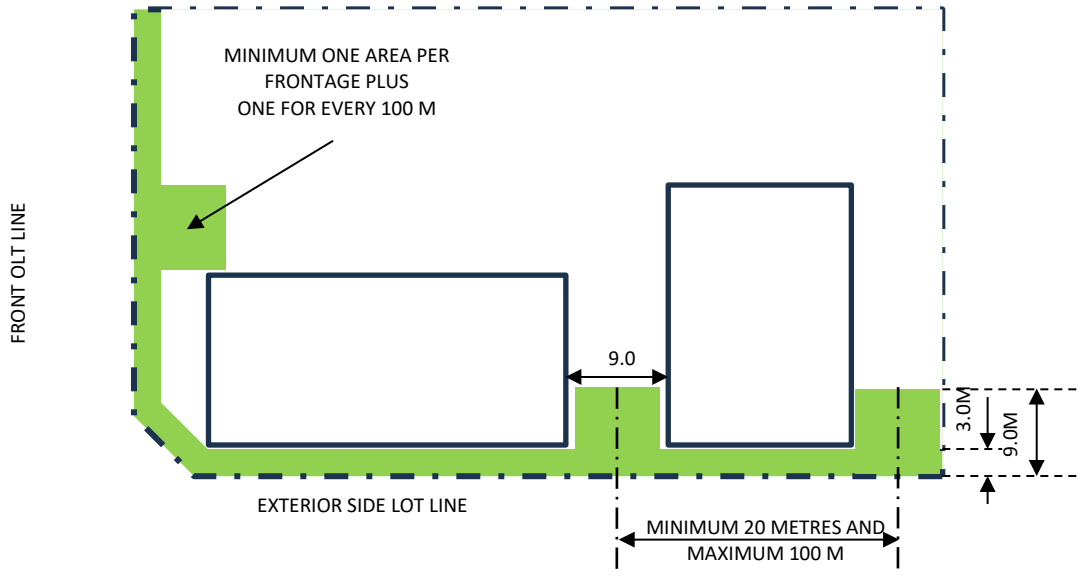
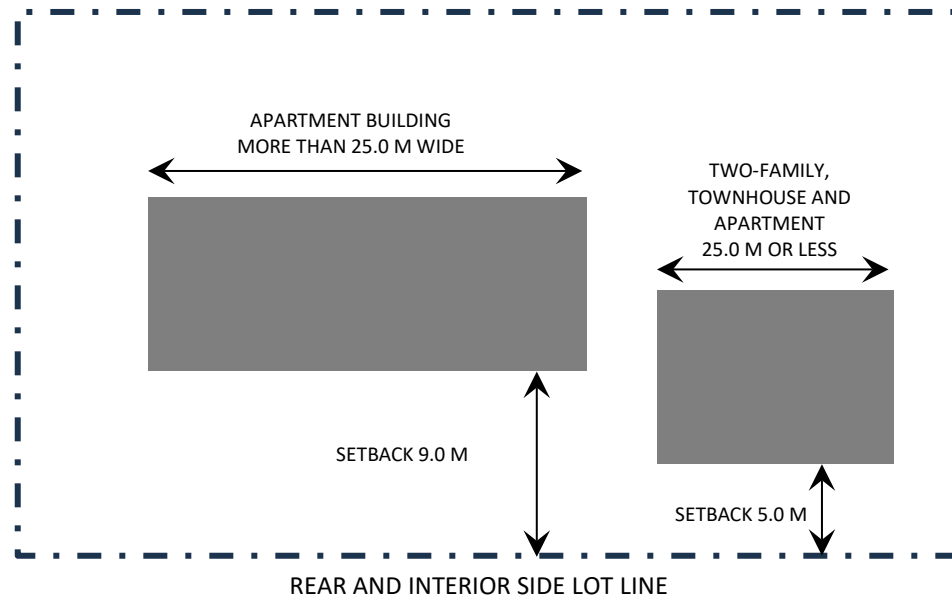


Figure 212-5

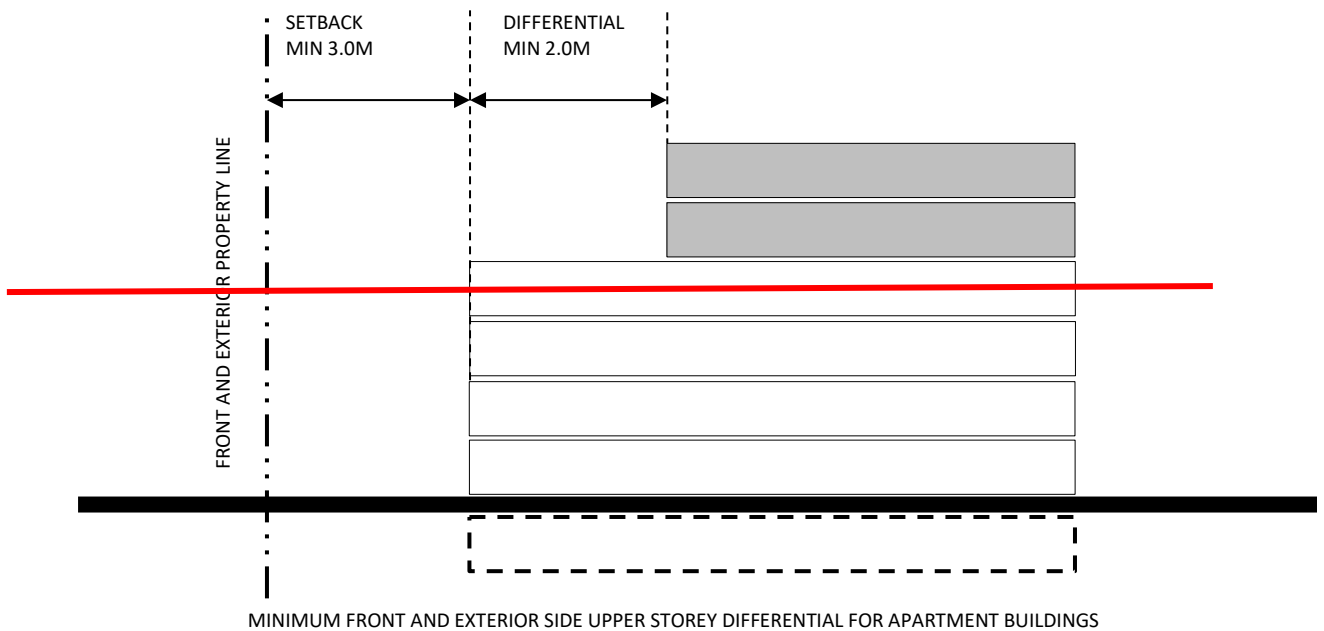


- (5) Notwithstanding sections 212.9 (1) to (4), where a parcel abuts a greenway that is not less than 10.0 metres in width, the setback from a lot line abutting a greenway shall be not less than 5.0 metres; and
- (6) Notwithstanding sections 212.9, building setback from any lot lines shared with the Agricultural Land Reserve shall be not less than 15.0 metres.

212.11 Required Storey Differentials

~~For apartment buildings over 4 storeys in height, along front and exterior side elevations at the 5th and 6th storeys, a minimum differential setback of 2.0 metres shall be provided from the storeys below, in accordance with Figure 212-6.~~

~~Figure 212-6.~~



212.12 Accessory Buildings and Structures

Accessory Buildings shall:

- (1) Not be located within front or exterior side yard;
- (2) Have a parcel coverage not exceeding 5%;
- (3) Not exceed 9.0 metres and 2 storeys in height, for accessory buildings used for multi-family amenity and recreation;
- (4) Not exceed 4.5 metres in height, for other accessory buildings;
- (5) Notwithstanding section 21.12(4) accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit: not exceed 6.5 m² in gross floor area per dwelling unit and not exceed 2.5 metres in height; and

- (6) Accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit be excluded from required rear and interior side setbacks, provided that no accessory building is located closer than 2.0 metres to a rear or interior side lot line;

212.13 Screening

- (1) Along a front or exterior side yard, parking located within an apartment building more than 0.6 metres but less than 1.50 metres above finished grade at building frontage shall be screened from streets by vegetation and landscaping.
- (2) The following shall be screened in accordance with Section 8:
 - (f) Off-street parking and loading areas;
 - (g) Above ground utility boxes and utility transformers;
 - (h) Garbage or recycling compounds and collection areas, unless enclosed in a building;
 - (i) RM6.2 zoned parcels from abutting Residential zoned parcels; and
 - (j) Parcels abutting land within the Agricultural Land Reserve, in accordance with specifications in Appendix B1.

212.14 Off-Street Vehicle Parking and Loading

- (1) Off-street vehicles parking and loading shall be provided in accordance with Section 6;
- (2) No more than one driveway per street frontage shall be permitted on a parcel;
- (3) Internal driveways shall not exceed 6.0 metres in width.

212.15 Off-Street Bicycle Parking

- (1) Off-street parking for bicycles shall be provided in accordance with Section 7; and
- (2) Notwithstanding Section 7, Class II bicycle rooms may be provided within individual storage units located on either the ground level or underground parkade level of an apartment building with direct access to the outdoors.

212.15 Other Requirements:

- (1) Overhead wiring shall not be permitted on a parcel. All new services on a parcel shall be placed underground.
- (2) Unoccupied open spaces shall be fully and suitably landscaped with landscape material; this does not include environmentally sensitive areas and required buffers where native planting is to be left undisturbed, including watercourse setbacks as specified in Section 5.19.
- (3) All buildings shall conform to Section 5.19, Watercourse Regulations.

BYLAW 1850.46

SCHEDULE "1C"

310. C 7.1 COMMERCIAL RESIDENTIAL

310.1 Permitted Uses:

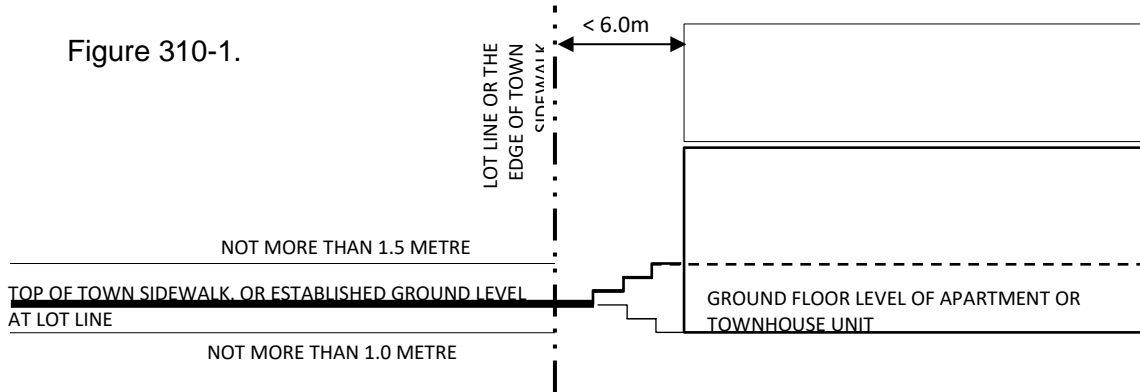
In the C 7.1 zone, the following uses are permitted, and all other uses are prohibited:

- (1) Accessory structures and uses, excluding:
 - (i) Buildings other than those used for dwelling unit accessory or child care facility uses; and
 - (ii) Outside storage
- (2) Apartment dwellings
- (3) Townhouse dwellings
- (4) Two-family dwellings
- (5) Following Commercial Uses
 - (a) Artist studios
 - (b) Art galleries
 - (c) Child care facilities
 - (d) Dental clinics
 - (e) Denturist labs
 - (f) Home occupations
 - (g) Libraries
 - (h) Locksmiths
 - (i) Medical clinics
 - (j) Offices
 - (k) Personal service establishments
 - (l) Pet grooming
 - (m) Restaurants
 - (n) Restaurants – coffee shops
 - (o) Retail stores, which have a gross floor area not in excess of 120 square metres
 - (p) Small appliance repair shops, processing or packaging of food or beverage products, or establishments that repair or assemble electronic equipment which:
 - (i) have a total non-retail floor area not in excess of 120 square metres,
 - (ii) retail directly from the premises, and
 - (iii) have the retail area extending the full width of the premises and located adjacent to the primary pedestrian entrance.
 - (q) Tailor shops, which:
 - (i) have a gross floor area not in excess of 120 square metres,
 - (ii) retail directly from the premises, and
 - (iii) have the retail area extending the full width of the width of the premises and located adjacent to the primary pedestrian entrance
 - (r) Veterinary Clinic – Small Animal Practice, excluding the boarding or kenneling of animals.

310.2 Conditions of Use:

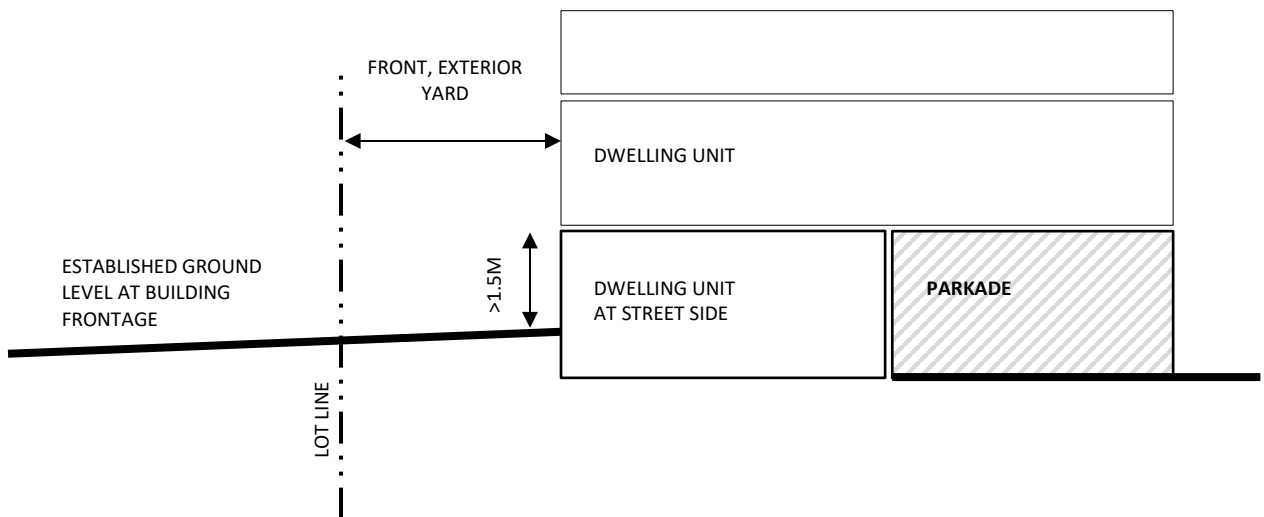
- (1) All permitted uses shall be located within a portion of a building, completely enclosed by exterior walls, except for landscape material, child care facilities, restaurants, accessory structures and accessory uses.
- (2) Child care facilities shall be located on the ground floor of an apartment building or accessory building.
- (3) Permitted uses other than dwelling units, dwelling unit accessory uses, and child care facilities shall
 - (a) be located within an apartment building, and
 - (b) not be located above the ground floor.
- (4) For a distance of not less than 60.0 m measured parallel to a road list in section 310.2(3)(a), permitted use shall be limited to apartment buildings with ground floor commercial use for a depth of not less than 7.5 m, measured perpendicularly from the exterior of the building elevation facing the road:
 - (a) Aspen Road.
- (5) Permitted uses other than dwelling units and child care facilities shall:
 - (a) Have a ground floor located not more than 0.50 metres above and not more than 0.50 metres below the top of adjacent Town sidewalk; and
 - (b) Section 310.2(5)(a) does not apply to commercial units located not less than 6.0 metres from a front and exterior side lot line.
- (6) Townhouse dwelling units may be located within an apartment building;
- (7) Townhouse dwelling units located within an apartment building shall:
 - (c) not be located above or below another townhouse dwelling unit; and
 - (d) not be located above a second storey of an apartment building.
- (8) The combined number of two-family and townhouse dwelling units on a parcel shall not exceed 20% of all dwelling units on the parcel. Where the calculation of 20% dwelling units results in a fractional number, the nearest whole number shall be used.
- (8) Not less than 25% of all dwellings units on a parcel shall include 2 or more bedrooms. Where the calculation of 25% of dwelling units results in a fractional number, the nearest whole number shall be used.
- (9) Apartment, townhouse and two-family dwelling units shall:
 - (c) Have a ground floor located not more than 1.5 metres above or 1.0 metre below the top of an adjacent Town sidewalk in accordance with Figure 310-1 when located along a front or exterior side yard, and in the absence of a Town sidewalk, as measured from established ground level at the front or exterior side lot line; and
 - (d) Section 310.2(9)(a) does not apply to dwelling units located 6.0 metres or more from an edge of a Town sidewalk, and in the absence of a Town sidewalk, from a front or exterior side lot line.

Figure 310-1.



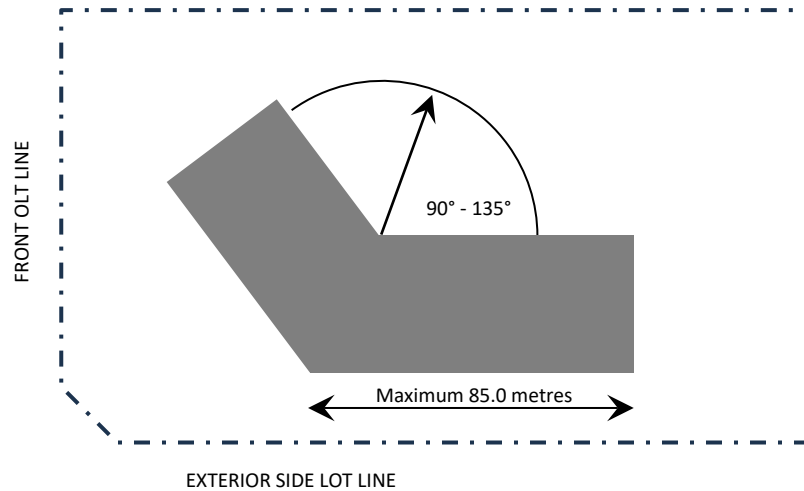
- (10) Along a front or exterior side yard, parking located within an apartment building at or more than 1.50 metres above the established ground level shall be screened from the street by dwelling units in accordance with Figure 310-2.

Figure 310-2.



- (11) Residential building length shall:
 - (c) not exceed 85.0 metres in length, including projections; and
 - (d) where a directional turn of 90° to 135° degrees is provided; no section of a residential building shall exceed 85.0 metres in length, as shown in Figure 310-3.

Figure 310-3.



- (12) Garbage and recycling storage, and accessory uses such as dog runs and community gardens shall not be located within front or exterior side yard.

310.3 Density:

- (1) Density shall not be less than 55 units per hectare;
- (2) Density shall not exceed 80 units per hectare;
- (3) Notwithstanding 310.3(2), where not less than 40 % of total on-site parking is provided underground or within a residential building footprint, density shall not exceed 150 units per hectare; and
- (4) Notwithstanding 310.3(2), where in excess of 80 % of total on-site parking is underground parking or provided within a building footprint – density N/A.

310.4 Parcel Area:

Parcel area shall not be less than 3,500 square metres.

310.5 Parcel Frontage:

Parcel frontage shall not be less than 50.0 metres.

310.6 Parcel Depth:

Parcel depth shall not be less than 50.0 metres.

310.7 Parcel Coverage:

- (1) Parcel coverage shall not exceed 40%; and
- (2) Parcel coverage including parking areas, loading areas and driveways that are open sided and roofless shall not exceed 75%.

310.8 Height and Storeys:

- (1) Apartment building height shall not exceed:
~~(a) 24.0 m for buildings up to 6 storeys;~~
~~(b) 20.0 m for buildings up to 5 storeys;~~
(c) 16.0 m for buildings up to 4 storeys; and
(d) 12.0 m for buildings up to 3 storeys.
- (2) Townhouse and two-family dwelling building height shall not exceed 12.0 metres and 3 storeys.

310.9 Required Setbacks

- (1) Front
 - (a) For 9.0 metres as measured parallel to and at the front setback – Front setback shall not be less than 9.0 metres in accordance with Figure 310- 4;
 - (b) Additional 310.9(1)(a) setback area shall be provided for every 100 metres of front lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
 - (c) All other situations – Front setback shall not be less than 3.0 metres.
- (2) Rear
 - (a) Rear setback shall not be less than 9.0 metres; and
 - (b) Notwithstanding 310.9(2)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the rear yard – Rear setback shall not be less than 5.0 metres, in accordance with Figure 310- 5.
- (3) Side-interior
 - (a) Interior side setback shall not be less than 9.0 metres; and
 - (a) Notwithstanding 310.9(3)(a), for buildings under 3 storeys high containing only two-family and townhouse dwellings; and for apartments buildings' elevations measuring 25.0 metres or less in width along the interior side yard – Interior side setback shall not be less than 5.0 metres, in accordance with Figure 310- 5.
- (4) Side- exterior
 - (b) For 9.0 metres as measured parallel to and at the exterior side setback – Exterior side setback shall not be less than 9.0 metres in accordance with Figure 310- 4;
 - (c) Additional 310.9(4)(a) setback area shall be provided for every 100 metres of exterior side lot line length, excluding lot lines that form a corner cut-off or are within 6.0 metres of the intersection of two streets, other than a lane; and minimum spacing between the multiple 9.0 metres setback areas shall be 20.0 metres;
 - (d) All other situations – Exterior side setback shall not be less than 3.0 metres.

Figure 310-4.

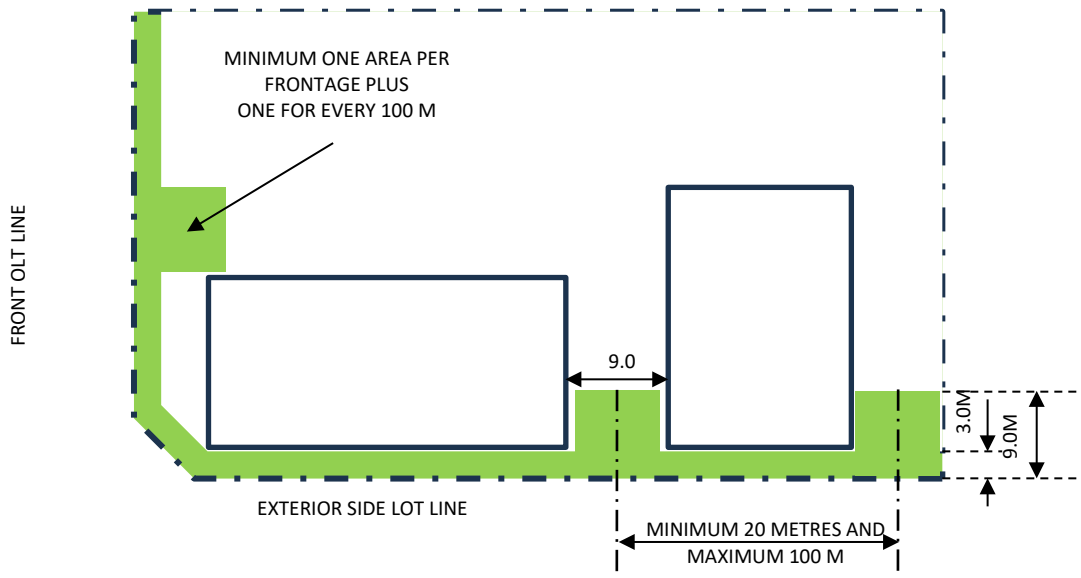
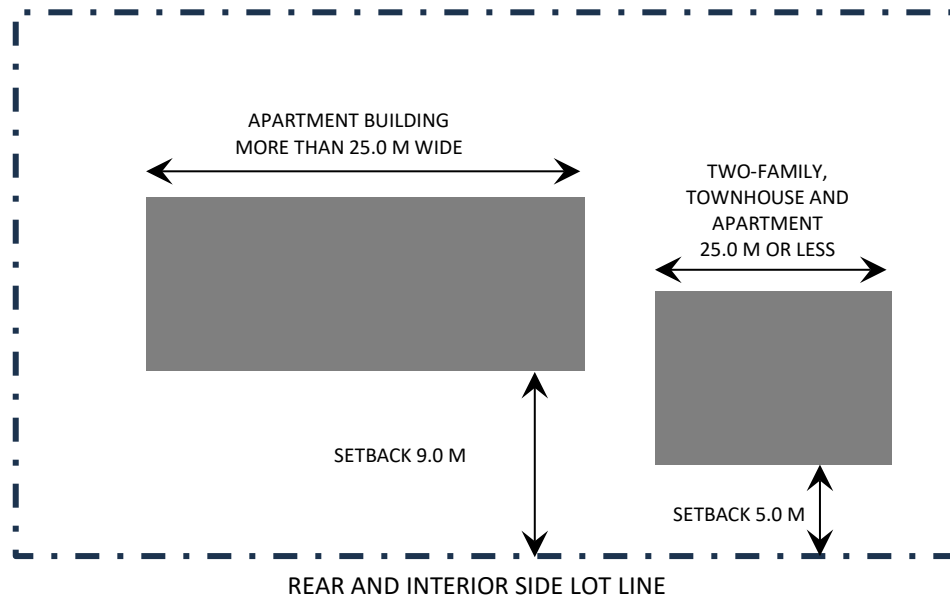


Figure 310-5

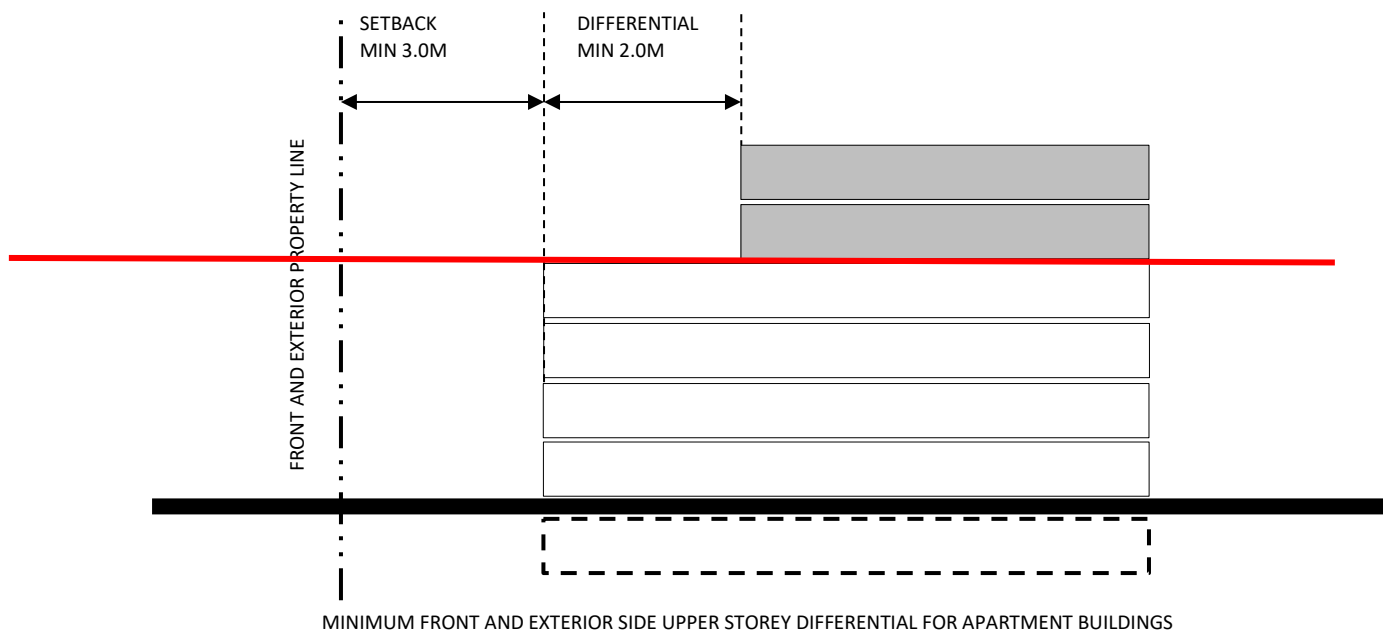


- (5) Notwithstanding sections 310.9 (1) to (4), where parcel abuts a greenway that is not less than 10.0 metres in width, the setback from a lot line abutting a greenway shall be not less than 5.0 metres; and
- (6) Notwithstanding sections 310.9, building setback from any lot lines shared with the Agricultural Land Reserve shall be not less than 15.0 metres.

310.10—Required Storey Differentials

For apartment buildings over 4 storeys in height, along front and exterior side elevations at the 5th and 6th storeys, a minimum differential setback of 2.0 metres shall be provided from the storeys below, in accordance with Figure 310—6.

Figure 310-6.



MINIMUM FRONT AND EXTERIOR SIDE UPPER STOREY DIFFERENTIAL FOR APARTMENT BUILDINGS

310.11 Accessory Buildings and Structures

Accessory Buildings shall:

- (1) Not be located within front or exterior side yard;
- (2) Have a parcel coverage not exceeding 5%;
- (3) Not exceed 9.0 metres and 2 storeys in height, for accessory buildings used for multi-family amenity and recreation;
- (4) Not exceed 4.5 metres in height, for other accessory buildings;
- (5) Notwithstanding section 21.12(4) accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit: not exceed 6.5 m² in gross floor area per dwelling unit and not exceed 2.5 metres in height; and
- (6) Accessory buildings intended for exclusive use of individual two-family or townhouse dwelling unit be excluded from required rear and interior side setbacks, provided that no accessory building is located closer than 2.0 metres to a rear or interior side lot line;

310.12 Screening

- (1) Along a front or exterior side yard, parking located within an apartment building more than 0.6 metres but less than 1.50 metres above finished grade at building frontage shall be screened from streets by vegetation and landscaping.
- (2) The following shall be screened in accordance with Section 8:
 - (k) Off-street parking and loading areas;
 - (l) Above ground utility boxes and utility transformers;
 - (m) Garbage or recycling compounds and collection areas, unless enclosed in a building;
 - (n) C 7.1 zoned parcels from abutting Residential zoned parcels; and
 - (o) Parcels abutting land within the Agricultural Land Reserve, in accordance with specifications in Appendix B1.

310.13 Off-Street Vehicle Parking and Loading

- (1) Off-street vehicles parking and loading shall be provided in accordance with Section 6;
- (2) No more than one driveway per street frontage shall be permitted on a parcel;
- (3) Internal driveways shall not exceed 6.0 metres in width.

310.14 Off-Street Bicycle Parking

- (1) Off-street parking for bicycles shall be provided in accordance with Section 7; and
- (2) Notwithstanding Section 7, Class II bicycle rooms may be provided within individual storage units located on either the ground level or underground parkade level of an apartment building with direct access to the outdoors.

310.15 Other Requirements:

- (1) Overhead wiring shall not be permitted on a parcel. All new services on a parcel shall be placed underground.
- (2) Unoccupied open spaces shall be fully and suitably landscaped with landscape material; this does not include environmentally sensitive areas and required buffers where native planting is to be left undisturbed, including watercourse setbacks as specified in Section 5.19.
- (3) All buildings shall conform to Section 5.19, Watercourse Regulations.

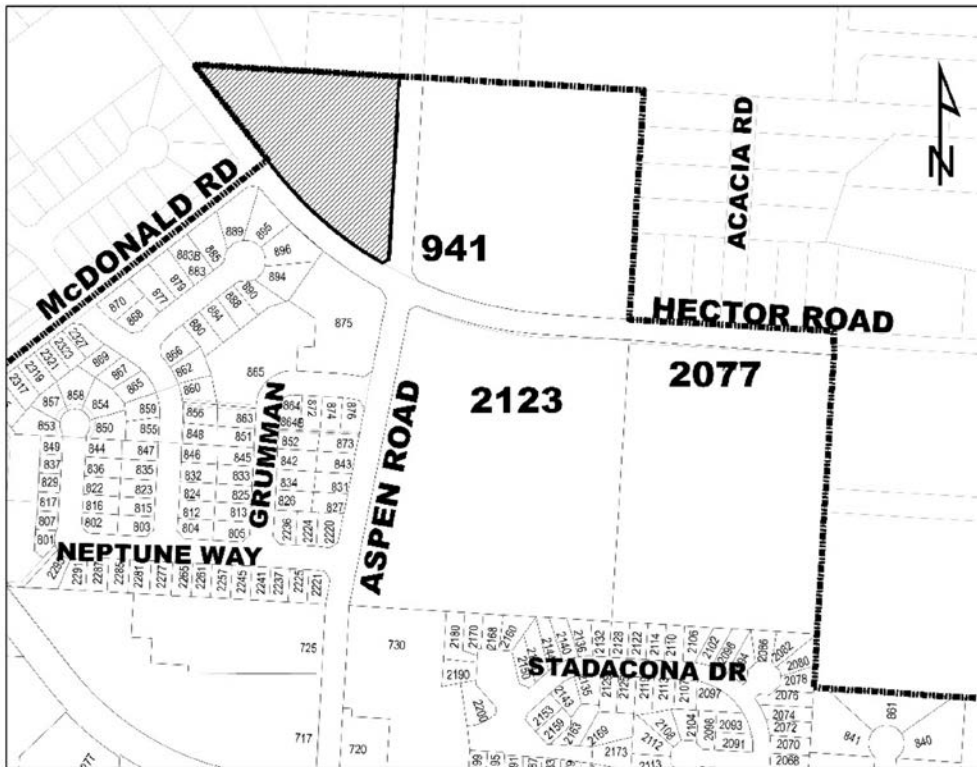
BYLAW 1850.46

SCHEDULE "2A"

941 Aspen Road (Aspen-West)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



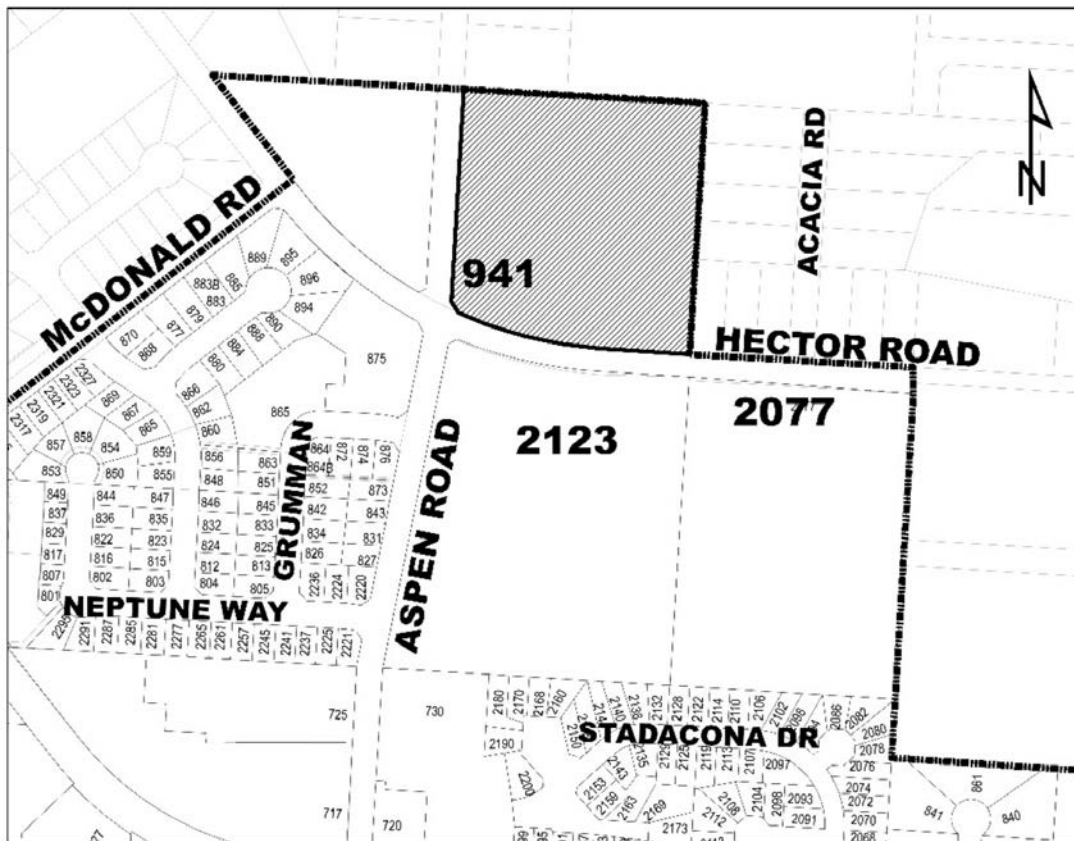
BYLAW 1850.46

SCHEDULE "2B"

941 Aspen Road (Aspen-East)

PID 023-020-113

Lot 1 DL 170 Comox District Plan VIP60685 Except Plan EPP118279



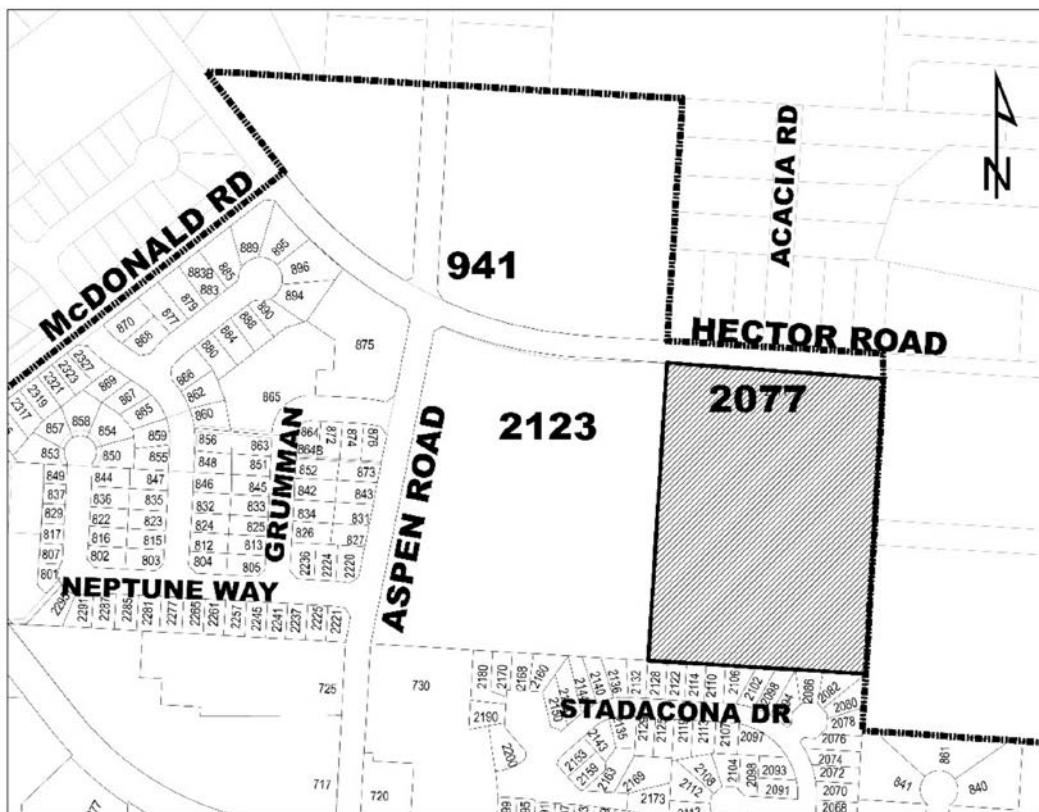
BYLAW 1850.46

SCHEDULE "2C"

2077 Hector Road

PID 003-856-704

Lot A, DL 170, Comox District, Plan 18002



Comox Subdivision and Development Servicing Bylaw, Amendment Bylaw 1261.18 Markup

TOWN OF COMOX

BYLAW 1261.18

A BYLAW TO AMEND TOWN OF COMOX SUBDIVISION AND DEVELOPMENT SERVICING BYLAW, 1261

WHEREAS Council has adopted a Subdivision And Servicing Bylaw and has the authority under the provisions of the *Community Charter* to amend the Subdivision And Development Servicing Bylaw, 1261;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. TITLE

This Bylaw may be cited for all purposes as "Comox Subdivision and Development Servicing Amendment Bylaw 1261.18".

2. AMENDMENTS

(1) Town of Comox Subdivision And Development Servicing Bylaw, 1261 is amended as follows:

(a) By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 1.4, the following text:

Notwithstanding Section 1.1 above, for sections of Aspen Road abutting parcels legally described: Lot 1 District Lot 170 Comox District Plan VIP60685 Except Plan EPP118279 (941 Aspen) and Lot 4 District Lot 170 Comox District Plan VIP60685 (2123 Hector) the minimum widths of rights-of-way and pavement and the requirements for sidewalks and curbs shall be those shown in Standard Drawing SC-17.

(b) By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 1.5, the following text:

Notwithstanding Section 1.1 above, for sections of Hector Road abutting parcels legally described: Lot A, District Lot 170, Comox District Plan 18002 (2077 Hector) and Lot 4 District Lot 170 Comox District Plan VIP60685 (2123 Hector) the minimum widths of rights-of-way and pavement and the requirements for sidewalks and curbs shall be those shown in Standard Drawing SC-18.

- (c) By adding to Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, as Standard Drawing SC-17 figure in Schedule "1", which is attached to and forms part of this bylaw.

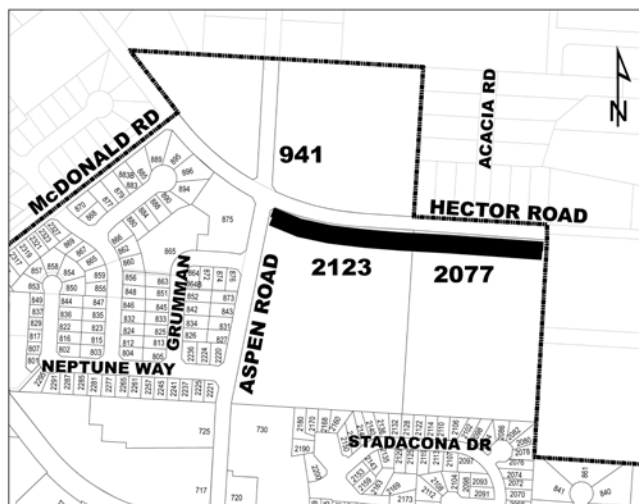
- (d) By adding to Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, as Standard Drawing SC-18 figure in Schedule "2", which is attached to and forms part of this bylaw .

- (e) By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 8.3, the following text and figure:

Hector Greenway section shown in black bold line in Figure C-3 below shall:

- (i) Be not less than 10.0 metres wide;
- (ii) Align with south side of Hector road dedication and connect to 3.0 metres wide stormwater facility maintenance roads;
- (iii) Be constructed to the following specifications:
 - minimum 3.0 metres wide multi-use asphalt path aligned so as to not be in conflict with significant trees;
 - minimum 10 significant trees for every 50 metres of greenway length, retained existing or replanted in groups to create groves;
 - irrigation 1.5" water meter on Town property with separate zones for turf and tree drip.

Figure C-3

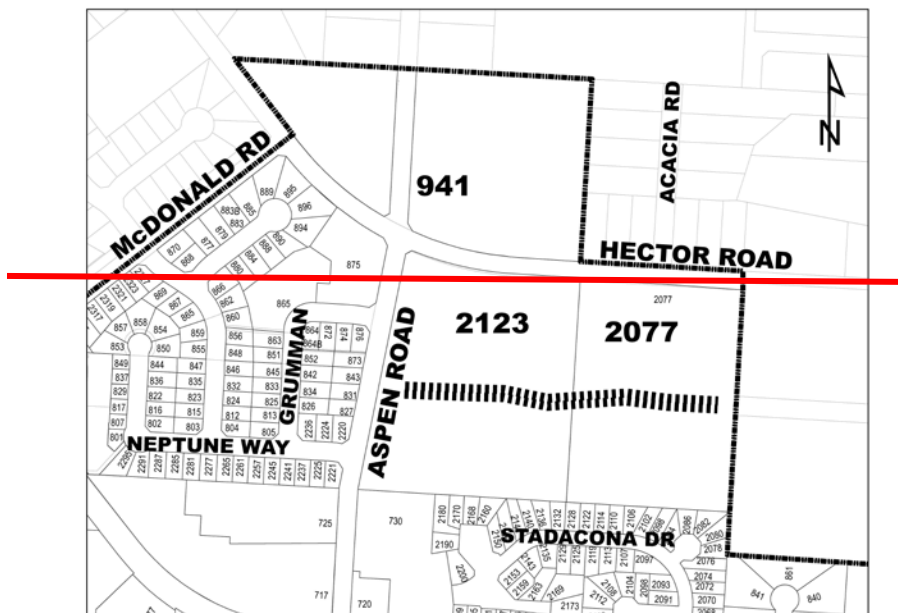


(f) ~~By adding as Schedule C.1, Appendix C, Specifications for Highways, Classification And Width, Section 8.4, the following text and figure:~~

~~Neighbourhood trail shown in black dotted line in Figure C-4 below shall:~~

- ~~(i) Be not less than 10.0 metres wide;~~
- ~~(ii) Align approximately East-West so as to not be in conflict with wetlands and environmental areas, and connect to 3.0 metres wide stormwater facility maintenance roads;~~
- ~~(iii) Be constructed to the following specifications:~~
 - ~~• minimum 4.0 metres wide multi-use asphalt path aligned so as to not be in conflict with significant trees;~~
 - ~~• minimum 10 significant trees for every 50 metres of greenway length, retained existing or replanted in groups to create groves;~~
 - ~~• irrigation 1.5" water meter on Town property with separate zones for turf and tree drip.~~

Figure C-4



- (2) "Town of Comox Subdivision And Development Servicing Bylaw, 1261" is hereby further amended by making such consequential changes as are required to reflect the foregoing amendments, including without limitation changes in the numbering and order of the sections of this bylaw.

3. DEFINITIONS

(1) In this Bylaw, unless the context otherwise requires

- (a) "Council" means the Council of the Town of Comox;

4. Adoption

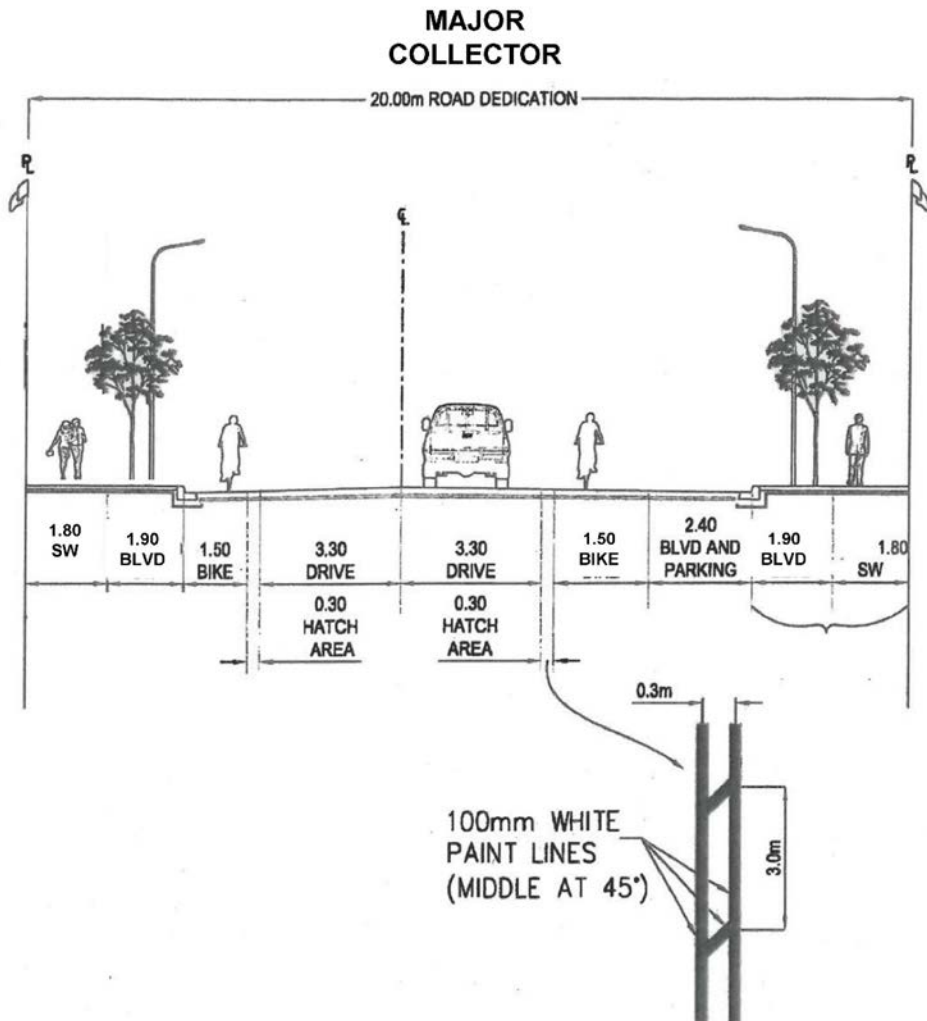
- | | |
|---------------------------------------|-----------------------------------|
| (1) READ A FIRST and SECOND time this | 5 th day of June, 2024 |
| (2) READ A THIRD time this | 5 th day of June, 2024 |
| (3) AMENDED this | ___ day of ___, 2024 |
| (4) ADOPTED this | ___ day of _____, 2024 |

Mayor

Corporate Officer

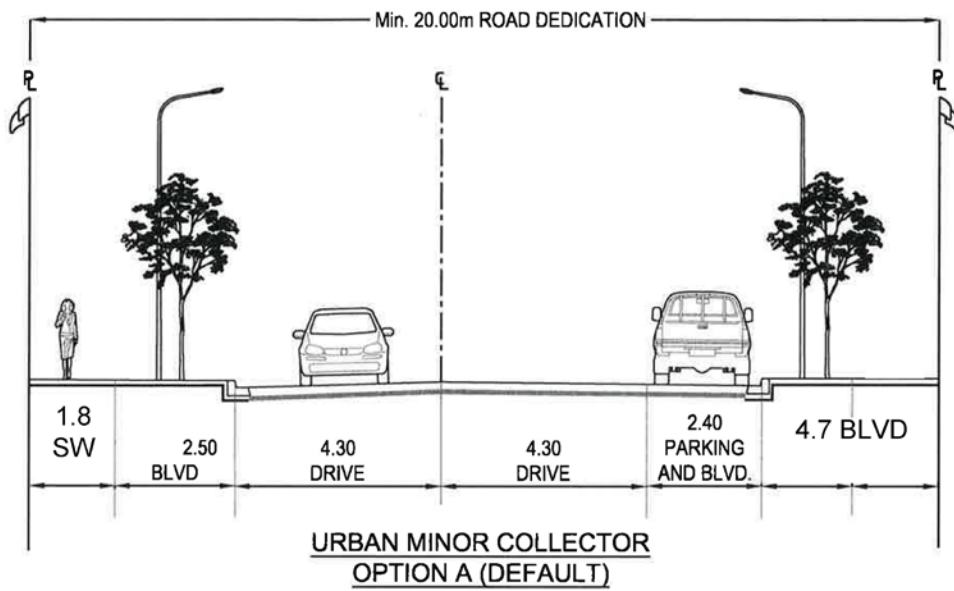
BYLAW 1261.18

SCHEDULE "1"



BYLAW 1261.18

SCHEDULE "2"



ATTACHMENT 8

Letters from Highstreet and Broadstreet confirming they support 4 storeys

Jordan Wall
Town of Comox

July 19, 2024

RE: PROPOSED OCP BYLAW AMENDMENT NO. 1685.11

Broadstreet Properties is agreed to the potential reduction in development yield that could result from the proposed OCP Bylaw Amendment NO. 1685.11 by reducing the maximum allowed height from 6 stories to 4 stories as it fits with our development intentions. Further, we agree to the removal of the required east-west path through the middle of the property that is contained within the bylaw as well.

Regards,



Kris D. Mailman
Chief Executive Officer

July 25, 2024

Town of Comox
1809 Beaufort Avenue
Comox, BC
V9M 1R9

Attn: Jordan Wall

Re: OCP Bylaw Amendment No. 1685.11

Highstreet Ventures Inc. agrees to the potential reduction in development yield that could result from amending the proposed OCP Bylaw Amendment NO. 1685.11 by reducing the maximum allowed height from 6 stories to 4 stories. Further, we agree to the removal of the required east-west path through our property that is contained within the bylaw.

If you have any questions, please let me know.

Regards,

A handwritten signature in blue ink, appearing to read 'Neil Bolton', with a stylized flourish at the end.

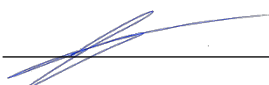
Neil Bolton
VP, Development



REGULAR COUNCIL MEETING

TO: Mayor and Council	FILE: 0340-50/1850-25
FROM: Jordan Wall, City Manager	DATE: August 7, 2024
SUBJECT: Permissive Tax Exemption Policy Discussion V.3	

Report Approved:



Jordan Wall, CAO

RECOMMENDATION(S) FROM THE CHIEF ADMINISTRATIVE OFFICER:

THAT Permissive Tax Exemption Policy CCL-025 be adopted as amended to include section 6.05, which states, "Any property that engages in behaviour which Council deems to be abhorrent, egregious, or criminal may be ineligible for future Permissive Tax Exemptions for a set period of time."

ALTERNATIVES TO THE RECOMMENDATIONS

- 1) THAT places of worship be removed from the Permissive Tax Exemption qualification list in section 4.01 of Permissive Tax Exemption Policy CCL-025

PURPOSE

To seek resolution to outstanding items in Council's Permissive Tax Exemption Policy CCL-025, specifically to seek a decision on whether places of worship will continue to receive tax exemptions.

STRATEGIC PLAN LINKAGE

Good Governance- Decision Making. Establishing a policy will provide Administration and Council direction on how to create and manage a Permissive Tax Exemption policy that is fair and transparent for the community.

BACKGROUND

On June 19th Council passed the following motion,

“THAT Administration provide a report for Council review detailing the ability to partially or fully remove a permissive tax exemption, for example, based on actions taken, regional considerations, or changes in property use.”

During the July 10th Council meeting Council resolved to remove the ability for Council to remove certain properties due to their behavior from the policy as well as began discussion on potentially removing the land value of places of worship as well. Prior to a conclusion being reached on this issue, the policy was forwarded to this meeting.

Administration is recommending language be included that will establish a process for Council to remove properties in the unlikely event that they are used for egregious, abhorrent, or criminal behavior. This standard is unlikely to ever be met but it will provide Council with more surety that action can be taken. Council could defeat the recommendation to include this in the policy and rely on the courts taking a common sense approach should Council not have this included in the policy but be required to remove a property from the approved Permissive Tax Exemption (PTE) list *and* have that decision challenged in court.

In regards to the removal of places of worship, there is precedent for the removal of these types of properties from the approved PTE list. A report conducted by the BC Humanist Society in 2018 noted 5 municipalities in BC that do not provide exemptions to the land value of places of worship and 58 that require the place of worship to pass a ‘public benefits test’ and thus is not automatically approved.

A Permissive Tax Exemption allows Council to exclude specific types of properties, through bylaw, from paying municipal taxes in the following year. Each exemption can last for a maximum of 10 years. This bylaw must be adopted by Council by October 31st of the year preceding tax collection. A PTE is a tool for Council to direct financial support and in turn provide community benefit. A PTE is based on property value and the associated taxes charged and shifts that properties tax burdens to those remaining in the taxable base. It will not result in a decrease in taxes collected.

Some property types, such as the building of a place of worship, are automatically provided a statutory permissive tax exemption by the Province. Most others, including the land value surrounding the building at a place of worship, require Council approval to be included in the permissive tax exemption list. Council may include or exclude the following types of property:

- Property owned or held by a charitable, philanthropic, and non-profit corporation and used for the purposes of the corporation
- Property owned by a local authority and used for the purposes of that local authority (e.g. property owned by Municipality X but located within the boundaries of Municipality Y)
- Properties owned or held by a public authority that is not statutorily exempt from taxes
- Properties occupied by a public authority or non-profit organization but owned by a different public authority

- Properties owned by a person (including a business, society or corporation) providing a partnering agreement, but only in relation to the provision of the agreement.
- Public parks owned and held by an athletic or service club
- Art galleries or museums owned by a charitable or philanthropic organization
- Not-for-profit seniors and community housing
- property located around a place of public worship but not eligible for a statutory exemption

Councils are given wide latitude in the types of properties they wish to provide a tax exemption. The court test for Councils in this regard is Procedural Fairness. A Procedural Fairness test is a lower test than a Correctness Test. The courts won't substitute their judgement in place of a Council but rather will just ensure that the process Council used to make the decision was fair and transparent.

Permissive Tax Exemptions do not decrease the amount of taxes collected in a given year. Through the Town's budgeting process a total amount of revenue will be determined to be collected. We will then collect those funds from the tax base. Any property exempted from being included in that tax base will result in more funds to be collected from the remaining properties, not less funds collected overall.

In 2024 the Town exempted properties which would have generated \$232,766 in tax revenue. This translates into approximately 2.1% or \$32 additional cost to the representative household.

Procedural Fairness in this context generally means that the process that Council used to make a decision was a fair and transparent one. It is not so much focused on whether or not Council made the right decision but rather that the steps taken to get to the decision were correct.


For a PTE policy, procedural fairness would likely include the types of behavior which would result in a denial of a PTE and the length in time that denial will last for. Further, it may include the ability of a representative of the property to speak with Council prior to a final decision being made.

ANALYSIS/ISSUES/IMPLICATIONS

2024 Permissive Tax Exemptions (estimated)

Note: 2024 tax rates and 2023 assessed values

Property Description	Assessed Value				Taxes Exempted		
	Assessed Value	Permissive Exemption	Statutory Exemption	Total Exemptions	Taxes exempted Permissive	Taxes exempted Statutory	Total Comox taxes
Comox United Church	1,288,000	754,000	534,000	1,288,000	7,171.72	5,079.18	12,250.90
Comox Pentecostal Church	1,177,400	550,000	627,400	1,177,400	5,231.36	5,967.56	11,198.92
St. Peter's Anglican Church	844,000	459,000	385,000	844,000	4,365.81	3,661.95	8,027.76
Bay Community Church	1,258,600	763,000	495,600	1,258,600	4,416.33	2,868.59	7,284.92
Comox Valley Presbyterian Church	1,029,800	499,000	530,800	1,029,800	4,746.27	5,048.74	9,795.01
Little Red Church	829,500	819,500	10,000	829,500	4,288.50	52.33	4,340.83
Total	6,427,300	3,844,500	2,582,800	6,427,300	30,219.99	22,678.34	52,898.33
Total Comox taxes exempted (est.) due to permissive exemptions excl. Town of Comox leased space					275,207.87		

 TOWN OF COMOX		POLICY AND PROCEDURE MANUAL	
Section: COUNCIL	Number: CCL – 025.01	Office of Primary Responsibility: CORPORATE SERVICES	
PERMISSIVE TAX EXEMPTION POLICY			
Type: <input checked="" type="checkbox"/> Policy <input checked="" type="checkbox"/> Procedure		Authority: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Administrative	
Date Adopted: October 6, 2004		Date Last Amended: 	
		Approved By: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Chief Administrative Officer <input type="checkbox"/> Department Head	
		Date to be Reviewed: December, 2025	
Manner Issued: Website, Internal Memo, Upon request			

1 PURPOSE

1.01 To Provide a framework for Permissive Tax Exemption Policies

2 POLICY STATEMENT

2.01 The Council of the Town of Comox recognizes that local not for profit societies are valuable resources in helping the municipality provide a vibrant community. The Town will provide Permissive Tax Exemptions (PTEs) on an ongoing to basis to approved properties unless otherwise directed by Council.

3 POLICY PRINCIPLES

3.01 Council will consider the provision PTEs for any property which is approved by Section 7 Division 7 of the Community Charter.

4 SCOPE

4.01 All PTE requests that are submitted to the Town of Comox including and not limited to community groups, non-profits, schools, youth groups, and other governmental organizations.

5 POLICY

5.01 Council will review all Permissive Tax Exemption requests submitted by qualified applicants. Any applicant that is approved for a PTE will continue to receive a property tax exemption until and including 2034 unless disqualified under section 6.03 or until this policy is amended or appealed.

6 PROCEDURES

6.01 Council will accept Permissive Tax Exemption applications from qualified applicants prior to May 31st of each year.

6.02 Applications must include the following information:

- Copy of certificate of society or non-profit registration.
- In the case of a lease agreement for premises rather than ownership, documents are required which indicate that the applicant will benefit by the exemption. Documents should demonstrate that the lease is currently, or will, on approval of the exemption, be reduced by the amount of the exemption, or that other considerations will be provided by the landlord equivalent to the value of the exemption.
- Description of programs/services/benefits delivered from the subject lands/improvements including participant numbers, volunteer hours, benefiting groups/individuals/special needs populations, fees charged for participation
- Description of any 3rd party use of the subject land/improvements including user group names, fees charged conditions of use.
- Information as to the extent to which the activities of the applicant are regional or local (within the Town of Comox) in nature.
- Financial information on how the tax exemption amount is put back into the community through charitable means or reduced fees paid by the general population of the Town of Comox.
- Confirmation that the organization's activities do not compete with any other duly licensed business in the Municipality.



6.03 Any property that undergoes a change to use or any substantial change to the information submitted in section 6.02 must inform the Town and may be required to apply for a new PTE.

6.04 All properties approved will be granted a 100% exemption or maximum allowable under legislation.

6.05 Any property that engages in behaviour which Council deems to be abhorrent, egregious, or criminal may be ineligible for future Permissive Tax Exemptions for a set period of time

Amendment Date	Section Amended or Description of Amendment	Resolution Number
Oct 6, 2004	CCL-025.00. Policy established as Council Policy No. CCL-025 Property Tax Exemption Policy.	2004-420
Aug 7, 2024	CCL-025.01. Policy renamed to Permissive Tax Exemption Policy and rewritten to restrict exemptions to "qualified" applicants.	

TO: Mayor and Council	FILE: PR 24-10
FROM: Elliot Turnbull, Planner II	DATE: August 7, 2024
SUBJECT: Planning Report PR 24-10 Amend Building Bylaw to delete Form VIII Owner's Declaration of Secondary Suite and Form IX Owner's Declaration of Coach House	

Prepared by:  _____ <i>Elliot Turnbull, Planner II</i>	Supervisor: _____	Report Approved:  _____ <i>Jordan Wall, CAO</i>
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RECOMMENDATION(S) FROM THE CHIEF ADMINISTRATIVE OFFICER:

That Comox Building Bylaw Amendment No. 1472.14 be given First, Second, and Third Reading.

PURPOSE/PROPOSAL

The purpose of this report is to amend Comox Building Bylaw 1472 to align building permit requirements with recent amendments to Comox Zoning Bylaw 1850 respecting secondary suite and coach house regulations.

REPORT SUMMARY

Subject Property/Area:

All residential properties within Town

Key Issues:

**Provincial
SSMFH**

Comox Zoning Bylaw 1850 was recently updated to implement the provincial Small-Scale Multi-Family Housing program, allowing up to four units on most residential properties in Town. As part of this program, several zoning regulations have been removed in respect of secondary suites and coach houses, which will provide greater flexibility for owners who wish to provide additional density on their properties.

**Building
Bylaw
Requirements**

To obtain occupancy for a secondary suite or a coach house, Comox Building Bylaw 1472 requires submission of a signed and notarized form (called Form VIII and IX in the Bylaw) which states that property owners will operate their secondary suite or coach house in accordance with a list of regulations that were taken directly from the zoning bylaw of the time. As most of these regulations no longer exist in the Zoning Bylaw, Forms VIII and IX and now inconsistent with the Town's regulations and it is proposed to delete the requirement for these forms from the Building Bylaw.

**Reduce
processing
times**

Removing the requirement for signed and notarized forms prior to occupancy for a secondary suite or coach house will reduce processing times for a building permit and remove the cost of having a form notarized for property owners wishing to provide additional units.

Council Decision

To give 1st, 2nd, and 3rd Reading to Bylaw No. 1472.14.

Decision options	Implications
<p style="color: red; font-weight: bold;">Recommended</p> <div style="border: 1px dashed red; padding: 5px; margin-bottom: 10px;"> <p>1. Council gives 1st, 2nd, and 3rd Reading</p> </div> <p>2. Alternative 1 - Council amends the bylaw and gives 1st, 2nd, and 3rd Reading</p> <p>3. Alternative 2 - Council does not give 1st, 2nd, and 3rd Reading.</p>	<p>Bylaw will be brought back to Council at the next Regular Council Meeting for adoption.</p> <p>Council may amend any aspects of Bylaw 1472.14 including leaving the requirement for Form VIII and IX but changing the contents of the form to align with existing zoning regulations.</p> <p>No changes to Comox Building Bylaw 1472 and Forms VIII and IX will continue to be a requirement for property owners who are applying for occupancy for a secondary suite or coach house.</p>

STRATEGIC PLAN LINKAGE

This report addresses the following strategic priorities identified in the 2022-2026 Strategic Plan:

Strategic Priority	Areas of Focus
Organizational Excellence	Internal processes - Streamline and develop new processes that are efficient, cost effective and support our overall objectives.
Good Governance	Decision Making - We make evidence-based decisions focused on the best interests of the Town over the long term.

Balanced Community Planning	Housing - We will create the conditions for a diversity of housing options in our unique seaside Town.
------------------------------------	---

BACKGROUND

At the June 19th, 2024 Regular Council Meeting, Council adopted Bylaw 1850.47 which amended Comox Zoning Bylaw 1850 to implement the provincial Small-Scale Multi-Family Housing program, allowing up to four units on most residential properties in Town. As part of this program, several zoning regulations have been removed in respect of secondary suites and coach houses, which will provide greater flexibility for owners who wish to provide additional density on their properties.

ZONING BYLAW IMPLICATIONS

Prior to June 19th, 2024, the Zoning Bylaw contained several regulations related to Secondary Suites and Coach Houses which are shown in Attachment 2 and summarized below:

1. Limited to one coach house one secondary suite per parcel, not both;
2. Only permitted on parcels where a single-family dwelling exists;
3. Secondary suites only permitted in single-family dwellings;
4. Requirement of owner occupation in either the principal dwelling or the secondary suite or coach house;
5. Coach houses only permitted in a rear yard.

These regulations were mostly repealed at the June 19th, 2024 RCM. See Attachment 2 for a full comparison of the regulations before and after June 19th.

BUILDING BYLAW IMPLICATIONS

Comox Building Bylaw 1472 contains two forms (Forms VIII and IX) which are required to be signed and notarized by property owners who are seeking occupancy for a secondary suite or a coach house. Forms VIII and IX and included in Attachment 3.

These two forms duplicate all the zoning requirements discussed above and other requirements which are already in the Building Bylaw. The forms were intended to strengthen the property owner’s awareness of the Zoning Bylaw regulations associated with secondary suites or coach houses, particularly the requirement for owner-occupation of one of the dwelling units. At the time, this was seen as a critical element of public support for these housing forms.

Given that most of the regulations discussed above, including the requirement for owner occupation, have been repealed, and most single-family properties in Town permit densities greater than that of just a secondary suite or a coach house, Form VIII and IX are inconsistent with Town regulations and are proposed to be removed from the Building Bylaw.

Removing the requirement for these forms will reduce processing time and costs by eliminating the need for property owners who are applying for occupancy for a secondary suite or coach house to have a form notarized.

ET

Attachments:

1. Comox Building Bylaw Amendment No. 1472.14
2. Comparison of Secondary Suite/Coach House Zoning Regulations Pre-and Post-June 19th
3. Form VIII and IX excerpted from Comox Building Bylaw 1472

Attachment 1
Comox Building Bylaw Amendment No. 1472.14

TOWN OF COMOX

BYLAW NO. 1472.14

A BYLAW TO AMEND TOWN OF COMOX BUILDING BYLAW No. 1472

WHEREAS Council has adopted a Building Bylaw and has the authority under the provisions of the *Community Charter* to amend the Building Bylaw;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. Title

This Bylaw may be cited for all purposes as “Comox Building Bylaw Amendment No. 1472.14”.

2. Amendments

“Comox Building Bylaw 1472” is amended as follows:

- (1) Section 18 Occupancy Permits is amended by deleting sub-section (c);
- (2) Section 32 Forms and Schedules is amended by replacing sub-section (1) with the following:
 - (1) Forms I through VII and Schedules 1, 2 and 3 form a part of this Bylaw.
- (3) Forms VII and IX are deleted.
- (4) Comox Building Bylaw 1472 is further amended by making such consequential changes as are required to reflect the foregoing amendments, including without limitation changes in the numbering and order of the sections of the bylaw.

3. Adoption

- (1) READ A FIRST and SECOND time this ___day of ____, 2024
- (2) READ A THIRD time this ___day of ____, 2024
- (3) ADOPTED this ___day of ____, 2024

Mayor

Corporate Officer

Attachment 2

Comparison of Secondary Suite/Coach House Zoning Regulations Pre-and Post-June 19th

ZONING REGULATIONS PRIOR TO JUNE 19, 2024:

Secondary Suite

A dwelling unit that is secondary to another dwelling unit. Single-family dwellings containing a secondary suite shall be owner occupied.

Secondary suites shall:

- 1) be located only in single-family dwellings;
- 2) be limited to one (1) per single-family dwelling;
- 3) be completely contained within the principal building;
- 4) have a dwelling unit gross floor area not exceeding 40% of the gross floor area of the principal building or 90 m², whichever is the lesser;
- 5) have their own entrance separate from that of the other dwelling unit; and
- 6) not be located on a parcel, on which a coach house exists.

Coach House

A building consisting of one dwelling unit or one modular unit. Coach houses shall:

- 1) Only be permitted on a parcel on which a single family dwelling exists;
- 2) Only be permitted on a parcel where either the single family dwelling or the coach house is owner occupied;
- 3) Be located in a rear yard; and
- 4) Be limited to one (1) per parcel.

ZONING REGULATIONS AFTER JUNE 19, 2024:

Secondary Suite

A dwelling unit that is secondary to another dwelling unit. Secondary suites shall:

- (1) be located in a single-family dwelling unit or a two-family dwelling unit;
- (2) be completely contained within a principal building; and
- (3) have its own entrance separate from that of the other dwelling unit.

Coach House *(only permitted on properties which retain their R1.1 zone or large lots >4050 m² or lots which are not serviced by municipal sewer or water)*

A building consisting of one dwelling unit or one modular unit.

Coach houses shall:

- (1) Only be permitted on a parcel on which a single-family dwelling exists;
- (2) Be located in a rear yard; and
- (3) Be limited to one per parcel.

SSMFH Coach House *(permitted in the new R1.0 zone, which comprised most of the residential zoned lots in Town)*

Means a dwelling unit or modular unit located within a building that is subordinate to a principal building on the parcel.

Attachment 2

Forms VIII and IX

FORM VIII (#1903 Mar 20/19)

OWNER'S DECLARATION OF SECONDARY SUITE

BUILDING PERMIT NUMBER: _____

DATE: _____

Please Print

I/We, _____ being the registered
NAME OF REGISTERED PROPERTY OWNER(S)

Property Owner and residing at _____, have a suite
PROPERTY ADDRESS

located _____, and hereby declare:
LOCATION OF SECONDARY SUITE WITHIN THE DWELLING

1. That I understand that the suite located within the dwelling, as stated above, can be occupied and used as a secondary suite only in accordance with the following provisions of Comox Zoning Bylaw 1850:
 - A. Section 3.2 Definitions:

"Secondary Suite: a dwelling unit that is secondary to another dwelling unit. Single-family dwellings containing a secondary suite shall be owner occupied. Secondary suites shall:

 - (1) Be located only in single-family dwellings;
 - (2) Be limited to one (1) per single-family dwelling;
 - (3) Be completely contained within the principal buildings;
 - (4) Have a dwelling unit gross floor area not exceeding 40% of the gross floor area of the principal building, or 90 m² whichever is the lesser; and
 - (5) Have their own entrance separate from that of the other dwelling unit."
 - B. Section 6 Off-Street Parking and Loading:

Two individually accessible off-street parking spaces must be provided.
2. That I understand that the registered owner(s) of the property must occupy either the primary dwelling unit, or the secondary suite, as their permanent residence.
3. That I understand that the secondary suite must be constructed in accordance with the requirements of the British Columbia Building Code.
4. That I understand that no person may occupy a secondary suite and no person shall permit or suffer a secondary suite to be occupied, unless an occupancy permit has been issued under Comox Building Bylaw 1472.

Signature of owner(s)

Address of Owner(s)

{ DECLARED BEFORE ME in _____
 { _____ (municipality)
 { in the Province of British Columbia, on _____
 { _____ (date),
 { _____ (signature)
 { a commissioner for taking affidavits for British
 { Columbia / a Notary Public in and for the
 { Province of British Columbia.

FORM IX (#1903 Mar 20/19)

OWNER'S DECLARATION OF COACH HOUSE

BUILDING PERMIT NUMBER: _____

DATE: _____

Please Print

I/We, _____ being the registered
NAME OF REGISTERED PROPERTY OWNER(S)

Property Owner and residing at _____, have a coach house
PROPERTY ADDRESS

located _____, and hereby declare:
LOCATION OF COACH HOUSE ON THE PROPERTY

1. That I understand that the coach house can be located on the property as stated above, and be occupied and used as a coach house only in accordance with the following provisions of Comox Zoning Bylaw 1850:

A. Section 3.2 Definitions:

“Coach House: a building consisting of one dwelling unit or one modular unit.

Coach houses shall:

- (1) Only be permitted on a parcel on which a single family dwelling exists;
- (2) Only be permitted on a parcel where either the single family dwelling or the coach house is owner occupied;
- (3) Be located only in a rear yard; and
- (4) Be limited to one (1) per parcel.”

B. Section 6 Off-Street Parking and Loading:

Two individually accessible off-street parking spaces to the maximum aggregate width of 6.20 metres must be provided.

2. That I understand that the registered owner(s) of the property must occupy either the primary dwelling unit, or the coach house, as their permanent residence.

3. That I understand that the coach house must be constructed in accordance with the requirements of the British Columbia Building Code.

4. That I understand that no person may occupy a coach house and no person shall permit or suffer a coach house to be occupied, unless an occupancy permit has been issued under Comox Building Bylaw 1472.

Signature of owner(s)



{ DECLARED BEFORE ME in _____
{ _____ (municipality)
{ in the Province of British Columbia, on
{ _____

Address of Owner(s)

_____ (date),
{ _____ (signature)
{ a commissioner for taking affidavits for British
{ Columbia / a Notary Public in and for the
{ Province of British Columbia.

REGULAR COUNCIL MEETING

TO: Mayor and Council	FILE: 1830-30
FROM: Edward Henley, Director of Finance	DATE: August 1, 2024
SUBJECT: 2023 Statement of Financial Information (SOFI)	

Prepared by:  Edward Henley, Director of Finance	Report Approved:  Jordan Wall, CAO
--	---

RECOMMENDATION(S) FROM THE CHIEF ADMINISTRATIVE OFFICER:

THAT the 2023 Statement of Financial Information report be approved.

PURPOSE

To present the Statement of Financial Information for the Town of Comox for the year ended December 31, 2023.

STRATEGIC PLAN LINKAGE

Core Services: Financial management and accountability

BACKGROUND

At the [May 8, 2024, meeting](#), Council approved the 2023 audited financial statements.

The [Financial Information Act, Financial Information Regulation, Schedule 1](#) states that within six months after the municipality's fiscal year end, a statement of financial information must be prepared which includes:

- a) the audited financial statements, including a management letter;
- b) schedule of guarantee and indemnity agreements;
- c) schedule of remuneration and expenses for elected officials;
- d) schedule of remuneration and expenses for employees with remuneration greater than \$75,000 per year. Expenses for employees are those that are required to undertake their duties including professional association dues, ongoing training and professional requirements for professional development, and related travel costs;

- e) schedule of severance agreements;
- f) schedule of payments to suppliers of goods or services totalling more than \$25,000 each;
- g) schedule of grant payments.

[Section 377 of the Local Government Act](#) requires that a municipality follow the provisions of [Section 168 of the Community Charter](#), which states that at least once a year, a council must have prepared a report separately listing the following for each council member by name:

- a) the total amount of remuneration paid to the council member for discharge of the duties of office, including any amount specified as an expense allowance;
- b) the total amount of expense payments for the council member made to the board member as reimbursement for expenses incurred by the council member or as an allowance that is not reported under paragraph (a);
- c) the total amount of any benefits, including insurance policies and policies for medical or dental services, provided to the council member or the member's dependants.
- d) any contracts reported under [Section 107](#) including a general description of their nature.

The 2023 Statement of Financial Information will be posted on the [website](#).



TOWN OF COMOX


Statement of Financial Information Year Ended December 31, 2023

Town of Comox
Statement of Financial Information
Table of Contents
For the year ended December 31, 2023

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Consolidated Financial Statements and Notes	5
Schedule of Debts	Notes 4 to 6 of the Consolidated Financial Statements
Schedule of Guarantee and Indemnity Agreements	N/A - none outstanding during year
Schedule of Remuneration & Expenses for Elected Officials	31
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Town of Comox
Statement of Financial Information
Statement of Financial Information Approval
For the year ended December 31, 2023

I certify that I have prepared the whole of the Statement of Financial Information and am responsible for its contents. It has not been audited by the Town's independent auditors.

_____ 
Edward Henley, Director of Finance Date

This whole Statement of Financial Information has been approved by the Town of Comox Council.

_____ 
Nicole Minions, Mayor Date

Town of Comox

Management Report

The Financial Statements contained in this Statement of Financial Information under the *Financial Information Act* have been prepared by management in accordance with Canadian Public Sector Accounting Standards and are outlined under "Significant Accounting Policies" in the notes to the financial statements, and the integrity and objectivity of these statements and schedules are management's responsibility. Management is also responsible for all of the statements and schedules, and for ensuring that this information is consistent, where appropriate, with the information contained in the financial statements.

Management is also responsible for implementing and maintaining a system of internal controls to provide reasonable assurance that reliable financial information is produced.

The Council of the Town of Comox is responsible for ensuring that management fulfills its responsibilities for financial reporting and internal control.

The external auditors, MNP LLP, conduct an independent examination, in accordance with Canadian Public Sector accounting standards, and express their opinion on the financial statements. Their examination does not relate to the other schedules and statements required by the Act. Their examination includes a review and evaluation of the corporation's system of internal control and appropriate tests and procedures to provide reasonable assurance that the financial statements are presented fairly.

Edward Henley
Director of Finance

To the Mayor and Council of the Town of Comox:

Opinion

We have audited the consolidated financial statements of Town of Comox (the "Town"), which comprise the consolidated statement of financial position as at December 31, 2023, and the consolidated statements of operations, remeasurement gains and losses, changes in net financial assets and cash flows for the year then ended, and notes to the consolidated financial statements, including a summary of significant accounting policies.

In our opinion, the accompanying consolidated financial statements present fairly, in all material respects, the consolidated financial position of the Town as at December 31, 2023, and the results of its consolidated operations, its consolidated remeasurement gains and losses, changes in its net financial assets and its consolidated cash flows for the year then ended in accordance with Canadian public sector accounting standards.

Basis for Opinion

We conducted our audit in accordance with Canadian generally accepted auditing standards. Our responsibilities under those standards are further described in the Auditor's Responsibilities for the Audit of the Consolidated Financial Statements section of our report. We are independent of the Town in accordance with the ethical requirements that are relevant to our audit of the consolidated financial statements in Canada, and we have fulfilled our other ethical responsibilities in accordance with these requirements. We believe that the audit evidence we have obtained is sufficient and appropriate to provide a basis for our opinion.

Other Information

Management is responsible for the other information, consisting of an annual report, which is expected to be made available to us after the date of this auditor's report.

Our opinion on the consolidated financial statements does not cover the other information and we do not express any form of assurance conclusion thereon.

In connection with our audit of the consolidated financial statements, our responsibility is to read the other information and, in doing so, consider whether the other information is materially inconsistent with the consolidated financial statements or our knowledge obtained in the audit or otherwise appears to be materially misstated.

When we read the annual report, if we conclude that there is a material misstatement therein, we are required to communicate the matter to those charged with governance.

Responsibilities of Management and Those Charged with Governance for the Consolidated Financial Statements

Management is responsible for the preparation and fair presentation of the consolidated financial statements in accordance with Canadian public sector accounting standards, and for such internal control as management determines is necessary to enable the preparation of consolidated financial statements that are free from material misstatement, whether due to fraud or error.

In preparing the consolidated financial statements, management is responsible for assessing the Town's ability to continue as a going concern, disclosing, as applicable, matters related to going concern and using the going concern basis of accounting unless management either intends to liquidate the Town or to cease operations, or has no realistic alternative but to do so.

Those charged with governance are responsible for overseeing the Town's financial reporting process.

Auditor's Responsibilities for the Audit of the Consolidated Financial Statements

Our objectives are to obtain reasonable assurance about whether the consolidated financial statements as a whole are free from material misstatement, whether due to fraud or error, and to issue an auditor's report that includes our opinion. Reasonable assurance is a high level of assurance, but is not a guarantee that an audit conducted in accordance with Canadian generally accepted auditing standards will always detect a material misstatement when it exists. Misstatements can arise from fraud or error and are considered material if, individually or in the aggregate, they could reasonably be expected to influence the economic decisions of users taken on the basis of these consolidated financial statements.

As part of an audit in accordance with Canadian generally accepted auditing standards, we exercise professional judgment and maintain professional skepticism throughout the audit. We also:

- Identify and assess the risks of material misstatement of the consolidated financial statements, whether due to fraud or error, design and perform audit procedures responsive to those risks, and obtain audit evidence that is sufficient and appropriate to provide a basis for our opinion. The risk of not detecting a material misstatement resulting from fraud is higher than for one resulting from error, as fraud may involve collusion, forgery, intentional omissions, misrepresentations, or the override of internal control.
- Obtain an understanding of internal control relevant to the audit in order to design audit procedures that are appropriate in the circumstances, but not for the purpose of expressing an opinion on the effectiveness of the Town's internal control.
- Evaluate the appropriateness of accounting policies used and the reasonableness of accounting estimates and related disclosures made by management.
- Conclude on the appropriateness of management's use of the going concern basis of accounting and, based on the audit evidence obtained, whether a material uncertainty exists related to events or conditions that may cast significant doubt on the Town's ability to continue as a going concern. If we conclude that a material uncertainty exists, we are required to draw attention in our auditor's report to the related disclosures in the consolidated financial statements or, if such disclosures are inadequate, to modify our opinion. Our conclusions are based on the audit evidence obtained up to the date of our auditor's report. However, future events or conditions may cause the Town to cease to continue as a going concern.
- Evaluate the overall presentation, structure and content of the consolidated financial statements, including the disclosures, and whether the consolidated financial statements represent the underlying transactions and events in a manner that achieves fair presentation.

We communicate with those charged with governance regarding, among other matters, the planned scope and timing of the audit and significant audit findings, including any significant deficiencies in internal control that we identify during our audit.

Nanaimo, British Columbia

May 8, 2024

MNP LLP

Chartered Professional Accountants

Town of Comox
 Consolidated Statement of Financial Position
 as at December 31, 2023

	2023	2022
Financial Assets:		
Cash & Cash equivalents	\$ 2,370,240	\$ 10,914,514
Short-term investments (Note 2c)	49,384,766	37,619,045
Receivables (Note 3)	3,009,101	1,479,547
Land for resale	9,847	9,847
	<u>54,773,954</u>	<u>50,022,953</u>
Financial Liabilities:		
Payables (Note 4)	4,481,407	3,976,779
Deferred revenue (Schedule 2)	1,698,286	1,605,064
Restricted revenue (Schedule 2)	8,272,529	8,049,856
Deposits	3,374,743	3,437,103
Net debenture debt (Note 5)	-	20,371
Equipment Financing (Note 6)	1,699,547	-
Future payroll obligations (Note 9)	1,084,184	1,131,611
Asset Retirement Obligations (Note 17)	275,402	-
	<u>20,886,098</u>	<u>18,220,784</u>
Net Financial Assets	<u>33,887,856</u>	<u>31,802,169</u>
Non-financial assets:		
Tangible capital assets (Schedule 4)	99,067,651	89,754,706
Supply inventory	260,753	280,716
Prepaid expenses	142,079	86,070
Shares in Courtenay Golf Club Ltd.	8,750	8,750
	<u>99,479,233</u>	<u>90,130,242</u>
Accumulated surplus	<u>\$ 133,367,089</u>	<u>\$ 121,932,411</u>
Accumulated Surplus consists of:		
Accumulated surplus (Note 16)	133,278,466	122,093,344
Accumulated rereasurement gain (loss)	88,623	(160,933)
Accumulated surplus	<u>\$ 133,367,089</u>	<u>\$ 121,932,411</u>

Contingent liabilities (Note 7)
 Commitment (Note 19)



Edward Henley, Director of Finance, responsible for financial administration.

The accompanying notes are an integral part of these consolidated financial statements.

Town of Comox
 Consolidated Statement of Operations
 Year Ended December 31, 2023

	Budget (Note 15)	2023	2022
Revenue			
Property taxation	\$ 14,045,739	\$ 14,125,882	\$ 12,813,373
Sale of services	12,645,770	13,686,804	11,262,306
Government transfers	5,960,054	7,993,292	3,650,755
Investment income	400,000	1,916,700	925,021
Development contributions	79,500	427,543	685,908
Gain on disposal of tangible capital assets	-	205,282	51,332
Other	145,000	148,744	126,063
	33,276,063	38,504,247	29,514,758
Expenses			
General government	3,953,051	3,822,517	3,506,695
Protective services	4,694,382	4,672,379	4,145,253
Solid waste management	1,806,965	1,863,581	1,591,869
Affordable Housing	-	35,267	17,634
Development services	754,309	687,202	763,926
Transportation	4,185,706	4,512,611	4,267,171
Parks, recreation, and culture	5,533,025	5,439,315	4,944,143
Marina	292,930	283,581	350,981
Water services	3,411,701	3,166,185	2,515,835
Sewer services	3,507,507	2,836,487	2,604,212
	28,139,576	27,319,125	24,707,719
Annual surplus	5,136,487	11,185,122	4,807,039
Accumulated surplus, beginning of year	122,093,344	122,093,344	117,286,305
Accumulated surplus, end of year	\$ 127,229,831	\$ 133,278,466	\$ 122,093,344

The accompanying notes are an integral part of these consolidated financial statements.

Town of Comox
 Consolidated Statement of Remeasurement Gains and Losses
 Year Ended December 31, 2023

	2023	2022
Accumulated remeasurement losses, beginning of the year	\$ (160,933)	\$ -
Unrealized Gain (loss) attributable to portfolio investments	249,556	(160,933)
Accumulated remeasurement gains (loss), end of year	\$ 88,623	\$ (160,933)

The accompanying notes are an integral part of these consolidated financial statements.

Town of Comox

Consolidated Statement of Change in Net Financial Assets

For the Year Ended December 31, 2023

	Budget	2023	2022
	(Note 15)		
Annual surplus	\$ 5,136,487	\$ 11,185,122	\$ 4,807,039
Acquisition of tangible capital assets	(20,502,214)	(12,181,724)	(6,173,949)
Increase in tangible capital assets due to asset retirement obligations		(251,134)	
Tangible capital assets contributed	-	(422,486)	-
Additions of Capital Assets	(20,502,214)	(12,855,344)	(6,173,949)
Amortization of tangible capital assets	3,300,000	3,542,399	3,278,188
	(12,065,727)	1,872,177	1,911,278
Unrealized gain (loss) on investments	-	249,556	(160,933)
Decrease / (increase) in supply inventory	-	19,963	(41,553)
(Increase) in prepaid expenses	-	(56,009)	(50,037)
	-	213,510	(252,523)
Increase (decrease) in Net Financial Assets	(12,065,727)	2,085,687	1,658,755
Net Financial Assets at beginning of year	31,802,169	31,802,169	30,143,414
Net Financial Assets at end of year	\$ 19,736,442	\$ 33,887,856	\$ 31,802,169

The accompanying notes are an integral part of these consolidated financial statements.

Town of Comox
Consolidated Statement of Cash Flows
as at December 31, 2023

	2023	2022
Net inflow (outflow) of cash related to the following activities:		
Operating activities:		
Annual surplus	\$ 11,185,122	\$ 4,807,039
Non-cash items included in operations:		
Contribution from developers	(422,486)	-
Amortization of tangible capital assets	3,542,399	3,278,188
Accretion	24,268	-
Disposition of tangible capital assets	-	-
Actuarial adjustment on debt	(11,907)	(20,754)
Unrealized loss on investments	249,556	(160,933)
Net changes to financial assets & liabilities through operations:		
(Increase) / decrease in receivables	(1,529,554)	454,744
Increase in payables	504,628	606,038
Increase in deferred revenue	93,222	81,885
Increase / (decrease) in restricted revenue	222,673	(1,767,096)
(Increase) in other deposits	(62,360)	(1,059,577)
(Decrease) in future employee obligations	(47,427)	(42,084)
Decrease / (increase) in supply inventory	19,963	(41,553)
(Increase) in prepaid expenses	(56,009)	(50,037)
	13,712,088	6,085,860
Investing Activities		
Short-term investing	(11,765,721)	4,081,177
Capital Activities		
Acquisition of tangible capital assets	(12,181,724)	(6,173,949)
Financing activities:		
Debenture principal repaid	(8,464)	(23,225)
Long-term debt borrowing	1,804,024	
Long-term debt repayment	(104,477)	
	1,691,083	(23,225)
(Decrease) / increase in cash and cash equivalents	(8,544,274)	3,969,863
Cash and cash equivalents, beginning of year	10,914,514	6,944,651
	\$ 2,370,240	\$ 10,914,514
Supplemental cash flow information		
Interest paid	\$ 77,386	\$ 21,355
Interest received	\$ 1,916,700	\$ 925,021

The accompanying notes are an integral part of these consolidated financial statements.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

1. PURPOSE

The Town of Comox was incorporated in 1946 to provide municipal services to its residents, and is governed by the Community Charter and the Local Government Act of British Columbia.

2. SIGNIFICANT ACCOUNTING POLICIES

(a) British Columbia Municipalities

It is the policy of the Town to follow accounting principles generally accepted for British Columbia municipalities and to apply such principles consistently. As part of this policy, the resources and operation of the Town are segregated into various funds for accounting and financial reporting purposes, each being treated as a separate entity with responsibility for the stewardship of the assets allocated to it.

The consolidated financial statements are prepared in accordance with recommendations of the Public Sector Accounting Board (PSAB). The consolidated financial statements include the financial position, financial operations, change in remeasurement gains and losses, change in net financial assets, and change in cash flows of the one economic entity of the Town of Comox. Inter-fund transactions and fund balances have been eliminated on consolidation.

(b) Investments and Financial Instruments

Financial instruments are classified into two categories fair value or cost.

- (i) Fair value category: investments quoted in an active market are reflected at fair value as at the reporting date. Sales and purchases of investments are recorded on the trade date.

Transaction costs related to the acquisition of investments are recorded as an expense. Unrealized gains and losses on financial assets are recognized in the Statement of Remeasurement Gains and Losses until such time that the financial asset is derecognized due to disposal or impairment. At the time of derecognition, the related realized gains and losses are recognized in the Statement of Operations and related balances reversed from the Statement of Remeasurement Gains and Losses.

- (ii) Cost category: investments not quoted in an active market, financial assets and liabilities are recorded at cost or amortized cost. Gains and losses are recognized in the Statement of Operations when the financial asset is derecognized due to disposal or impairment. Sales and purchases of investments are recorded on the trade date.

Transaction costs related to the acquisition of financial assets are included in the cost of the related instrument.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

2. SIGNIFICANT ACCOUNTING POLICIES (continued)

Financial assets are assessed for impairment on an annual basis. If there is an indicator of impairment, the Town of Comox determines if there is a significant adverse change in the expected amount or timing of future cash flows from the financial asset. If there is a significant adverse change in the expected cash flows, the carrying value of the financial asset is reduced to the highest of the present value of the expected cash flows, the amount that could be realized from selling the financial asset or the amount the Town of Comox expects to realize by exercising its right to any collateral. If events and circumstances reverse in a future period, an impairment loss will be reversed to the extent of the improvement, not exceeding the initial adjusted cost base.

(c) Short-term Investments

Investments are comprised of various investment funds which have fluctuating returns and are carried at the market value, which is equal to the carrying value, and Guaranteed Investment Certificates which are recorded at cost.

Short-term Investments	2023	2022
Municipal Finance Authority of BC Funds:		
Short-term Bond Fund *	\$ 5,105	\$ 4,871
Intermediate Bond Fund	4,617,062	4,382,983
Money Market Fund	14,943	14,224
* Adjusted for Trust Fund	4,637,110	4,402,078
Guaranteed Investment Certificates		
Scotiabank	2,000,000	-
Scotiabank (30-day Note Plan)	4,220,849	48,760
Coast Capital Savings	10,526,807	21,668,207
Raymond James	28,000,000	11,500,000
Subtotal	44,747,656	33,216,967
	<u>\$ 49,384,766</u>	<u>\$ 37,619,045</u>

(d)

Trust Fund

A Town-administered trust fund has been excluded from the consolidated financial statements. The balance in the fund was \$286,469 at December 31, 2023 (\$271,945 in 2022). It was invested in the MFA Short-term Bond Fund in 2023 and 2022.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

2. SIGNIFICANT ACCOUNTING POLICIES (continued)

(e)

Tangible Capital Assets

Tangible capital assets are recorded at cost, net of disposals, write-downs and amortization. Acquisitions exceeding the capitalization thresholds are capitalized. The Town records certain infrastructure assets as one network without breaking down the asset into component parts. Expenditures to replace components of such a network are expensed as incurred. Amortization is calculated straight line over the estimated life of the class of assets. Amortization is not taken until the asset is in use. Repairs and maintenance are not capitalized, but are expensed in the period in which they occur. Betterments which enhance the asset life or capacity are capitalized.

Major Asset Category	Threshold	Estimated Life
Land	\$ 10,000	Indefinite
Land improvements	\$ 10,000	25 years
Buildings	\$ 10,000	50 years
Vehicles, Machinery and Computers and programs	\$ 10,000	8 to 20 years
Engineered Structures		
Road surfaces	\$ 10,000	25 years
Road sub-surfaces	\$ 10,000	50 years
Sidewalks	\$ 10,000	50 years
Storm drains surface	\$ 10,000	25 years
Storm drains sub-surface	\$ 10,000	75 years
Water	\$ 10,000	75 years
Sewer	\$ 10,000	75 years
Other surface structures	\$ 10,000	25 years

(f) Future Payroll Obligations

Earned but unpaid vacation is fully accrued and recorded in the financial statements.

An accrual is made for expected payments of employee sick leave, payout on retirement, disability or death, and termination pay discounted from expected future values to net present value at year-end.

(g) Inventory

Inventory of supplies is recorded as a non-financial asset and is valued at the lower of cost and net realizable value. Cost is determined by the first-in, first-out method. Net realizable value is the estimated selling price in the ordinary course of business, less estimated costs of completion and selling costs.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

2. SIGNIFICANT ACCOUNTING POLICIES (continued)

(h) Funds

The financial operations of the Town of Comox are divided into seven funds: General Operating Fund, General Capital Fund, Reserve Fund, Water Operating Fund, Water Capital Fund, Sewer Operating Fund and Sewer Capital Fund. Functionally, the Water and Sewer Funds are used for the services of distribution of potable water and collection and treatment of sanitary sewage respectively. The General Operating Fund is used for all other Town services. Capital Funds (General, Water and Sewer are used to segregate tangible capital assets and related debt). All revenue and expenses for services are recognized in the operating funds. The Reserve Fund is used to segregate capital and other statutory reserves.

(i) Revenue Recognition

Property tax revenues are recognized in the year they are levied. User fees and garbage revenues are recognized when the service is provided. Interest and operating grants are recognized as earned. Grants in lieu of taxes are recorded on an accrual basis when it is possible to reasonably estimate the amounts receivable. User fees, garbage collection and disposal are recognized as revenue when the service is provided according to rates set in various fees and charges bylaws. Permit fees are recognized once the permit has been approved and the fee collected. Development cost charges are deferred and recognized as revenue in the year an acquisition authorized by bylaw is incurred. Contributed tangible capital assets are recorded at fair market value when the contribution is received.

Government transfers are recognized in the consolidated financial statements as revenues in the period in which events giving rise to the transfer occur, providing the transfers are authorized, any eligibility criteria have been met, and reasonable estimates of the amounts can be made, except when and to the extent the transfer gives rise to an obligation that meets the definition of a liability.

(j) Liability for contaminated sites

A liability for remediation of a contaminated site is recognized at the best estimate of the amount required to remediate the contaminated site when contamination exceeding an environmental standard exists, the Town is either directly responsible or accepts responsibility, it is expected that future economic benefits will be given up, and a reasonable estimate of the amount is determinable. The best estimate of the liability includes all costs directly attributable to remediation activities and is reduced by expected net recoveries based on information available at December 31, 2023.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

2. SIGNIFICANT ACCOUNTING POLICIES (continued)

At each financial reporting date, the Town reviews the carrying amount of the liability. Any revisions required to the amount previously recognized is accounted for in the period revisions are made. The Town continues to recognize the liability until it is settled or otherwise extinguished. Disbursements made to settle the liability are deducted from the reported liability when they are made. As at December 31, 2023 the Town has not recorded any liability for contaminated sites as no such sites have been identified.

(k) Measurement Uncertainty

The preparation of consolidated financial statements in conformity with Canadian public sector Accounting standards requires management to make estimates and assumptions that affect the reported amounts of assets and liabilities and the disclosure of contingent assets and liabilities at the date of the consolidated financial statements, as well as the reported amounts of revenues and expenses during the reporting period. Significant areas requiring management estimates are the estimated life of tangible capital assets (used to establish amortization), asset retirement obligations, determination of future payroll obligations, collectability of accounts receivable and provisions for contingencies. Actual results may vary from the estimates and adjustments will be reported and reflected in operations as they become known. Liabilities for contaminated sites are estimated based on the best information available regarding potentially contaminated sites that the Town is responsible for.

(l) Asset Retirement Obligation

A liability for an asset retirement obligation is required at the best estimate of the amount required to retire a tangible capital asset (or a component thereof) at the financial statement date when there is a legal obligation for the Town to incur retirement costs in relation to a tangible capital asset (or component thereof), the past transaction or event giving rise to the liability has occurred, it is expected that future economic benefits will be given up, and a reasonable estimate of the amount can be made. The best estimate of an asset retirement obligation incorporates present value technique, when the cash flows required to settle or otherwise extinguish an obligation are expected to occur over extended future periods.

When a liability for an asset retirement obligation is initially recognized, a corresponding asset retirement cost is capitalized to the carrying amount of the related tangible capital asset (or a component thereof). The asset retirement cost is amortized over the useful life of the related asset. At each financial reporting date, the Town reviews the carrying amount of the liability. The Town recognizes period-to-period changes to the liability due to the passage of time as accretion expense. Changes to the liability arising from revisions to either the timing, the amount of the original estimate of undiscovered cash flows or the discount rate are recognized as an increase or decrease to the carrying amount of the related tangible capital asset. The Town continues to recognize the liability until it is settled or otherwise extinguished. Disbursement made to settle the liability are deducted from the reported liability when they are made.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

(m) Change in Accounting Policy

Asset Retirement Obligations

Effective January 1, 2023, the Town adopted the Public Sector Accounting Board's (PSAB) new standard for the recognition, measurement and disclosure of a liability for asset retirement obligations under PS 3280 Asset Retirement Obligations. The new standard establishes when to recognize and how to measure a liability for an asset retirement obligation, and provides the related financial statement presentation and disclosure requirements.

Under the new standard, a liability for an asset retirement obligation is recognized at the best estimate of the amount required to retire a tangible capital asset when certain criteria are met, as described in Note 2(l). Pursuant to the recommendations, the change was applied prospectively and prior periods have not been restated. As such, the Town recognized asset retirement obligations for those arising on or after January 1, 2023, as well as for those arising prior to January 1, 2023 but for which an obligation was not previously recognized. See note 17 for details of the impact on the consolidated financial statements.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

3. RECEIVABLES

Receivables are composed of the following amounts:

	2023	2022
Employees	\$ 1,617	\$ 7,859
Governments	674,611	252,190
Other	2,332,873	1,219,498
Total receivables	\$ 3,009,101	\$ 1,479,547

4. PAYABLES

Payables are composed of the following amounts:

	2023	2022
Wages payable	\$ 377,265	\$ 325,008
Governments	1,732,925	2,175,912
Trade & Other	2,371,218	1,475,859
Total payables	\$ 4,481,407	\$ 3,976,779

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

5. NET DEBENTURE DEBT

The Town’s debenture debt arose from participation in debenture issued by the Municipal Finance Authority (MFA) of B.C.

The debt was taken to help fund the Kye Bay Water and Sewer system expansion under Bylaw #2426. The balance of the debt at year-end is nil (2022 - \$20,371). The interest rate was 2.40% and was paid off over 19 years.

6. EQUIPMENT FINANCING

General Capital Fund	Balance, Beginning of Year	Additions	Principal Payments	Interest Paid	Balance, End of Year
Equipment Financing	\$ -	1,804,024	(104,477)	(33,060)	1,699,547

The entire loan is repayable on-demand by the Municipal Finance Authority. The applicable rate of interest is variable, based on the annual variable rate offered by the Municipal Finance Authority. The rate of interest for the payments in 2023 was 5.07%, it may increase or decreased based upon market fluctuations. The requirements for future repayments of principal and interest on existing debt for the next five years are estimated as follows:

	General Capital Fund	
	Principal	Interest
2024	\$ 325,333	\$ 87,276
2025	344,330	68,279
2026	364,151	48,458
2027	385,113	27,496
2028	280,621	6,121
Total	\$1,699,547	\$ 237,630

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

7. CONTINGENT LIABILITIES

(a) Regional District Debt

Regional District debt is, under the provisions of the Local Government Act (Section 836), a direct, joint and several liability of the District and each member municipality within the District, including the Town of Comox. Readers are referred to the Comox Valley Regional District 2023 Audited Financial Statements for specific information and detail.

(b) Comox Fire Department

The Comox Fire Department protects both the Town of Comox and the Comox Rural Fire Protection Improvement District, and so is jointly funded by the Town and the District. In the event that this relationship ended, the Town would have to compensate the District for its share of the equity of Fire Department assets. At December 31, 2023 the District's share of those tangible capital assets (at cost and net book value) were \$1,424,243 and \$709,776 respectively (\$1,133,698 and \$442,542 in 2022) and its share of a Fire Department Capital Reserve was \$411,770 (\$485,422 in 2022). Due to the new Asset Retirement Obligation rules (See Note 17), the Improvement District has a related balance of \$19,485 which is for the buildings they maintain 1/3 ownership in.

(c) Claims

At December 31st there existed outstanding claims against the Town. These claims have been referred to legal counsel and to the Town's liability insurers. It is not possible to determine the Town's potential liability, if any, with respect to these matters.

Town of Comox

Notes to the Consolidated Financial Statements
For the year ended December 31, 2023

8. PENSION INFORMATION

The Town of Comox and its employees contribute to the Municipal Pension Plan (a jointly trustee pension plan). The board of trustees, representing plan members and employers, is responsible for administering the plan, including investment of assets and administration of benefits. The plan is a multi-employer defined benefit pension plan. Basic pension benefits are based on a formula. As at December 31, 2022, the plan has about 240,000 active members and approximately 124,000 retired members. Active members include approximately 43,000 contributors from local governments.

Every three years, an actuarial valuation is performed to assess the financial position of the plan and adequacy of plan funding. The actuary determines an appropriate combined employer and member contribution rate to fund the plan. The actuary's calculated contribution rate is based on the entry-age normal cost method, which produces the long-term rate of member and employer contributions sufficient to provide benefits for average future entrants to the plan. This rate may be adjusted for the amortization of any actuarial funding surplus and will be adjusted for the amortization of any unfunded actuarial liability.

The most recent actuarial valuation for the Municipal Pension Plan as at December 31, 2021, indicated a \$3,761 million funding surplus for basic pension benefits on a going concern basis.

The Town of Comox paid \$563,594 for employer contributions to the plan in fiscal 2023 (\$519,477 in 2022).

Employers participating in the plan record their pension expense as the amount of employer contributions made during the fiscal year (defined contribution pension plan accounting). This is because the plan records accrued liabilities and accrued assets for the plan in aggregate, resulting in no consistent and reliable basis for allocating the obligation, assets and cost to individual employers participating in the plan.

9. FUTURE PAYROLL OBLIGATIONS

The Town calculates the value of vacation, sick, severance, and meritorious service accruals to be \$1,084,184 at December 31, 2023 (\$1,131,611 in 2022).

Vacation Accrual is calculated to be \$548,302 at December 31, 2023 (\$599,096 in 2022). Sick, Severance, & Meritorious Service is calculated to be \$535,883 at December 31, 2023 (\$532,516 in 2022). In 2023, an unamortized net actuarial gain of \$110,906 (\$187,390 in 2022) was recognized as a result of an overvaluation from prior years.

Meritorious Service is for Town employees who have worked at least 10 years for the Town and then retire, become permanently disabled, or die receive 2 days pay for each year of service. The reported liability reflects the likelihood that employees will become eligible for this benefit. The calculations were projected into the future with an inflation factor of 6% and discounted back at a discount rate of 4.10% (2.5% and 4.5% respectively in 2022). Employee retirement benefit payments are being funded by an accounting charge on wages paid in the year.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

10. FINANCIAL RISKS AND CONCENTRATION OF RISK

The Town of Comox is potentially exposed to credit risk, market and interest rate risk, and liquidity risk from the Town's financial instruments. Qualitative and quantitative analysis of the significant risks from the Town's financial instruments is provided below by type of risk.

(a) Credit Risk:

Credit risk primarily arises from the Town's cash, investments and accounts receivable. The risk exposure is limited to their carrying amounts as at the date of the statement of financial position.

Accounts receivable primarily consist of amounts receivable from other government organizations and residents. To reduce the risk, the Town regularly reviews the collectability of its accounts receivable and if needed, will establish an allowance based on its best estimate of potentially uncollectible amounts. As at December 31, 2023, the amount of allowance for uncollectible amounts was nil (2022 - nil). The Town historically has not had difficulty collecting receivables, nor have counterparties defaulted on any payments.

(b) Market and Interest Rate Risk:

Market risk is the risk that changes in market prices and inputs, such as interest rates, will affect the Town's income. The objective of market risk management is to control market risk exposures within acceptable parameters while optimizing the return on risk.

The Town manages market risk by holding cash balances with top rated Canadian Schedule I financial institutions. The investments are managed following the investment policy which is approved by the Town's council. The Town periodically reviews its investments and is satisfied that the portfolio investments are being managed in accordance with the investment policy.

Interest rate risk is the risk that the fair value of future cash flows of a financial instrument will fluctuate because of changes in the market interest rates. The Town's investments are disclosed in Note 2 and changes in the fair value of investments have parallel changes in unrealized gains or losses until realized on disposal. The Town's exposure to interest rate risk in relation to debt instruments is limited to long-term debt and short-term financing. The risk applies only to long-term debt when amortization periods exceed the initial locked-in term. Short-term financing is subject to daily floating rates, which can result in variability over the course of short-term financing. Interest rate risk related to debt instruments is managed through budget and cash forecasts.

Interest rates have increased during the year, which primarily affects interest costs of short-term debt, and interest earnings on investments.

(c) Liquidity Risk:

Liquidity risk is the risk that the Town will not be able to meet its financial obligations as they become due. The Town manages liquidity risk by continually monitoring actual and forecasted cash flows from operations, anticipated investing, and financial activities to ensure that its financial obligations are met.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

11. FEDERAL PAYMENTS IN LIEU OF TAX

Although the Town is not economically dependent upon receipt of Federal Payments in Lieu of Tax (PILT), it normally receives 15% of its revenue from taxation from this source of revenue. Were the Government of Canada to halt its PILT, there would be a significant impact on operations that could result in either a reduction in service levels and/or an increase in property tax rates.

12. RESTRICTION ON DEFERRED REVENUE

The Town receives Development Cost Charges from developers. Section 935 of the Local Government Act requires that these funds are deposited to reserves and limits the use of those reserves (including the interest earned on them) to capital costs related to the purpose for which the charge was collected. Section 941 of the Local Government Act similarly limits the use of cash payments received in lieu of the 5% parklands provided at subdivision of land, for acquisitions of parkland. See Schedule 2 for details of deferred revenues.

Town of Comox

Notes to the Consolidated Financial Statements
For the year ended December 31, 2023

13. SEGMENTED INFORMATION

The Town of Comox is a local government that provides a wide range of services to its citizens. For management and reporting purposes the Town's operations are organized and reported by segments. Schedules 5 and 6 (for 2023 and 2022, respectively) of these Financial Statements disclose the Town's revenue and expenses split into the following segments:

General Government – including the activities of Council plus general Town administration, finance, and enforcement of Town bylaws.

Protective Services – including the activities of the RCMP on behalf of Comox, plus the Comox Volunteer Fire Department, building inspection, emergency planning and animal control.

Transportation – including roads, sidewalks, street lighting, traffic control and storm drains.

Solid Waste Management – including collection and disposal of garbage, recyclables and yard waste. The Town does not operate any landfills or transfer facilities.

Development Services – including Town planning and our annual grants to the Downtown Business Improvement Area Association.

Marina – is operation of the Town's marina.

Parks, Recreation, and Culture – including operating the Town's recreation centre and public recreation programs, Town parks and greenways, cultural grants from the Town and events sponsored for the Town, Christmas lighting, and Town buildings used for recreation and cultural activities. The Town is a member of the Vancouver Island Regional Library (VIRL), and owns library space that it rents to VIRL, which operates the Comox library branch.

Water Service – is the distribution of water to Town residents. The Town buys treated bulk water from the Comox Valley Regional District, so the Town neither produces nor treats the water it distributes.

Sanitary Sewer Service – is the collection of sanitary sewage and transmission of the same to the sanitary sewer treatment system operated by the Comox Valley Regional District. The Regional District charges the Town for a share of the costs to operate its system.

Certain allocations are employed in the preparation of segmented financial information. General Property Taxation, Payments in Lieu thereof, and Investment income are not allocated between segments, but are allocated to General Government. Government grants and DCCs used for capital acquisitions are allocated to segments depending on the purpose of the grant. Investment income is allocated to segments depending on the nature of the capital reserves that it has been earned on. Some expenses are allocated to segments based on an estimated use of resources.

The accounting policies used in these segments are consistent with those followed in preparation of the consolidated financial statements as disclosed in Note 2.

There are no inter-segment sales of goods or services.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

14. COMMUNITY WORKS (FEDERAL GAS TAX) FUNDS

The Town receives transfers of Community Works (federal gas tax) Funds through the Union of BC Municipalities that can be used to fund eligible expenditures.

Continuity of Funds:	2023	2022
Opening balance at start of year	\$ 5,329,402	\$ 5,339,909
Transfers received during year	704,736	675,280
Interest on investments	269,466	103,371
Draws to fund eligible projects	(2,147,046)	(789,158)
Closing balance at end of year	\$ 4,156,558	\$ 5,329,402

These transfers are recorded as conditional grant revenue because they can only be used for eligible expenditures under the terms of a 2014 agreement between the Government of Canada and the Union of BC Municipalities.

15. RECONCILIATION OF 2023 BUDGET

The Town's budget figures come from a five-year Financial Plan adopted May 10, 2023 in accordance with Section 165 of the Community Charter of BC. That section requires that municipalities must set out for each year of their Plan, the proposed expenditures (including debt principal repayments, transfers to reserves, and tangible capital asset acquisitions) and the proposed funding sources for them (including debt issues and transfers from reserves and accumulated surplus). However, for financial reporting purposes the Town follows public sector accounting standards and reports revenues and expenses, so the following adjustments must be made to the budgeted figures to reconcile them to the Towns 2023 Financial Plan:

Reconciliation of 2023 Budget Reported to Financial Plan:	
Net budget for 2023 reported	\$ 5,136,487
Capital acquisitions in Financial Plan	(20,502,214)
Transfers from capital reserves in Financial Plan	14,454,972
Transfers to capital reserves in Financial Plan	(3,996,639)
Debt principal repayments in Financial Plan	(196,630)
Loan Proceeds	1,804,024
Amortization expense	3,300,000
Net of Financial Plan	\$ -

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

16. ACCUMULATED SURPLUS

Operating Funds - comprise the principal operating activities of the Town and are separated into General, Water, and Sanitary Sewer Funds.

Capital Funds - used to record the acquisition and disposal of tangible capital assets and related financing and are similarly separated into General, Water and Sanitary Sewer Funds. The General Capital Fund also contains shares in Courtenay Golf Club Ltd. carried at a value of \$8,750.

Reserve Funds - created to hold cash, and investments for specific future requirements. They are comprised of the funds shown in Schedule 1.

Accumulated Surplus	2023	2022
Operating Funds:		
General Operating Fund	\$ 2,977,984	\$ 5,168,417
Water Operating Fund	2,771,083	2,140,072
Sanitary Sewer Operating Fund	4,614,975	4,067,185
	<hr/>	<hr/>
	10,364,042	11,375,674
Capital Funds:		
General Capital Fund	80,280,443	73,393,402
Water Capital Fund	9,029,090	8,530,457
Sanitary Sewer Capital Fund	8,067,321	7,819,226
	<hr/>	<hr/>
	97,376,854	89,743,085
Reserve Funds (Schedule 1)	25,537,570	20,974,585
	<hr/>	<hr/>
	\$ 133,278,466	\$ 122,093,344

17. ASSET RETIREMENT OBLIGATIONS

The Town of Comox owns and operates several assets that are known to have asbestos, which represent a health hazard upon demolition or renovation of the assets. There is a legal obligation to remove and dispose of the hazardous materials. Following adoption of Public Sector Accounting Standard PS 3280 Asset Retirement Obligations, the Town The transition and recognition of asset retirement obligations involved an accompanying increase to the Buildings and Fire capital assets. The increase in capital assets is amortized on a straight-line basis over the remaining expected useful life of the related

The Town has adopted this standard prospectively. Under the prospective method, the discount rate and assumptions used on initial recognition are those as of the date of adoption of the standard. Estimated costs totalling \$251,134 have been discounted using a present value calculation with a discount rate of 4.72%. The timing of these expenditures is estimated to occur between 2024 and 2074 with the regular replacement, renovation or disposal of assets. No recoveries are expected at this time.

Town of Comox

Notes to the Consolidated Financial Statements

For the year ended December 31, 2023

17. ASSET RETIREMENT OBLIGATIONS (Continued)

	<u>2023</u>
Opening Asset Retirement Obligation	\$ -
Initial recognition of expected discounted cash flows	251,134
Increase due to accretion	<u>24,268</u>
Closing Asset Retirement Obligation	<u>\$ 275,402</u>

18. GROWING COMMUNITIES FUND

The Province of British Columbia distributed conditional GCF grants to communities at the end of March 2023 to help local governments build community infrastructure and grant to all municipalities in British Columbia.

The Town of Comox received \$4,693,000 of GCF funding in March 2023.

<u>Growing Communities Fund</u>	<u>2023</u>
Opening Balance	\$ -
Grant	4,693,000
Eligible Costs	
Queens Ave. Storm Drainage	(271,649)
Town Hall Reengineer	(17,237)
Net Zero Project	<u>(739,619)</u>
<u>Balance, end of year</u>	<u>\$ 3,664,495</u>

The remaining balance of the Growing Communities Fund was allocated to the Capital Works Reserve for the 2023 fiscal year, any funds remaining at the end of 2024 will be allocated to a separate Growing Communities Fund reserve.

19. COMMITMENT

The Town of Comox has an agreement with Emterra Environmental for garbage, recycling Service is provided by Emterra weekly to the residents in the Town of Comox and costs are expensed monthly. Contracted cost estimate for 2024 is \$1,645,302.

20. COMPARATIVE FIGURES

Comparative figures have been reclassified to conform to current year's presentation.

Town of Comox

Schedule of Continuity of Reserve Funds
 Year ended December 31, 2023

Schedule 1
 (unaudited)

	2023	2022
Opening Balance of Reserve Funds	\$ 20,974,585	\$ 19,366,742
Contributions to funds from operations	13,567,430	4,808,446
Interest earned on funds	1,060,521	374,906
Withdrawals from funds for capital acquisitions	(10,064,966)	(3,575,509)
Closing balance of Reserve Funds	\$ 25,537,570	\$ 20,974,585

Represented by the following financial assets:

Cash and short-term investments	\$ 25,537,570	\$ 20,974,585
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Including the following Reserve Funds:

Federal Gas Tax (Community Works) Funds (Note 14)	\$ 4,156,558	\$ 5,329,402
Capital Works Reserve	7,237,424	3,848,110
Equipment Replacement Reserve	2,367,765	858,150
Municipal Marina Reserve	658,099	531,238
Water Utility Reserve	2,966,781	2,847,974
Sewer Enterprise Reserve	6,223,635	5,480,297
Lancaster Sewer Lift Station Capital Reserve	86,995	82,808
Point Holmes Sewer Lift Station Capital Reserve	57,373	54,611
Recreation Centre Capital Reserve	6,312	6,009
Waterfront Walkway Reserve	366	348
Affordable Housing Reserve	197,343	187,845
Public Safety Reserve	330,212	219,132
Fire Department Capital Reserve - Town Share	836,937	1,043,239
	25,125,800	20,489,163
Fire Department Capital Reserve CFPID share (Note 7b)	411,770	485,422
	\$ 25,537,570	\$ 20,974,585

	2022 Balance	Receipts	Interest	Refunded	Recognized as Revenue	2023 Balance
Deferred Revenue						
Prepaid property taxes	\$ 1,071,634	1,127,585	17,227	-	1,071,634	\$ 1,144,812
Parcel Tax Commutation, Kye Bay	7,662	7,662	-	-	15,324	-
Prepaid General Fees						
Prepaid Storm Drainage Development Works Payment						
Prepaid marina fees	113	-	-	-	73	40
Prepaid dog tags Amounts are in 1-411-410-000						
Prepaid business licer Amounts are in 1-411-410-000						
Prepaid environmental mitigation (\$7,670)						
General Unearned Income	99,361	104,489	-	-	103,525	100,325
Growing Communities Fund	-	4,693,000	-	-	4,693,000	-
General Unearned Income - 2nd account	-	-	-	-	-	-
Prepaid General Fees	99,474	4,797,489	-	-	4,796,598	100,365
<u>Prepaid Recreation</u>						
Recreation Unearned Income	423,877	2,871,337	-	-	2,844,199	451,015
Recreation Unearned Income - 2nd account	2,417	-	-	(323)	-	2,094
Recreation, unearned renters income	-	-	-	-	-	-
Recreation, facility deposits	-	-	-	-	-	-
Recreation, facility deposits - 2nd account to be remapp	-	-	-	-	-	-
Prepaid Recreation	426,294	2,871,337	-	(323)	2,844,199	453,109
Total Deferred Revenue	1,605,064	8,804,073	17,227	(323)	8,727,755	1,698,286
Restricted Revenue						
<u>Development Charges:</u>						
Open Space Development Cost Charges	3,839,030	6,515	184,688	-	-	4,030,233
Water Development Cost Charges	925,107	-	44,466	-	-	969,573
Major Road Development Cost Charges	1,205,986	8,245	58,170	(5,057)	-	1,267,344
Storm Drain Development Cost Charges	407,229	-	19,574	-	-	426,803
Sanitary Sewer Development Cost Charges	489,807	3,216	23,624	-	-	516,647
	6,867,159	17,976	330,522	(5,057)	-	7,210,600
<u>Other Restricted Revenue</u>						
5% Payment in Lieu of Parks Dedication	268,856	-	12,922	-	-	281,778
Developer Payments in Lieu of Parking	31,775	-	1,528	-	-	33,303
Developer Contribution for Public Amenities	10,084	-	485	-	-	10,569
Developer Payments for Affordable Housing	169,293	-	8,137	-	-	177,430
Provincial Government, Climate Action	135,082	135,082	-	-	-	270,164
Provincial Government, Child Care Spaces	-	-	-	-	-	-
	-	-	-	-	-	-
Provincial Government, Marine Services Building	567,607	224,949	27,283	-	819,839	-
Regional Government, Transportation	-	250,000	0	0	-	250,000
Provincial Government, Community Economic Developm	-	80,000	-	-	56,315	23,685
Provincial Government, Fire	-	15,000	-	-	-	15,000
	1,182,697	705,031	50,355	-	819,839	1,023,244
Total Restricted Revenue	8,049,856	723,007	380,877	(5,057)	876,154	8,272,529

Town of Comox

Continuity of Equity in Tangible Capital Assets
Year ended December 31, 2023

Schedule 3

	2023	2022
Opening balance of equity in Tangible Capital Assets	89,743,085	86,803,345
Changes in Capital Assets		
Acquisitions of tangible capital assets	12,181,724	6,173,949
Increase in tangible capital assets due to asset retirement obligations	251,134	
Tangible capital assets contributed	422,486	-
Additions of Capital Assets	12,855,344	6,173,949
Write-offs of assets replaced (at NBV)	-	-
Amortization expense	(3,542,399)	(3,278,188)
Changes in related liabilities		
Principal payment on debenture (to sinking fund)	8,464	23,225
Actuarial earnings on debenture sinking fund	11,907	20,754
Borrowing in year	(1,804,024)	
Borrowing principal repaid	104,477	-
Closing Balance of Equity in Tangible Capital Assets	\$ 97,376,854	\$ 89,743,085

Represented by the following:

Tangible capital assets at net book value (Schedule 4)		
Capital assets at cost	\$ 161,326,096	\$ 148,981,583
Less accumulated amortization	(62,258,445)	(59,226,877)
Tangible capital assets at net book value	99,067,651	89,754,706
Net debenture debt (Note 5)	-	(20,371)
Equipment Financing (Note 6)	(1,699,547)	-
Shares in Courtenay Golf Club Ltd.	8,750	8,750
	\$ 97,376,854	\$ 89,743,085

Town of Comox
Schedule of Tangible Capital Assets
Year ended December 31, 2023

Schedule 4

										Totals	
	Land	Land Improvements	Buildings	Equipment	Transportation	Storm Drain	Water Service	Sewer Service	Work in Progress	2023	2022
Cost											
Opening costs	9,138,380	5,131,949	21,294,928	9,824,625	50,479,606	23,652,295	14,127,253	12,176,596	3,155,951	148,981,583	142,939,030
Additions during the year	-	241,612	1,800,280	1,493,449	4,959,365	419,427	708,736	427,023	2,554,318	12,604,210	6,173,949
Add/(Less): Assets put to use	-	-	491,176	-	1,807,861	-	12,305	-	(2,311,342)	-	-
Additions due to ARO recognition Note (17)		7,733	243,401							251,134	-
Disposals and write downs	-	-	-	(510,831)	-	-	-	-	-	(510,831)	(131,396)
Closing costs	9,138,380	5,381,294	23,829,785	10,807,243	57,246,832	24,071,722	14,848,294	12,603,619	3,398,927	161,326,096	148,981,583
Accumulated Amortization											
Opening accum'd amortization	-	2,817,169	5,744,668	5,991,378	26,653,373	7,973,756	5,504,436	4,542,097	-	59,226,877	56,080,085
Amortization	-	171,558	496,303	676,145	1,466,467	327,148	225,850	178,928	-	3,542,399	3,278,188
Accum Amort Adj for Disp/WD	-	-	-	(510,831)	-	-	-	-	-	(510,831)	(131,396)
Closing accum'd amortization	-	2,988,727	6,240,971	6,156,692	28,119,840	8,300,904	5,730,286	4,721,025	-	62,258,445	59,226,877
Net Book Value of Tangible Capital Assets	9,138,380	2,392,567	17,588,814	4,650,551	29,126,992	15,770,818	9,118,008	7,882,594	3,398,927	99,067,651	89,754,706

Town of Comox
Schedule of Operations by Segment
Year ended December 31, 2023

Schedule 5

	General Government	Protective Services	Waste Disposal	Affordable Housing	Development Services	Transportation	Parks, Recreation & Culture	Marina	Water	Sanitary Sewer	Total All Segments	2023 Budget
Revenue												
Taxation	\$ 13,140,435	\$ -	\$ -	\$ -	\$ 72,874	\$ 4,239	\$ -	\$ -	\$ 440,564	\$ 467,770	\$ 14,125,882	\$ 14,045,739
Sale of Services	204,048	1,636,528	2,111,323	-	170,065	598,586	1,891,080	487,632	3,265,326	3,322,216	13,686,804	12,645,770
Government Transfers	5,606,786	84,000	-	-	-	1,033,768	184,213	1,084,525	-	-	7,993,292	5,960,054
Investment Income	1,235,222	88,372	-	9,498	-	299,563	-	-	6,949	277,096	1,916,700	400,000
Development Contributions	-	-	-	-	-	237,930	54,501	-	74,578	60,534	427,543	79,500
Gain (loss) on Disposals	-	-	-	-	-	204,900	382	-	-	-	205,282	-
Other	136,223	3,662	-	-	-	2,943	5,916	-	-	-	148,744	145,000
	20,322,714	1,812,562	2,111,323	9,498	242,939	2,381,929	2,136,092	1,572,157	3,787,417	4,127,616	38,504,247	33,276,063
Expenses												
Employees	1,859,558	1,777,489	-	-	576,332	1,272,301	3,010,680	47,582	228,359	53,907	8,826,208	9,093,813
Materials	227,412	316,158	-	-	1,557	276,618	466,788	35,822	2,552,773	41,151	3,918,279	3,788,037
Services	1,438,638	2,428,976	1,863,581	-	109,313	741,031	1,537,007	118,221	155,584	2,562,501	10,954,852	11,879,166
Interest	40,707	-	-	-	-	33,060	-	-	3,619	-	77,386	78,560
Amortization	256,202	149,756	-	35,267	-	2,189,601	424,840	81,956	225,850	178,928	3,542,400	3,300,000
	3,822,517	4,672,379	1,863,581	35,267	687,202	4,512,611	5,439,315	283,581	3,166,185	2,836,487	27,319,125	28,139,576
Surplus (deficit) for the year	\$ 16,500,197	\$ (2,859,817)	\$ 247,742	\$ (25,769)	\$ (444,263)	\$ (2,130,682)	\$ (3,303,223)	\$ 1,288,576	\$ 621,232	\$ 1,291,129	\$ 11,185,122	\$ 5,136,487

Year ended December 31, 2022

Schedule 6

	General Government	Protective Services	Waste Disposal	Affordable Housing	Development Services	Transportation	Parks, Recreation & Culture	Marina	Water	Sanitary Sewer	Total All Segments	2022 Budget
Revenue												
Property taxation	\$ 11,894,370	\$ -	\$ -	\$ -	\$ 73,008	\$ 4,239	\$ -	\$ -	\$ 439,556	\$ 402,200	\$ 12,813,373	\$ 13,165,150
Sale of services	197,771	808,301	1,857,250	-	116,324	39,317	1,570,392	442,186	3,106,607	3,124,158	11,262,306	9,986,585
Government transfers	1,954,355	80,519	-	1,147,479	-	-	468,402	-	-	-	3,650,755	6,223,842
Investment income	550,115	47,786	-	3,567	-	175,920	121	8,189	40,668	98,655	925,021	400,000
Development contributions	-	-	-	-	-	359,594	326,314	-	-	-	685,908	1,254,500
Gain (loss) on disposal of assets	3,450	-	-	-	-	47,500	382	-	-	-	51,332	-
Other	113,441	11,036	-	-	-	-	1,586	-	-	-	126,063	105,000
	14,713,502	947,642	1,857,250	1,151,046	189,332	626,570	2,367,197	450,375	3,586,831	3,625,013	29,514,758	31,135,077
Expenses												
Employees	1,802,339	1,465,095	-	-	641,250	1,250,188	2,763,904	71,552	211,275	64,407	8,270,010	8,223,942
Materials	79,911	158,381	126	-	2,243	297,336	413,925	13,547	2,014,383	18,730	2,998,582	3,036,142
Services	1,375,146	2,378,726	1,591,743	-	120,433	701,709	1,394,873	185,826	48,134	2,342,994	10,139,584	10,743,719
Interest	12,534	-	-	-	-	-	-	-	8,821	-	21,355	27,240
Amortization	236,765	143,051	-	17,634	-	2,017,938	371,441	80,056	233,222	178,081	3,278,188	3,300,000
	3,506,695	4,145,253	1,591,869	17,634	763,926	4,267,171	4,944,143	350,981	2,515,835	2,604,212	24,707,719	25,331,043
Surplus (deficit) for the year	\$ 11,206,807	\$ (3,197,611)	\$ 265,381	\$ 1,133,412	\$ (574,594)	\$ (3,640,601)	\$ (2,576,946)	\$ 99,394	\$ 1,070,996	\$ 1,020,801	\$ 4,807,039	\$ 5,804,034

Town of Comox
Statement of Financial Information
Schedule of Remuneration & Expenses for Elected Officials
For the year ended December 31, 2023

	Remuneration	Expenses
Steve Blacklock, Councillor	26,371	6,527
Ken Grant, Councillor	26,371	13,630
Chris Haslett, Councillor	26,371	4,661
Jonathan Kerr, Councillor	26,371	5,160
Jenn Meilleur, Councillor	26,371	4,194
Nicole Minions, Mayor	58,275	9,323
Maureen Swift, Councillor	<u>26,371</u>	<u>-</u>
	<u><u>\$ 216,501</u></u>	<u><u>\$ 43,495</u></u>

Expenses include expense claims by Council members, together with registration fees and travel expenses paid by the Town on their behalf.

In addition, each member of Council is covered under an accident insurance policy (when on Town business) which provides:

- 1) \$ 100,000 life insurance
- 2) \$ 300 weekly indemnity for total disability
- 3) \$ 3,000 additional medical expense

Town of Comox
Statement of Financial Information
Schedule of Remuneration & Expenses for Employees Earning over \$75,000
For the year ended December 31, 2023

	Remuneration	Expenses
Shelley Ashfield, Director of Operations	139,103	794
Regina Bozerocka, Planner II	85,747	3,516
Corey Brooks, Platoon Captain	118,953	940
Brent Craven, Lieutenant	90,987	940
Kirsten Edwards, Chargehand	80,757	734
Dany Fortin, Arborist	76,936	296
Clive Freundlich, Director of Finance	139,091	259
Eric Gaylor - Maintenance Operator	78,603	1,152
Elsa Gilroy, Lieutenant	79,212	3,728
Samuel Greene - Maintenance Operator	81,844	3,633
Koreen Gurak, Communications and Legislative Coordinator	87,211	567
Ted Hagmeier, Recreation Director	114,173	-
Kyle Jorgensen, Labourer/Equipment Operator	78,618	88
Marvin Kamenz, Town Planner	137,469	4,302
Geoffrey Kreek, Deputy Director of Finance	105,844	1,417
Jim Lariviere, Assistant Fire Chief	130,012	1,400
Pascal Lafreniere - Platoon Captain	117,271	610
Joel Louke, Utilities Operator	81,912	777
Kenneth Makortoff, Foreman	93,841	1,787
Tryna McLean - Chargehand	79,914	-
Wayne Meyer - Equipment Operator	80,978	349
Robert Nall, Parks Superintendent	97,287	621
Robin Pallett, Planner II	84,521	3,302
Craig Perry, Manager of Public Works	118,940	923
Lia Pesklevits, Executive Coordinator	81,883	1,076
Jessie Proske, Deputy Director of Finance	109,851	7,530
Ian Rogers, Building Inspector II	94,250	3,110
Shelly Russwurm, Corporate Officer	131,278	-
Mark Salter, Groundskeeper	76,238	-
Gord Schreiner, Fire Chief	146,036	-
Andrew Scott - Equipment Operator	80,567	777
Rick Shelton, Assistant Fire Chief	137,040	-
Michael Springer, Asset and Facilities Manager	100,269	-
Jeffrey Spruyt, Maintenance Operator	75,834	105
Robert Stevens, Platoon Captain	116,586	680
Marisa Thurlborn, Revenue Manager	81,493	4,749
Elliot Turnbull, Planner II	86,037	1,277
Doug Ure, Tradesperson I	77,562	163
Jordan Wall, Chief Administrative Officer	172,384	5,198
John Williams, Maintenance Operator	75,363	2,567
subtotal	4,021,899	59,367
all employees under \$75,000	2,952,254	31,584
Total for all employees	<u>\$ 6,974,153</u>	<u>\$ 90,951</u>

Reconciliation to Salaries, Wages & Employee Benefits reported in Financial Statement

Total employee remuneration (above) less taxable benefits	\$ 6,904,488
Plus Council remuneration	216,501
Decrease to Accrued Payroll & Post-Employment Benefits in year	4,830
Plus employee benefits included in Disbursements	<u>1,700,389</u>
Salaries, wages and employee benefits reported on Schedule 5 of the Consolidated Financial Statements for the Town of Comox	<u>\$ 8,826,208</u>

NOTES:

- Expenses include payments to employees plus expenses paid on their behalf.
- No schedule of severance agreements is included because there were none in the year.

Town of Comox
Statement of Financial Information
Schedule of Severance Agreements
For the year ended December 31, 2023

NIL

Prepared under the Financial Information Regulation, Schedule 1, subsection 6(7).

Town of Comox
Statement of Financial Information
Schedule of Disbursements over \$25,000
For the year ended December 31, 2023

1297398 B.C. Ltd.	50,405
ABC Printing & Signs	29,246
ACL Landscaping Ltd	40,641
A-Mais Technologies Inc.	33,412
Active Network Ltd	31,920
Alpha Strategy Media Inc.	590,351
Andrew Sheret Limited	77,335
Aon Reed Stenhouse Inc	257,273
B C Hydro	379,327
Ballard Fine Homes Ltd	51,500
Benco Ventures Ltd	379,831
Big Island Building Services Ltd.	59,815
Brogan Fire & Safety	49,303
Bunzl Cleaning & Hygiene	53,100
C3 Mainline Inspections Incorporated	46,287
Cameron Contracting Ltd	120,811
Ceridian Canada Ltd.	43,431
City of Courtenay	33,331
Civic Legal LLP	42,916
Cohlmeier Architecture Limited	77,587
Commercial Emergency Equipment Co.	648,712
Comox Archives And Museum Society	32,499
Comox Business In Action	93,150
Comox Firefighters Association	35,877
Comox Nautical Days Society	28,000
Comox Valley Martial Arts & Fitness	60,064
Comox Valley Regional District	9,572,295
Comox Valley Transition Society	30,000
Comox-Strathcona Regional Hospital Dist	1,540,665
Compass Adventure	30,219
CUPE Local 556	63,635
D.K.I Services Ltd	32,771
Duncan Electric Motor Ltd.	155,993
Edgett Excavating Ltd.	993,753
Elisco Enterprises Inc.	37,041
Emterra Environmental	951,669
Enex Fuels Ltd.	60,877
Expertec	30,268
GHD Limited	78,429
GROW Tree Care	63,432
Habitat Systems Inc.	167,194
Hand-In-Hand Early Years	121,880
Highstreet Aurora Developments Ltd.	28,750
Hyatt Regency Vancouver	34,395
ICBC Insurance Corporation of BC	31,305
ISL Engineering and Land Services Ltd	90,086
Johnson Controls #V4020	1,598,719
JP Morgan	47,139
Kinetic Construction Ltd.	1,773,383
KTI Limited	82,838
Lafrentz Road Marking,	36,243
Lee Mac Electric Ltd.	54,832
Leighton Contracting (2009)Ltd	4,209,775
LITTLUNIVERSE Education	26,339
MacLeod James	45,739
McElhanney Ltd	68,068
Metro Motors Ltd.	152,506
Minister Of Finance	2,928,866
MNP LLP	49,193
Municipal Finance Authority of BC	103,241
Municipal Pension Plan	1,061,736
North Island Tractor	58,889
North Nanaimo Rental Ltd	46,986
Onsite Engineering Ltd.	275,428
Oyster River Fire Rescue	104,297
Peninsula Co-op	79,309

Town of Comox
Statement of Financial Information
Schedule of Disbursements over \$25,000
For the year ended December 31, 2023

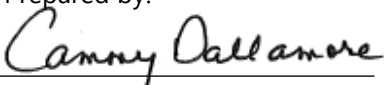
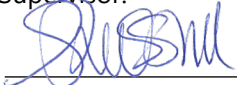

Progressive Systems Ltd.	51,978
Raylec Power LP	96,215
RDH Building Science	25,341
Receiver General For Canada	4,137,273
Riptide Marine Sales Ltd	55,935
Rocky Mountain Phoenix	27,017
Silvacare Inc	48,077
Slegg Building Materials	51,992
Sun Life Assurance Company of Canada	612,585
Suncorp Valuations	27,825
Tayco Paving Co Ltd	79,217
Telus	34,559
Terralink Canada	33,740
The Owners of Strata EPS5411	68,272
Thomson, Steve	63,384
Tower Fence Products Ltd.	46,434
Trane Canada ULC	59,202
Trans-Care Rescue Ltd.	31,190
U.S. Bank Canada	300,774
Urban Systems Ltd.	315,520
Vancouver Island Regional Library	813,945
Vimar Equipment Ltd.	501,301
Westview Ford Sales Ltd.	99,053
Worksafe BC	225,863
Young, Anderson	53,056
Your City Solutions Inc	31,500
	<hr/>
subtotal	38,155,555
Plus all payees disbursed under \$25,000	2,313,906
Total disbursements, all payees	<hr/> <hr/> 40,469,461
 <u>Reconciliation to Total Expenditures reported in Financial Statement:</u>	
Total Disbursements for all payees (reported above)	40,469,461
Plus Council & Employee remuneration	7,120,989
Less remittances of property taxes collected for others (net of offsets) not included in expenditures on the Financial Statements	(8,976,464)
Less long term debt principal payments not included in expenditures on the Financial Statements	(112,941)
Less employees share of payroll disbursements not included in expenditures on the Financial Statements	(2,156,529)
Less GST recoveries from disbursements in year	(857,207)
Less expenditures on tangible capital assets	(12,181,724)
Plus amortization expense from tangible capital assets	3,542,399
Less DCCs, development bonds and deposits refunded in year	(288,797)
Less changes in other asset & liability accounts included in expenses on the Financial Statements	759,939
Total Expenses per Consolidated Statement of Operations	<hr/> <hr/> 27,319,125

Town of Comox
Statement of Financial Information
For the year ended December 31, 2023
Schedule of Payments for Grants or Contributions

Recipient of Grant or Contribution		Purpose of Grant
Comox Business in Action Association	\$ 72,900	Business Improvement Area
Comox Archives & Museum Society	32,500	Operation of Society
Comox Valley Citizens On Patrol Society	4,700	Operation of Society
Comox Valley Community Policing	6,150	Auxiliary Policing & Crime Prevention
Comox Valley Art Gallery	5,000	Gallery Operations
Comox Valley Affordable Housing	10,000	Operation of Society
Comox Valley Transition Society	30,000	Operation of Society
Sid William Theatre Society	25,000	Theatre Operations
Zachary MacQuarrie	500	Mack Laing Bursary
	<hr/>	
	<u>\$ 186,750</u>	

REGULAR COUNCIL MEETING

TO: Mayor and Council	FILE: 0630-01
FROM: Cammy Dallamore, Corporate Coordinator	DATE: August 2, 2024
SUBJECT: Flag Protocol and Community Events Recognition Policies	

Prepared by:  <i>C. Dallamore, Corp. Coordinator</i>	Supervisor:  <i>S. Russwurm, Director of Corp. Services</i>	Report Approved:  <i>Jordan Wall, CAO</i>
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RECOMMENDATION(S) FROM THE CHIEF ADMINISTRATIVE OFFICER:

1. THAT the Flag Protocol Policy CCL-058 as attached to the August 2, 2024, report from the Corporate Coordinator, titled "Flag Protocol and Community Events Recognition Policies" be adopted as amended to incorporate the flag-raising procedures from the Community Events Recognition Policy CCL-059, and to add September 30th, National Day for Truth and Reconciliation, to the protocol for flags flown at half-mast.
2. THAT the discontinuation of the Community Events Recognition Policy CCL-059 as detailed in the July 30, 2024, report from the Corporate Coordinator, titled "Flag Protocol and Community Events Recognition Policies" be Approved.

PURPOSE

To obtain Council's approval to amend the Flag Protocol Policy CCL-058 to:

- include additional procedures on flag raising requests,
- include September 30th, National Day for Truth and Reconciliation, to the protocol for flags flown at half-mast, and
- discontinue the Community Event Recognition Policy CCL-059.

STRATEGIC PLAN LINKAGE

Regular updates and revisions to the Council Policy Manual supports the following Strategic Priority and Area of Focus:

Strategic Priority	Area of Focus
Organizational Excellence: We will modernize and create	Internal process: Streamline and develop new processes

stability to ensure high quality service delivery over time.	that are efficient, cost effective and support our overall objectives.
--	--

BACKGROUND

The "Community Event Recognition Policy CCL-059," originally titled "Proclamation Policy," was adopted on December 16, 2020. It was later amended to include flag raisings and building light ups and renamed on May 18, 2022.

The original "Flag Raising and Flag Protocol Policy CCL-058" also originated from the "Proclamation Policy" and was adopted on December 16, 2020. It was amended on May 18, 2022, to move the language on flag raisings to the Community Event Recognition Policy. Remaining language in the Flag Protocol Policy establishes a consistent protocol for flying flags at all Town facilities, including procedures for half-masting.

On June 5, 2024, Council resolved to no longer approve proclamations and building light up requests, thus removing Item 7.05, "Issuance of Proclamations," and Item 7.07, "Light up of Town Hall" From the Community Event Recognition Policy CCL-059.

DISCUSSION

Subject to Council's approval, and for better alignment and efficiency, it is recommended that the community flag-raising procedures be integrated into the Flag Protocol Policy CCL-058, eliminating the need for the Community Event Recognition Policy CCL-059.

Currently, there are five flagpoles at Town Hall – a set of four in the landscaped area across from the access drive that fly the national, provincial, Town of Comox and K'ómoks First Nation flags, and one community flagpole next to Town Hall that can accommodate requests for flag-raising for different causes and events.

All flag-raising requests previously underwent staff and Council review and were either approved or denied under the Community Event Recognition Policy CCL-059. The recommended Flag Raisings and Protocol Policy CCL-058.03 now handles flag-raising requests similarly to community event requests.

It is recommended that the policy be further amended so that requests for community flag raisings that meet the policy are automatically approved without the need to go to Council. This streamlined process reduces the time Council dedicates to these matters.

IMPLICATIONS

Consolidating protocols into a single policy reduces redundancy and administrative burden.

GOVERNANCE CONSIDERATIONS

Q: How will the proposed amendments affect the process for flag raising requests?

A: Requests for Flag Raisings that meet the updated policy will no longer be forwarded to a Council agenda for approval. Incorporating community flag-raising requests into our current flag protocol policy ensures consistency and efficiency. This streamlines approval processes, saving time and resources, and promotes transparency and accountability in decision-making, aiming to harmonize operations and maintain standardized practices.

*ATTACHED: DRAFT Flag Raising and Protocol Policy CCL-058.02
Community Event Recognition Policy CCL-059*



FLAG RAISINGS AND PROTOCOL POLICY

Section: COUNCIL	Number: CCL-058.02	Office of Primary Responsibility: CORPORATE SERVICES
Type: <input checked="" type="checkbox"/> Policy <input checked="" type="checkbox"/> Procedure	Authority: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Administrative	Approved By: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Chief Administrative Officer <input type="checkbox"/> Department Head
Date Adopted: December 16, 2020	Date Last Amended: May 18, 2024	Date to be Reviewed: December, 2026
Manner Issued: Website, Internal Memo, Upon request		

1 PURPOSE

- 1.01 To establish a consistent protocol for the flying of flags at all Town Facilities.
- 1.02 To provide guidance for flying flags on the Town Hall Community Flagpole for Community Events.

2 SCOPE

This policy applies to:

- (a) the flying of flags at any Town Facility,
- (b) flags flown on the Town Hall Community Flagpole.

3 POLICY PRINCIPLES

- 3.01 Half-Masting is an expression of collective mourning of the Town.
- 3.02 Flag raising events celebrate diversity, civic pride and initiatives, and Town recognition of these events are important to local residents and community organizations.
 - (a) The Town’s consideration of a request for flying flags on the Town Hall Community Flagpole is to fulfill a request rather than endorse the content of the recognition.

4 DEFINITIONS

- 4.01 Council: The Council of the Town of Comox.
- 4.02 Community Events: include local recreational events, cultural celebrations and/or special occasions.
- 4.03 Community Flagpole: Means the single flagpole beside the Town Hall building provided for the purpose of highlighting community events and non-profit initiatives.

- 4.04 Flag Raising: A flag raising is the ceremonial raising of a flag that can occur with or without an accompanying ceremony.
- 4.05 Half-mast and Half-masting: Flags that are lowered to a position that is an equal distance from the top and the bottom of a flagpole, as a sign of respect, mourning and condolence.
- 4.06 National Flag: Means the flag approved by Parliament of Canada as a national symbol of Canada.
- 4.07 Order of Precedence: Means the positioning of the flags in priority of importance, order or rank.
- 4.08 Non-Profit: Any organization that is registered under the British Columbia Societies Act.
- 4.09 Provincial Flag: Means the flag approved by the Legislative Assembly of British Columbia as a provincial symbol of British Columbia.
- 4.10 Town: The Town of Comox.
- 4.11 Town Facility: Includes any Town facility owned, leased or operated by the Town.
- 4.12 Town Flag: Means the flag that represents the Town of Comox.
- 4.13 Town Website: is the official website for the Town of Comox, located at <http://comox.ca>.

5 POLICY

Flag Raising Requests

- 5.01 Requests shall be considered on a first-come, first-served basis.
- 5.02 Requests shall meet the following criteria:
 - (a) Be submitted by a representative of a community or Non-Profit group based in the Comox Valley.
 - (b) Be requested only in respect of activities that support a significant number of residents of the Comox Valley or related events taking place in the Town. This includes:
 - i. Civic promotions,
 - ii. Public awareness campaigns,
 - iii. Charitable fundraising campaigns,
 - iv. Arts and culture celebrations, and
 - v. Special honours for individuals or organizations for special achievement.
 - (c) Must demonstrate respect and tolerance for all Town residents and should foster a sense of community.

- 5.03 Requests will not be approved for:
- (a) Matters of ideological or religious beliefs that may be subject to opposing points of view, or individual conviction;
 - (b) Events or organizations with no direct relationship to the Comox Valley;
 - (c) Campaigns or events contrary to Town policies or bylaws; or
 - (d) Campaigns intended for profit-making purposes;
- 5.04 Items such as decals, signs, banners, brochures, or similar items that accompany a request will not be displayed at Town facilities unless, at the sole discretion of the Chief Administrative Officer, it is determined that they provide information of significant value to residents of the Town.
- 5.05 An organization does not have exclusive rights to any day, week or month related to their Flag Raising event.
- 5.06 Upon approval, the organization must provide the flag to the Corporate Services Department who will confirm the estimated number of attendees to any accompanying ceremony, if applicable. Flags must be in excellent condition

Half-Masting Requests

- 5.07 All Half-Masting requests shall be approved by the Chief Administrative Officer in consultation with the Mayor.
- 5.08 Dignitary Visits
- (a) In the event of a visit of a dignitary to a Town of Comox facility, the Town may fly a courtesy flag on the Community Flagpole representing that dignitary.
 - (b) The flying of a courtesy flag to mark the visit of a dignitary will be made only after consulting the appropriate protocol officer responsible for coordinating the visit.
- 5.09 Flag Protocols and Configuration
- (a) The following outlines the Order of Precedence to be usually flown at Town Facilities.
 - i. Flags Flown Permanently at Town Hall:
From left to right: Provincial Flag; National Flag; Town Flag
 - ii. Flags Appearing Permanently in Council Chambers, Town Hall:
From left to right: Provincial Flag, Town Flag, National Flag
 - (b) Only one flag shall be flown per pole.
 - (c) Where there are only two flagpoles the Provincial Flag and the Town Flag shall be flown.
 - (d) Where there is only one flagpole, the Town Flag shall be flown.

5.10 Flags at Half-Mast

- (a) Flags will be flown at Half-Mast to mark periods of official mourning upon the death of:
 - i. The Sovereign or an immediate member of the Canadian Royal Family;
 - ii. The current or former Governor General of Canada or Lieutenant Governor of British Columbia;
 - iii. The current or former Prime Minister of Canada;
 - iv. The Leader of Her Majesty's Loyal Opposition, Parliament of Canada;
 - v. The current or former Premier of British Columbia;
 - vi. The current or former member of the Town Council
 - vii. A current employee of the Town;
 - viii. A Comox Valley police officer, paramedic, firefighter or other person who dies in the line of duty; and
 - ix. Any other person at the discretion of the Mayor as a symbolic gesture of collective community mourning.
- (b) Flags at Town Hall will be flown at Half-Mast from sunrise to sunset on the following days each year:
 - i. April 28: National Day of Mourning for Persons Killed or Injured in the Workplace
 - ii. June 6: D Day
 - iii. September 30: National Day for Truth and Reconciliation
 - iv. November 11: Remembrance Day
- (c) Flags flown at Half-Mast may occur at all Town Facilities or specific locations.

PROCEDURES

5.01 Flying of Flags on the Community Flagpole:

- (a) Applications for Flag Raising shall be made in writing to the Corporate Services Department using the prescribed form of application and shall be submitted at least four (4) weeks prior to the date of recognition.
- (b) The Corporate Services Department shall review all applications to determine consistency with this Policy, and requests are consistent with this Policy
- (c) Approval of Flag Raisings shall be communicated to all applicants, posted on the Town Website and shared to the Town's social media channels.
- (d) A record shall be maintained of all Flag Raising requests received that shall include when the request was received, whether the request was granted or

denied, and the dates the Community Flag was flown.

- (e) The Community Flagpole at the Town Hall shall be used for Courtesy Flag Raisings.
- (f) The Town will endeavour to fly Community Flags as scheduled; however, civic uses and emergencies take precedence.
- (g) Community Flags shall be hung for a maximum of seven (7) days.

5.02 Half-mast Procedures

- (a) All flags flown together at Town Facilities shall be flown at Half-mast together.
- (b) In the case of a national or provincial official, flags flown at Half-mast shall be for the duration established by the appropriate federal or provincial protocol offices.
- (c) In the case of other persons, flags will be flown at Half-mast from the date the notice of death is received until sunset of the date of the funeral for that individual.
- (d) A flag is brought to the Half-mast position by first raising the flag to the top of the mast, and then immediately lowering it, slowly to the Half-mast position.

6 REFERENCES AND RELATED STATEMENTS OF POLICY AND PROCEDURE

Government of Canada – Rules for Flying the Flag - <http://www.pch.gc.ca/eng/1359048153800>

Flag Raising Request Form

Amendment Date	Section Amended or Description of Amendment	Resolution Number
Dec 16, 2020	CCL-058.00. Policy adopted (Flag Raisings and Flag Protocol Policy)	2020.426
May 18, 2022	CCL-058.01. Renamed to Flag Protocol Policy. Removed Community Flag Raising policy and procedures and added to Special Events Recognition Policy; removed flagpole located at Town Circle.	2022.194
August 7, 2024	CCL-058.02. Policy renamed to Flag Raisings and Protocol Policy. Flag raising policy and procedures from the Community Event Recognition Policy incorporated into amended policy.	



ECONOMIC DEVELOPMENT STRATEGY

July, 2024



TOWN OF
COMOX

EXECUTIVE SUMMARY

Comox is shaped by its relationship with water. Water inspires a dynamic waterfront and marina, a postcard-friendly downtown, and a marine-focused defence sector.

That Comox is a special place is reflected in an Economic Development Strategy that appreciates and seeks to enhance a BIG THREE infrastructure foundation that drives both economy and quality of life: MARINA, DOWNTOWN, and AIRPORT(s), while also recognizing a desire to achieve “better” not bigger for the sake of it. Comox aims to achieve community and economic development in balance with its inspired natural environment.

Vancouver Island and Comox are changing. Population growth is accelerating. Housing policy shift will shape affordability solution-seeking and attract a more youthful demographic, while also housing workforce for a Top 3 industry sector in Comox: tourism and retail.

There’s a desire to nurture a self-sufficient community that creates quality jobs, generates revenue that helps pay for desired services and amenities, and fully recapitalizes the Town’s assets – from streets to recreation facilities. Further INDUSTRIAL LAND is required to achieve this.

Entrepreneur ecosystem development, key industry sector development, and Town of Comox operationalization of economic development are key ENABLERS that help realize economic development goals.

Today we are healthcare service, defence, tourism and retail. Our DIVERSIFICATION efforts will additionally support Health and Wellness, Manufacturing, Agrifood, “Marine” (aquaculture/fishing), and attraction of business-minded lifestyle-seekers.

These community-building elements are inter-connected; they rely on each other to succeed. Our connectedness as a community gives us the strength we need to legacy-make with ambition.

COMOX. LIFE BY THE SEA.

“Great strategies aren’t about everything we do on a day-to-day basis; they solve challenges and/or step into opportunities. Great strategies are about catalytic intention and ambitious vision, connected to stepping-stone initiatives with best return on investment in achieving vision.”

August 7, 2024, Regular Council Meeting

Created with input from industry sector analysis, five key community group meetings interviews, two key community leaders workshops, a community survey, and extensive interaction with Town of Comox Administration and Council.



OUR VISION

Life by the Sea

OUR STORY

Seaside Comox. BC's most charming waterfront community. Human scale. Inter-generational. Safe, clean, and green. A harmonious blend of small-town feel and dynamic economic development and tourism. We embrace our infrastructure advantage in pursuit of a dynamic downtown, waterfront, and airport.

Our downtown at the soul of us is made for strolling, with a unique boutique atmosphere featuring green spaces, local businesses, and welcoming common areas.

Our waterfront will become a premier destination, attracting locals and visitors with exceptional dining, shopping, and recreational opportunities.

Commerce, industry, and entrepreneurship initiatives generate community vibrancy, and affordable services and amenities. Diverse housing options deliver affordability for all ages.

We will prioritize sustainable growth, ensuring new developments respect our town's aesthetic and connected

August 7, 2024, Regular Council Meeting



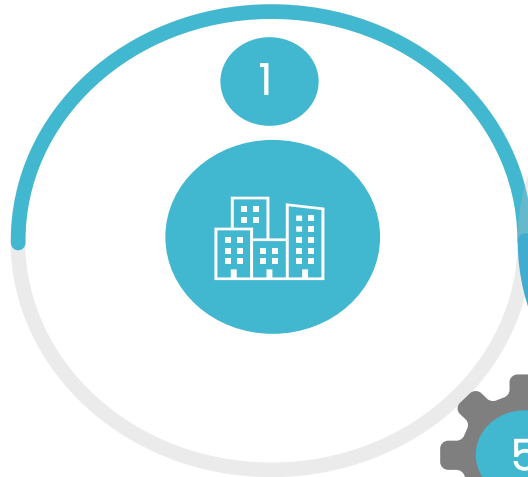
GOALS

GOALS are the way we will achieve our Economic Development Strategy. These goals are interconnected, ambitious, and achievable in pursuit of the transformative.

LEVERAGE OUR
INFRASTRUCTURE
ADVANTAGE

Dynamic Downtown

Enhance downtown as a
centre of civic life.



Waterfront Way

Create a destination
waterfront.



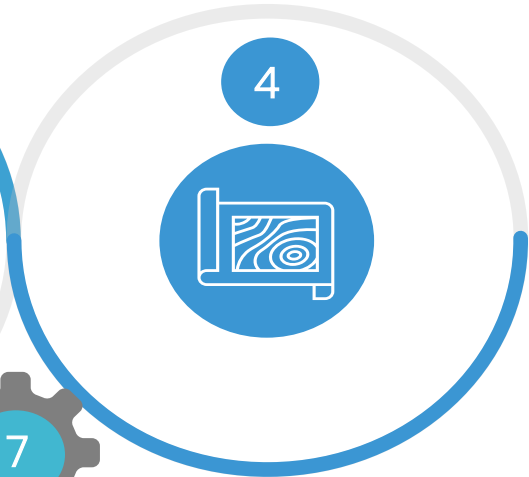
Airports Amplified

Enable expansion of Comox
Valley and waterfront airports.



Employment Lands Expansion

Secure additional industrial
land and enable industrial
investment.



ANIMATE
ECONOMIC
DEVELOPMENT



Entrepreneur Ecosystem

Support business retention &
expansion and creation of more
location options.



Economy Expanded

Step into focused opportunities
in key industry sectors: Tourism,
Retail, Health & Wellness,
Manufacturing, Agrifood.



Economic Development Operationalized

Resource economic
development capacity
and nurture investment-
friendly policy.

VALUE PROPOSITION

WHY DO WE MATTER? Answer this question and 80% of a community's economic development pathway is defined via connection to people and enterprise who best connect with value proposition. And the more unique or powerful the value proposition, the further the lengths people will go to pursue it.



Defence Industry

#2 industry sector in Comox (17% of total employment). #1 export sector (34% of total exports). Related aerospace products and parts, industry machinery manufacturing activity.



Infrastructure

The "Big 3": Marina, Airport, Downtown



Growing Population

1.7% annual population growth 2001-2021.

-Lifestyle Factors-

Community Survey Top 5: 1) Small community feel / friendly, 2) Natural environment / beautiful, 3) Safe, 4) Waterfront/marina, 5) Outdoor recreation opportunity.

Lifestyle factors deeply valued by local residents are a value proposition for resident attraction strategy (digital nomads, remote workers, creators, independent entrepreneurs).

DYNAMIC DOWNTOWN

The Downtown is the SOUL of the Town of Comox – the heartbeat of commerce. It’s where we connect to people and place, where we find inspiration in our most distinctive architecture and creativity of nature and humanity... where visitors and investors form their most memorable impression. As a #1 community survey infrastructure development priority, a Downtown Enhancement Action Plan was generated in conjunction with the Economic Development Strategy.

High Priority Public Investments:

- Parks and open spaces
- Walking and biking connections into/in the downtown incl. waterfront walkways / access
- Designated Market building / space – outdoor, indoor
- Plantings/public art/lighting/benches
- Sizeable town square
- Street improvements
- Public pop up / permanent commercial use/decking – east side of harbour

Most Supported Enhancement Initiatives:

- Hotel(s)
- Marketing the Town
- Events
- Mixed-use development
- Business retention and recruitment initiatives

Src: Community Survey, 2024



GOAL

1 Enhance downtown as a centre of civic life.

#1 Infrastructure Enhancement Priority – 70%
August 7, 2024, Regular Council Meeting
Support for further public investment – 89%

Src: Community Survey,
2024

GOAL 1

DYNAMIC DOWNTOWN: ENHANCE DOWNTOWN AS A CENTRE OF CIVIC LIFE



1.1 Downtown Enhancement Action Plan

1.1.1 Implement Downtown Enhancement Action Plan (2024).

1.1.2 Review Action Plan annually for successes, challenges and opportunities identification

1.1.3 Conduct capital planning for downtown municipal infrastructure / amenity investment.



1.2 Downtown Enhancement Action Plan: 6 Small Moves

Implement 5 Small Moves to generate downtown enhancement momentum:

1. Organize for Success

Create a "Downtown Leadership Group" consisting of the BIA, Council, Golf Course, Marina, and Comox Avenue Businesses, plus other younger "doers" in the community that provides a base for short term actions and resources.

2. Central Greenway and Pedestrian Connections to Waterfront

Develop a master plan of improvements for the connectors between Comox Avenue and the Waterfront to Marina Park.

3. Comox Avenue "Streets Alive" Program

Experiment with the Comox Avenue "Flex" Street concept that temporarily closes Comox Avenue between Port Augusta and Nordon Street that fronts onto the Comox Centre Mall. E.g. Saturday Farm and Fish Market pilot project.

4. Golf Course Green Loop

Build a multi-use path (3-4 metre width) with an initial east or west side Phase 1 that doesn't impact golf course operation.

5. Waterfront Connector- Rotary Park to Main Marina Breakwater Walkway

Build phased projects to develop the Comox waterfront as a continuous and connection waterfront amenity and trail network (hospital to Goose Spit). Phase 1 - extend the boardwalk along the waterfront to the breakwater boardwalk along the outside of the marina.

6. Placemaking

Develop and implement a Top 5 pragmatic, small beautification initiative action plan.



GOAL 1 DYNAMIC DOWNTOWN: ENHANCE DOWNTOWN AS A CENTRE OF CIVIC LIFE

Objective: the Town of Comox achieves these strategic objectives at zero net cost while enabling a standard 15-20% private sector return on revenue / expenses business model.

1.3 Municipal Property Strategy

Develop a Town of Comox downtown, municipally-owned land strategy that:
a) consolidates community services more efficiently, that in turn; b) links a number of catalytic actions that enhance placemaking and generate investment attraction and business retention/expansion:

Parking – creation of more efficient consolidated parking.

Civic Centre – mixed-used building including municipal government, community groups, housing, and amenity space.

Housing / Rental Accommodation – to support retail and tourism workforce attraction & retention.

Planning Policy Investment Enablement (OCP/Zoning) – via permitted uses with priority on retail, craft manufacturing with associated retail, and mixed-use, 4-6 storey height permissions with water facing step-down architecture to preserve views, greater site coverage, and more flexible parking standards.

Health Services Building / Prof. Services Consolidation – nurture 2nd floor location or additional professional services building to provide more ground level retail / entertainment opportunity.

Waterfront District – private sector land development opportunity on water facing municipal properties, supported by further waterfront amenity development to create a water facing “Waterfront District”

CATALYTIC ACTIONS

Retail – Town catalyzes new retail opportunity via project builds, joint venture development, or town-owned property development requirements.

Community Group Shared Space – creates property redevelopment opportunity while providing cost efficiency for community groups.

A Golf Course That Evolves – with a boundary trail and potential mixed-use clubhouse.

Water Viewshed Placemaking – infrastructure, municipal property, and private sector investments that nurture Port Augusta St. second viewshed / activity anchors as a second Mainstreet in addition to Comox Ave.

COMOX DOWNTOWN ENHANCEMENT ACTION PLAN

Shaping a greater downtown destination by the sea



9 BIG MOVES

Nine initiatives (in order of priority) form the core of the Downtown Enhancement Action Plan. These initiatives were created in two community workshops.

- 1. Organize for Success** – more support from various organizations in a group effort referred to as the *Comox Downtown Action Partnership*.
- 2. Marinas Areas and Waterfront** – connections to Seawall Walkway, pedestrian connection to Fisherman’s Wharf extended pier, extend waterfront walkways and non-motorized boat launches, redevelop privately-owned parcel W of Marina Park, Harbour Air terminal.
- 3. Residential Mixed-Use Intensification** – 4-6 storeys west of Comox Ave, Legion Hall, C and W Beaufort Ave.
- 4. Business Retention & Recruitment** – retain, expand, and recruit specific businesses.
- 5. Golf Course Public Recreational Asset** – exterior trail loop, Town park/parking south end, extend Buena Vista Ave.
- 6. Greater Downtown Approach** – links the enhanced “Golf Course Park” with the Marina seaside amenities in addition to Main Street (Comox Avenue). “Park your vehicle once approach”. New meeting place – Town Square.
- 7. Comox Avenue “Street Alive” Program** – events program, flex street block, bulges for traffic calming, parking patios, extend mid-block sidewalks.
- 8. Town Square Meeting Place** – Comox Ave and Port Augusta St.
- 9. Civic Centre** – mixed-use Town office and residential (4 storey), pathway/park. N side Beaufort Ave.

WATERFRONT WAYS

The Waterfront is the SPIRIT of the Town of Comox...a top lifestyle opportunity in the minds of Comox residents. Water is the simplicity of sound, movement, and a breath of fresh air. Water is the complexity of commerce – in an active fishing, aquaculture, and transportation economy. The Waterfront is recreational, in a stroll, or on a boat. A high experience, multi-purpose waterfront is rare in BC and Canada. The Economic Development Strategy focuses on a set of anchor enhancement activities that will further elevate the waterfront experience.



GOAL

2 Create a destination waterfront.

#2 Infrastructure Enhancement Priority – 48%

#4 Lifestyle Factor

August 7, 2024, Regular Council Meeting

Src: Community Survey,
2024

GOAL 2 WATERFRONT WAY: CREATE A DESTINATION WATERFRONT

2.1 Infrastructure – Private Sector



- 2.1.1 Consider a private-public partnership for potential waterfront hotel.
- 2.1.2 Conduct Town advocacy for marina enhancements via Harbour Authority, Gas and Go, and Private Marina.

2.2 Placemaking



- 2.2.1 Add marine-themed art throughout the current marina and downtown.
- 2.2.2 Install new lights on all pathways from Comox Ave. to Marina Park, and in Marina Park waterfront access.
- 2.2.3 Install new signage for the harbour for sport fishing, water taxi, charters, scenic tours, and sailing.
- 2.2.4 Add additional benches, view stations, and fish cleaning stations.

2.3 Infrastructure – Town



- 2.3.1 Assess state of repair of existing breakwaters.
- 2.3.2 Create a floating breakwater.

2.3.3 Consider municipal ownership as a matter of preservation of amenity, climate change adaptation, and grant potential.

2.3.4 Prioritize transient moorage mapping for specific locations outside the breakwaters with dedicated buoys and provide oversight for use.

2.3.5 Install new decking along Harbour Wharf on the west side to connect to breakwater at Harbour authority, including closure of current sidewalk, and addition of commercial addition of vehicle gate.

2.3.6 Create additional floating commercial spaces in the Town marina, and dedicated commercial tourism operator moorage access along dock A and near float buildings.

2.3.7 Install new dock ramp signage and summer monitoring.

2.3.8 Create new off-site parking for boat trailers during the season, and a winter boat storage yard owned by the Town.

2.3.9 Increase size of existing waterfront access within the Town.

2.3.10 Create a dedicated swimming platform.

2.3.11 Create a dedicated fishing platform.

2.3.12 Create a dedicated kayak and paddleboard launching site usable at low tide.

2.3.13 Expand the current waterfront: west to old hospital site; and east to Goose Spit.

AIRPORT(S)

AMPLIFIED

The Comox Valley Airport is a primary transportation gateway for central Vancouver Island. Approximately 400,000 passengers use the airport annually. Airports are centres of commerce, and are a Top 5 community location factor for “The Independents” (creators, digital nomads, hybrid workers, entrepreneurs, creative industry, professional services). Airport Master Planning predicts a doubling of passenger traffic over the next 30 years. Comox is additionally served by Harbour Air out of the marina, and seeks to nurture enhanced water flight service levels. The Economic Development Strategy focuses on support for enhanced airport service and physical expansion.

GOAL

3 Enable expansion of Comox Valley and waterfront airports.

#4 Infrastructure Enhancement Priority – 21%

Src: Community Survey, 2024



GOAL 3 AIRPORTS AMPLIFIED: ENABLE AIRPORT(S) EXPANSION



3.1 Advocacy

3.1.1 Support the recommendations of the Comox Valley Airport Master Plan (2024).

3.1.2 Conduct Town advocacy for airport enhancements based on Comox Valley Airport and Harbour Air expressed requirements.

3.1.3 Work with the Association of Vancouver Island and Coastal Communities (AVICC) to bring Uber and other rideshare options to all of Vancouver Island, including Comox Valley Airport. Gain the support of airports, CFB Comox, Chambers of Commerce, economic development organizations, and tour companies.

3.1.4 Advocate for enhanced BC Transit service to Comox airport.

3.1.5 Advocate for/consider municipal role in breakwater extension and enhancement to secure extended-season Harbour

Air service



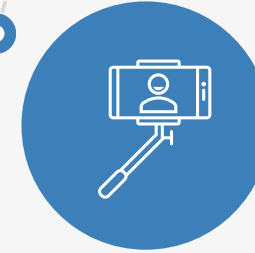
3.2 Parking

Support the Comox Valley Airport in securing a doubling of parking space requirement.



3.3 CFB Comox

Ensure the Town aligns to, and supports the needs, of 19 Wing, advocates on behalf of 19 Wing needs, and otherwise builds economic development capacity to attract increased investment to 19 Wing and related industries.



3.5 Tourism

Embrace the Comox Valley Airport as “Comox’s airport”, and develop a long-term intercept strategy to identify gaps in tourism destination development. Create clear solutions for an airport business parks and related services including car rentals, food and beverage, and accommodations.



3.4 Accommodation

Pursue investment attraction of a hotel on airport-adjacent lands.

EMPLOYMENT LANDS EXPANSION

Industrial land connects dots between quality job creation, key industry sector development, and generation of ability to pay for resident-desired services and amenities. Land must have competitive advantage. For the Town of Comox, advantage can be derived from proximate location to CFB Comox, anticipated land price/business cost advantage compared to major urban west coast BC markets, and lack of industrial land supply in central/south Vancouver Island. Comox has two key employment land components: 1) Commercial land – where intensification of use in the downtown core (retail, professional services, entertainment, accommodation and food services), and further development of the Anderton commercial services corridor offer further development opportunity; 2) Industrial land – where additional land supply contiguous with existing, limited land supply to the northwest of the airport offers further opportunity. Comox currently has half the regional average portion of total tax non-residential tax assessment.

GOAL

4 Secure additional industrial lands and enable industrial investment.

#3 Infrastructure Enhancement Priority – 28%
August 7, 2024, Regular Council Meeting

Src: Community Survey,
2024



GOAL 4 EMPLOYMENT LANDS EXPANSION: SECURE ADDITIONAL EMPLOYMENT LANDS AND ENABLE INVESTMENT

4.1 Secure Additional Industrial Lands

Implement the updated (2024) recommendation of the Development Taxation Projection Project – which estimates need for a minimum of 65 acres of additional industrial land to reach a 7.4% non-residential assessment ratio target (regional average), including consideration of annexation, and/or municipal purchase.

Note: this projection is over and above current Town of Comox industrial land supply of 76 acres gross, assuming these lands are fully developable vis a vis current stormwater-related challenges.

Matter of Fact:

Non-residential assessment is 4.7% of total assessment (2024). Assessment per acre is 50% higher than residential land. Every 10% of industrial density increase would generate an additional \$530K in annual municipal revenue



4.2 Planning Policy

4.2.1 Ensure OCP and Zoning Bylaw updating (2024) conserves industrial land designations for industrial and knowledge-based uses, and eliminates potential for retail or wholesale development on industrial lands.

4.2.2 Create an incentive package to attract new industrial investment, including Revitalization Tax Incentive, and fast-track permitting process.



4.4 Infill

4.4.1 Pursue an infill strategy to achieve additional commercial and industrial assessment via: a) a refreshed planning policy framework that enables density (e.g. reduced setbacks, lower parking standards, increase site coverage, greater building height, smaller minimum lot width and lot size); and b) completion of an under-utilized commercial and industrial land inventory (# acres occupied, vacant, infill potential), to identify infill opportunities.

4.4.2 Consider exemption of all non-residential uses from parking standards Ref: [Parking Reform Mandates Map](#). E.g. Village of Canal Flats. [City of Edmonton](#): parking as a key barrier to delivering missing middle housing forms due to cost and site constraints. [City of Vancouver](#)

4.4.3 Conduct a parking study (drone) to establish each of peak and non-peak time use to establish evidence-based rationale for reduction of non-residential parking standards. [Reference](#)



4.3 Infrastructure

Review current storm water pond infrastructure and conduct an audit of current development infrastructure requirements with the goal of aligning best practices to municipal airport adjacent lands in BC, western Canada and specific airports with past and current air force bases.

ANIMATE ECONOMIC DEVELOPMENT

Entrepreneur ecosystem development, key industry sector development, and Town of Comox operationalization of economic development are key catalysts that enable achievement of economic development goals.

Community Identified Key Economic Development Enablers:

1. Downtown enhancement
2. Housing affordability
3. Waterfront walkway expansion / enhancements
4. Public transit
5. Non-residential land supply

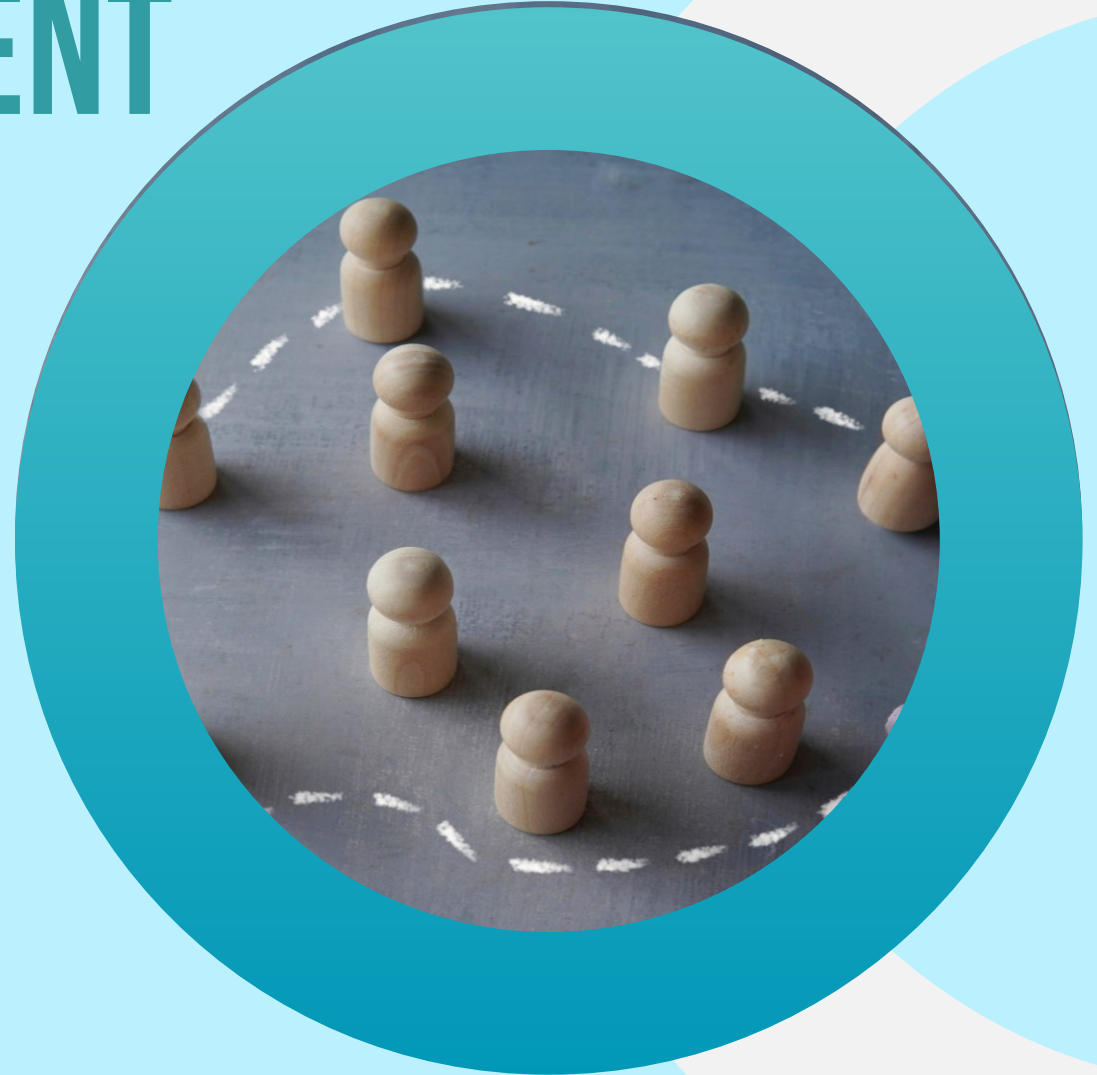
Src: Community Survey, 2024

GOAL 5 Support business retention & expansion and creation of more location options.

GOAL 6 Step into focused opportunities in key industry sectors.

GOAL 7 Resource economic development capacity and nurture investment-friendly policy.

August 7, 2024, Regular Council Meeting



GOAL 5

5.1 Business Retention & Expansion Program

Conduct a focused business retention and expansion program.

Guidance for program content can be found in priorities identified in the Community Survey, 2024:

Business Retention & Expansion:

- 1) Love local marketing
- 2) Signature events
- 3) Showcase local products
- 4) Tax incentives
- 5) Celebrate successes.

Business Programming:

- 1) Affordable location
- 2) Networking opportunities
- 3) Multi-family in residential zones
- 4) Help with business succession
- 5) Business planning advice.



GOAL 5 ENTREPRENEUR ECOSYSTEM: SUPPORT BUSINESS RETENTION & EXPANSION AND CREATION OF MORE LOCATION OPTIONS.



5.2 Enable Missing Middle Commerce Spaces that Help Enable Business Growth

Pursue creative physical space solutions to bridge the gap between home-based business and larger scale commercial and industrial enterprise.

Guidance for enabling entrepreneur spaces can be found in priorities identified in the Community Survey, 2024:

- 1) More mixed-use commercial – residential
- 2) Expand allowance for temporary uses
- 3) Small commercial space in marina
- 4) Permanent market space – outdoor / indoor
- 5) “Open streets”

..

A growing share of the workforce is “independent” (a catch-all to describe creators, digital nomads, tech, knowledge workers, hybrid workers, entrepreneurs, and professional services) out of intent and desire – the product of forces including but not limited to enabling technology like network connectivity and advanced network speeds, diminishing corporate loyalty, millennial and Gen Z interest in more fluid work relationships, globalization that has translated to global competition for and movement of talent, ease of transportation, and societal shift to a knowledge-based economy that is more conducive to footloose working relationships. Add the influence of a pandemic that will have a legacy impact on the nature of flex work.

Key economic development enablers for a self-employed/home-based segment of economy are: advanced broadband, housing choice, affordability and flex form amenable to enterprise space, placemaking initiatives, downtown vibrancy, quality of life amenities (breadth, quality, uniqueness, quality of experience in both recreation and entertainment), cultural diversity and openness, and enabling policy (e.g. home-based business). Specific economic sectors like transportation, technology, creative industries, and professional, scientific, and technical services are more conducive to self-employment or work from home.

Key community benefits to nurturing self-employment and work from home include: reduced commuter ratio and more local spending that grows retail and services opportunities, reduced crime via increased full-time population, stronger social and cultural connectivity, and environmental benefit (e.g. CO2 emissions) of reduced commuting.

Roughly 20% of the Comox workforce is self-employed and/or works from home – about BC’s “gig economy” via entrepreneurial enablement offers a KEY means to diversify the local economy. Research also indicates approximately 80% of investment is local.

GOAL 5

GOAL 5 ENTREPRENEUR ECOSYSTEM: SUPPORT BUSINESS RETENTION & EXPANSION AND CREATION OF MORE LOCATION OPTIONS.



5.3 Procurement

Pursue procurement as a portion of a business retention and expansion strategy.

A source of investment attraction potential is represented by supplier industries who provide services to major industry. Supply chain opportunities may involve the substitution of currently imported products and services or the expansion into export markets by local companies. Local industry has a self-interest in shorter supply chains and being physically closer to key partners and suppliers. Import replacement can be considered a form of circular economy via shortening of supply chains for local economic activity.

Opportunity is identified as purchasing dollar size x percentage of imported purchases.

Matter of Fact – Top 10 Comox Valley Importing Industries:

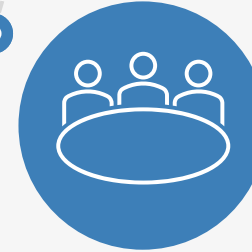
	Imported Purchases \$	% Imported
1. Real estate	\$56.9M	91%
2. Architectural, engineering and related services	\$42.4M	67%
3. Banks	\$35.7M	70%
4. Plastic product manufacturing	\$31.8M	100%
5. Insurance carriers	\$21M	69%
6. General freight trucking	\$21.6M	89%
7. Architectural and structural metals manufacturing	\$19.4M	83%
8. Other financial investment activities	\$18M	88%
9. Support activities for forestry	\$14.4M	54%
10. Office administration services	\$14.9M	68%

There is a key professional services opportunity (architectural / engineering, management, scientific / technical services, accounting, insurance, computer systems design, legal, financial) totaling an annual approximate \$125M.



5.4 Business Licenses

NAICS-code business license list to align with key target industry sectors (mining, tourism, transportation, tech) so as to better understand small and medium-sized enterprise presence (e.g. home-based business), and entrepreneur development opportunities.



5.5 Economic Development Summit

Hold an annual Economic Development Summit to discuss industry sector development, successes, issues, and opportunities.

GOAL 6

6.1 Industry Sector Development Plans

Create sector development plans for key diversification sectors: Tourism, Food and Beverage, Marine (aquaculture, fishing), Retail, Health & Wellness, Manufacturing, “The Independents”.



GOAL 6 ECONOMY EXPANDED: STEP INTO FOCUSED OPPORTUNITIES IN KEY INDUSTRY SECTORS

Community-Identified Economic Diversification Priorities:

Top Tier (>40%)

- Art, Culture & Recreation
- Marine / General Tourism

Mid-Tier (>30%)

- Retail
- Healthcare Services Hub
- Technology
- “Green” Economy

Src: Comox Community Survey, 2024

Industry Sector Development Pathways

An industry sector development framework that can guidepost future sector development work is as follows:

- 1) Resource assessment (e.g. minerals as base mining draw).
- 2) Business/value chain inventory.
- 3) Sector enterprise and ecosystem engagement to understand opportunity space.
- 4) Vision and Goals.
- 5) Targeted Services and Infrastructure.
- 6) Regulatory Framework (local permitting process, advocacy re enabling provincial and federal policy).
- 7) Skills Development (training programs, workforce attraction and retention).
- 8) Technology and Innovation (research and development, demonstration, adoption).
- 9) Marketing and Promotion (investor outreach, networking and promotion events).
- 10) Partnerships (public/private, regional).
- 11) Environment (promote responsible practices and environmental stewardship).
- 12) Monitoring and Evaluation (key performance indicators to measure sector development performance).

The Town of Comox’s top industry sectors have significant (for this type of data reporting) strength relative to comparative communities; Healthcare (#1 industry sector in Comox (22% of total employment – nursing care, health practitioners, out-patient care, physicians, dentists); and Defence (#2 industry sector in Comox (17% of total employment), #1 export sector (34% of total exports)) with associated aerospace products and parts, and industry machinery manufacturing activity. At a secondary level, industry sectors with modest sector presence in the Town of Comox are: Accommodation and food services; Retail; Professional, scientific and technical services; Educational Services. These sectors represent a mix of the Town of Comox’s proximity to CFB Comox and its defence industry heartbeat, and a core “service centre” economic functionality.

Of note, Retail and Accommodation / Food Services – bellwethers of a local tourism economy and bedrock of the local commercial sector, represents a combined 17% of Comox workforce. Rents and house purchase prices are not in reach based on average wages in these sectors, requiring housing affordability/choice solutions to generate tourism workforce availability.

GOAL 7

7.1 In-House Economic Development and Tourism

7.1.1 Create a Tourism Commission Bylaw to establish first steps in offering a converged Town of Comox economic development and tourism function.

7.1.2 Establish a funded Economic Development Department with a Manager-level Economic Development Officer.



GOAL 7 ECONOMIC DEVELOPMENT OPERATIONALIZED: RESOURCE ECONOMIC DEVELOPMENT CAPACITY AND NURTURE INVESTMENT-FRIENDLY POLICY.



7.2 Out-Sourced BRE

Contract with the BIA to: 1) Operate a business retention & expansion program on behalf of Town of Comox economic development. Include development of an annual work program, performance metrics, and quarterly written/in-person BIA update to the Town of Comox; 2) Support implementation of the Downtown Enhancement Plan.



7.3 Planning Policy Enablement

7.3.1 Implement investment-enabling Hybrid Code in OCP and Zoning Bylaw refresh. [Hybrid Code backgrounder](#).

7.3.2 Nurture infill housing policy embrace with the following priorities: 1) Planning Guide to help residents understand provincial housing policy; 2) Establish height restriction; create an Accessory Dwelling Unit Guide and fast-track process (e.g. Seattle); 3) Create development pre-approvals for “template” housing/accessory dwelling units; 4) Conduct information sessions to communicate housing policy change impact/opportunities for residents; 5) Create an Infrastructure asset management plan to project impact and plan for the future. Src: Community Survey, 2024

7.3.3 Establish an Advisory Design Panel (e.g. [Langley City](#)) in association with Hybrid Code to focus development review on aesthetic (performance) vs function.

7.3.4 Host an annual builder/developer meeting to deepen development industry knowledge and connectivity. In year one, include a development economics session to inform the Town about construction economics.

7.3.5 Review the home-based business bylaw.

7.3.6 Fully implement the Development Process Review (2023). **Page 281**

7.4 Tell Your Story

Implement foundation-building investment attraction marketing initiatives as follows:

1) Refresh Business section of Town of Comox website with site architecture and content focused on provision of information to investors: – comparative value proposition (vs raw data), key industry sectors, site-selection focused investor data and information, sector news, and success stories;

2) Investment Guide focused on value proposition, map, economic development vision, key facts (demographics, industry profile – comparative), investment opportunities, development incentives, quality of life amenities, business resources. E.g. [District of Squamish](#) ;

3) Resident Relocation Guide that includes targeting of “The Independents” – e.g. [Canal Flats](#) - Guides: [Resident Relocation](#), [Digital Nomad/Creator](#). Include a section on professional services/healthcare professionals recruitment.



GOAL 7 ECONOMIC DEVELOPMENT OPERATIONALIZED: RESOURCE ECONOMIC DEVELOPMENT CAPACITY AND NURTURE INVESTMENT-FRIENDLY POLICY.



GOAL 7

7.5 Healthcare

Participate in region-based care healthcare professional recruitment campaigns.

An Investment Guide is essentially a re-statement of an investment attraction-focused website. An Investment Guide is a convenient way to integrate with community websites/regional entities, etc. – generating an easily downloadable single-source Guide that can be easily linked. A printed output can be used as a key marketing tool at events, trade shows, etc.

4) Investment Opportunity Sheets for investment attraction priorities.

5) Key Industry Sector Profiles.

6) Development Guide (planning) to facilitate investment.

7) Tourism Guide focused on differentiated products and trip planning information

Differentiated tourism products are identified as: waterfront and marine focused: beaches, fishing charters, Harbour Air, yacht charters, water sports, water taxi to islands, sailing, paddle board, kayaks, diving, the marina and Goose Spit, boat launch in Lazo (incl. waterfront and campground).

Brand and marketing revolves around communication of the Comox value proposition to investment audiences.



GOAL 7
ECONOMIC DEVELOPMENT
OPERATIONALIZED: RESOURCE
ECONOMIC DEVELOPMENT
CAPACITY AND NURTURE
INVESTMENT-FRIENDLY
POLICY.

7.6 Placemaking

7.6.1 Develop a “Top 3” pragmatic / small beautification 2-Year Action Plan.

7.6.2 Establish a Small Projects Grant Program – e.g. [City of Surrey](#), [City of Surrey Beautification Programs](#)

7.6.3 Socialize community connectivity initiatives re linked research, social media posting, and consideration of a “Spirit of Comox” implementation-focused working group. E.g. [73 Community Service Project Ideas](#), [45 Resident Event Ideas That Actually Work](#).

7.6.4 Pursue [Island Coastal Trust funding](#) for community development/beautification initiatives – e.g. Business Façade Improvement, Community Places, Cultural Infrastructure funding streams.

7.6.5 Share research with residents that draws a link between placemaking investment, and community benefit to strengthen support for continued placemaking investment and design/development standards. E.g. Researched positive impact of placemaking: improved safety, increased economic activity, social interaction, community well-being, environmental sustainability, increased property values.

Src: Harvard Graduate School of Design, Canadian Architect and Creative School Studies – Toronto Metropolitan University.

Key Performance Indicators



1

Employment Lands

Current: 76 acres gross

Metric: 65 acres gross additional

Additional industrial land will generate quality jobs and contribute to fully-capitalized municipal asset management.



2

Non-Residential Assessment Ratio

Current: 4.7% of total assessment is major/light industry, business

Metric: 7.4% 2030

The non-residential assessment ratio fell by -1.9% between 2014 and 2023.. In 2023 Comox ranked 12th lowest of BC's 160 urban municipalities. A rising ratio will indicate success in securing additional non-residential land, and investment attraction.



3

"The Independents"

Current: 17.7% of total workforce. BC 17.6%

Metric: 25% of workforce self-employed by 2026.

Comox has a lifestyle-led value proposition and foundational amenity (e.g. downtown/waterfront) and transportation infrastructure (e.g. airport) that are highly desired by independent workforce. A rising proportion of self-employment indicates success at resident/entrepreneur attraction programming, and economic diversification. **Page 284**

LIFE BY THE SEA

There's something Zen about the sea. In the rhythm of the water there is a quieting of the mind. Freeing. The sea isn't just a place; it's a state of mind, a inspiration of the heart, and a playground for the curious soul.

Unleash your creative revival. Find joy of life. Away from the big city hubbub and costs. Simpler pleasures. In Comox.

- International airport.
- Lower business and housing costs than the big city.
- A postcard-friendly downtown.
- A waterfront that sings.





Royal Canadian Legion
Comox Branch 160

1825 Comox Avenue
Comox, BC V9M 3M3
Tel:/Fax: 250 339-2022
Email: branch160office@comoxlegion.ca
Website: <http://comoxlegion.ca/>

July 18, 2024

RECEIVED

July 22, 2024

Mayor Nicole Minions
Town of Comox
1809 Beaufort Ave
Comox, BC V9M 1R9

TOWN OF COMOX

LOG: 24-292	REFER:	AGENDA: RCM 07-Aug-24
FILE: 0230-20	ACTION: MR	

Copies: Council
JW/SR/CD/SA/CP/GS

Mayor Minions and Council,

In anticipation of the upcoming Poppy campaign, we once again request your permission to hold out annual Remembrance Day Parade on Monday, November 11, 2024

We respectfully request the use of Comox Ave for the following:

1. (a) Block off Comox Ave from Ellis to Nordin, 10:00am to 11:30am for the Remembrance Day Parade and Ceremonies, for which we require the road permit.
- (b) Use of the two (2) reviewing stands and barricades
- (c) Permission to put up no marking signs on both sides of Comox Ave between Church and Port Augusta from 7:00am to 11:30am

We further respectfully request a letter drafted to the Branch approving a flyby by the RCAF with the following working to be included:

"The municipality of Comox, British Columbia, Canada is supportive of a flyby by RCAF Aircraft as low as 500 feet for the planned Remembrance Day Ceremony being held in Comox, BC Canada, for transit, practice and shows on November 11, 2024"

Your attention to this request is very much appreciated. The flyby is under Special Events out of Winnipeg, thus written permission granted is required at your earliest convenience and we look forward to your prompt reply.

Yours in Comradeship

Valerie Greenlaw
Poppy Chair
Royal Canadian Legion Branch 160 Comox

RECEIVED

Copies: Council
JW/SA/CP/SR/CD/EH



SIMBA
INVESTMENTS

August 1, 2024

TOWN OF COMOX

LOG: 24-313	REFER:	AGENDA: RCM 07-Aug-24
FILE: 3320-20-22	ACTION: MR	

Dear Mayor and Council:

Cfile: 3320-20-22.01 Pritchard 1000

Over the last 4 years we have been developing a master servicing plan for the Northeast Pritchard Neighborhood. As part of this overall plan, McElhanney engineers were engaged to design a pond and related servicing lines. While the servicing lines were for the greater use of the entire Pritchard Neighborhood, in accordance with the Town Staff request, the pond located at 1194 Pritchard was designed with the capacity to service 1000 Pritchard, 1194 Pritchard and 1603 Brighton.

In April, the town staff then made a request to us, to see if there were any engineering solutions to increase the capacity of the pond and decrease the costs of the overall servicing, to allow additional properties to have access to the pond. Simba and the Town staff entered into a Memorandum of Understanding to develop these options with the intent that the Town would purchase additional capacity in the pond to service additional properties along the Pritchard corridor. Town Staff and McElhanney are currently working on the alternatives and nearing the end of this process.




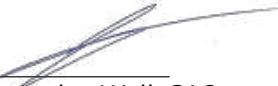
Recently, Simba's financial partner has decided not to move forward with any further development costs and has listed the properties for sale. Simba and McElhanney continue to work on the approvals for the projects in order to assist with potential buyers and would like to assist the town in the one pond solution.

A One Pond Solution will result in more developable land, higher density potential, and lower long-term maintenance and operating costs. With the Town striving for a One Pond Solution for the Northeast Pritchard Neighborhood, subsequent to the completion of the servicing plan in accordance with Town Staff request and direction, we believe at this point, it would be in the Town's best interest to bear the financial responsibility for the construction of the pond and service lines. This would shift the solution to a long term, economical and growth-oriented focus, with minimal future environmental impact. Should the Town agree, it could then recapture its infrastructure investment through collections surrounding the Area Servicing Agreement.

Alternatively with the sales of individual properties, developers could look to multiple pond servicing solutions on their individual developments. This being a potential cost heavy and environmentally impacted solution.

Based on the current design meeting the Town's requirements and without any consideration for cost savings measures that add additional capacity for future growth, the approximate cost for the pond and servicing lines, based on McElhanney and a local contractor's estimate are in the ballpark of \$4.5 - \$4.8 million. The area covered is from the pond to the north of Cambridge.

To: Mayor and Council	File: RZ/OCP 23-4 DP 23-5 DVP 23-2
From: Robin Pallett, Planner II	Date: August 7, 2024
Subject: 1966 Guthrie Road (Phase 3) First and Second Reading Comox OCP Amendment Bylaw 1685.10 Comox Zoning Amendment Bylaw 1850.43	

Prepared by:  Robin Pallett, Planner II	Municipal Engineer:  Shelley Ashfield, Director of Operations	Finance:  Edward Henly, Director of Finance	Report Approved:  Jordan Wall, CAO
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RECOMMENDATION FROM THE CHIEF ADMINISTRATIVE OFFICER:

1. THAT Comox OCP Amendment Bylaw 1685.10 be given First Reading (**Attachment 1**); and
2. That, having considered the Town of Comox May 2020 Housing Needs Report and Summary Form, the Comox Strathcona Waste Management 2012 Solid Waste Management Plan and the impact on the current Financial Plan, Comox Official Community Plan Amendment Bylaw 1685.10 be given Second Reading (**Attachment 1**);
3. THAT Comox Zoning Amendment Bylaw 1850.43 be given First and Second Reading (**Attachment 2**); and
4. THAT a Public Hearing in respect of Comox OCP Amendment Bylaw 1685.10 and Comox Zoning Amendment Bylaw 1850.43 be scheduled for September 4, 2024, 6:00 pm at d’Esterre House 1801 Beaufort Avenue, that it include online participation via Zoom, and that the Town publish the requisite notices as required by the Local Government Act.

ALTERNATIVES TO THE RECOMMENDATION

Alternative 1 – To not require the Affordable Housing Contribution (per the applicant’s request).

5. THAT Schedule 1, Outstanding Item 4 of Planning Report RZ/OCP 23-4, DP 23-5, DVP 23-2 dated July 10, 2024, which requires the applicant to provide the Affordable Housing Contribution prior to final adoption of Bylaws 1685.10 and 1850.43, be deleted.

Alternative 2 – To require additional parking spaces with electrical capacity and ducting to facilitate future Class I EV chargers.

5. THAT Schedule 1, Outstanding Item 6.d(iv) of Planning Report RZ/OCP 23-4, DP 23-5, DVP 23-2 dated July 10, 2024, which requires the applicant to register a restrictive covenant on the title of proposed Lot 2 that secures electrical capacity and ducting to at least 65 underground parking spaces to facilitate the future extension of wiring for Class I EV chargers, be amended to change the number of parking spaces with electrical capacity and ducting to _____.

1.0 PURPOSE

This report is to introduce bylaws for Council’s consideration and schedule a Public Hearing. The applicant’s submission is contained in **Attachment 3**.

A traffic impact assessment, arborist’s report, stormwater management report, environmental report, geotechnical report and hydrology report have been submitted as part of the application package and are on-record at Comox Town Hall and provided on the Town of Comox website¹ until the proposed bylaws are given 3rd reading.

¹ Located on the Town of Comox website: Development → Planning → Application Notices → OCP amendment and rezoning at 1966 Guthrie Road; or <https://www.comox.ca/node/6873>.

2.0 REPORT SUMMARY

2.1 Subject Property:

Eastern 1.03 ha of 1966 Guthrie
(as shown shaded and circled in red)

Owner: Avtar Properties

Applicant: Norman Laube

Property Contains: A commercial centre with eight Commercial Retail Units (CRUs). Phase 3 (circled in red on the inset map and as shown in **Figure 1**) is undeveloped and is the site of the proposed development.

Legal Address: The eastern 1.02 ha portion of LOT A SECTION 77 COMOX DISTRICT PLAN VIP86498



Development Proposal: As the third and final phase of the Highland Village shopping centre, the applicant proposes to construct a six-storey mixed-use building containing 205 residential dwelling units (100% rental), 1,400 m² of commercial space on the ground floor divided into nine commercial retail units (CRUs) and an underground parking structure. Three CRUs would face Guthrie Road and six would face the internal parking lot. The westernmost CRU facing Guthrie Road (CRU #9) includes drive-thru facilities and is intended for a 'restaurant – coffee shop' use.

An OCP amendment, Rezoning, Development Variance Permit, Development Permits (for both proposed lots) and a Covenant Amendment are required to facilitate the subdivision of 1966 Guthrie into two parcels and the proposed development.

- **OCP Amendment:**
 - Changing the land use designation and Development Permit Area of proposed Lot 2 (i.e. the currently-vacant eastern 1.02 hectares of 1966 Guthrie Road) from *Commercial: Neighbourhood* to *Mixed-Use Commercial-Residential*.
 - Changing the Development Permit Area of proposed Lot 2 from *DPA #5 Commercial: Neighbourhood and Tourist* to *DPA #4 Mixed-Use: Commercial Residential*.
- **Rezoning** proposed Lot 2 from C3.1 Arterial Commercial to a new Comprehensive Development zone (CD 31). Pursuant to the rezoning, the applicant also proposes:
 - A supplementary zoning text amendment to Comox Zoning Bylaw 1850 (the Zoning Bylaw), to exempt the proposed CD zone from a general prohibition of Drive-Thru services and restaurants.
- **Development Permit** for DPA #4 (Mixed-Use Commercial-Residential) to permit construction of the proposed mixed-use development.
 - Council can consider the development permit at third reading of the bylaws.
- **Development Variance Permit to vary on Lot 1:**
 - Parking development standards in Comox Zoning Bylaw 1850 to reduce the minimum setback for six existing parking spaces from the east lot line from 1.5 m to 0.0 m;

- Screening regulations in Comox Zoning Bylaw 1850 to not require screening between six parking spaces in proximity of the east lot line and an adjacent mixed-use property (Lot 2).
- These variances are based on the proposed subdivision, which is not yet approved.
- Council can consider these variances at third reading of the bylaws.
- **Development Variance Permit to vary on Lot 2:**
 - Parking regulation in Comox Zoning Bylaw 1850 to allow required residential visitor parking to be provided as off-site parking on an adjacent parcel. This variances is based on the proposed subdivision, which is not yet approved;
 - Screening regulations in Comox Zoning Bylaw 1850 to allow for additional types of shrubs in the ALR buffer and increase their required spacing, to accommodate the proposed ALR buffer.
 - Council can consider these variances at third reading of the bylaws.
- **The development application also involves several conditions of approval including:**
 - Registration of a Section 219 Restrictive Covenant, to replace an existing one that is registered on the title of the subject property, and to which the Town is a signatory, to accommodate proposed changes to the existing Agricultural Land Reserve (ALR) buffer.
 - Registration of 'nuisance' easement and restrictive covenant that grants noise, odours and other potential impacts generated from agricultural operations originating from the adjacent ALR parcels to trespass over proposed Lot 2.
 - The Applicant's request to waive the Affordable Housing Contribution required through Council Policy CCL-069 (not included in the staff recommendation).
 - Amenities proposed by the applicant, which are discussed in section 6.4 of this report, and the majority would be secured via restrictive covenants.
 - Servicing requirements, which are discussed in section 8.0 of this report.

Surrounding Land Uses:

North: Town boundary; ALR-designated lands in the CVRD; rural residential and agricultural uses.

East: Town boundary; ALR-designated lands in the CVRD; agricultural uses.

South: Multi-family (townhouse) use across Guthrie Road.

West: Commercial uses (in a shopping centre) across Anderton Road.



Figure 1: Proposed subdivision as shown on the application materials (subject property shown as hatched area)

2.2 Key Issues:

Building Height & Massing	<p>Proposed building is 6 storeys and up to 24 m high, which would be the highest building in Comox (existing or proposed).</p> <p>19 Wing (DND) and NavCanada (including Transport Canada) have confirmed their approval of the proposed building height.</p> <p>This matter is discussed in section 6.3.3 of this report.</p>
Urban-Agricultural Interface	<p>The subject property abuts agricultural lands within the ALR. An existing 15m ALR buffer area is already established around the north and east perimeter of the lot as a condition of Phase 1 development, and secured by a covenant as a condition of the Phase 1 development.</p> <p>The applicant proposes a significant update to the buffer design to incorporate active and passive uses for residents, stormwater management measures and controlled access into the buffer area. Council should review Attachment 12.</p> <p>A portion of an outdoor amenity area (containing garden plots) is proposed within the ALR buffer, but located 15 m from existing ALR lands.</p> <p>This matter is discussed in section 6.6 of this report.</p>
Rental Housing as a Proposed Amenity	<p>The proposed building is purpose-built rental secured through zoning and would not be stratified. The residential component would be market rental and would be available for rent to the general public.</p> <p>This matter is discussed in sections 6.4.1 & 7.1 of this report.</p>
Affordable Housing Contribution	<p>The applicant has requested that the Affordable Housing Contribution (\$763,327.00) be waived for the proposed development. It is not recommended by staff; however, a motion to accept the applicant's request is provided in Alternative Recommendation 1.</p> <p>This matter is discussed in section 6.5 of this report.</p>
Parking Variance for Lot 2 & Shared Parking with Lot 1	<p>The applicant proposes a parking shortfall of 16% (40 visitor spaces) from the zoning requirement, which is proposed to be offset by the provision of 40 off-site parking spaces located on and shared with the existing shopping centre on proposed Lot 1 (secured via legal agreement).</p> <p>This matter is discussed in sections 7.4 & 7.5 of this report.</p>
Guthrie Road Streetscape/ Frontage	<p>The proposed streetscape design along the Guthrie Road frontage is does not address design guidelines and OCP policies. The proposed CD31 zone prohibits stairs a grade change greater than 0.3m along the development frontage, which the applicant may choose to vary through a separate concurrent variance application.</p> <p>This matter is discussed in section 6.3.1 of this report.</p>
Concurrent Processing	<p>Concurrent processing of OCP amendment, Rezoning, Development Permit, and Development Variance Permit allows Council to consider the proposal and available information in relation to a broad context of issues.</p>
Subdivision	<p>The proposed development also requires a subdivision (as a requirement of CMHC funding for rental housing), dividing the subject property into two lots: Lot 1 would contain the existing shopping centre and Lot 2 would be the proposed development site.</p> <p>Subdivisions are processed through the Town's Approving Officer.</p>

2.3 Council Decision

Council considers whether to give first and second readings to the OCP and Zoning amendment bylaws and schedule the public hearing. Through the alternative recommendations, Council may also choose to, waive the Affordable Housing Contribution and/or require additional parking spaces to have electrical capacity and ducting to facilitate future EV charging stations.

Decision Options	Implications
<ul style="list-style-type: none"> • Recommended - Give first and second reading to the bylaws and schedule a public hearing. 	<ul style="list-style-type: none"> • The bylaws receive 1st and 2nd reading. • The application proceeds to the next step in the application process (Public Hearing).
<ul style="list-style-type: none"> • Alternative 1– Waive the Affordable Housing Contribution. 	<ul style="list-style-type: none"> • Town would not collect \$763,327 in affordable housing contributions and would likely result in similar requests from other developers.
<ul style="list-style-type: none"> • Alternative 2– Require additional parking spaces with electrical capacity and ducting to facilitate future Class I EV chargers. 	<ul style="list-style-type: none"> • The applicant would be required to commit additional parking spaces to having electrical capacity and ducting to facilitate future Class I EV charging stations.

Variances and the development permit can be considered at third reading of the bylaws.

3.0 STRATEGIC PLAN LINKAGE

Processing of the development applications relates to the following strategic priorities identified in the 2023 Strategic Plan.

Strategic Priority	Areas of Focus
Balanced Community Planning	Housing - We will create the conditions for a diversity of housing options in our unique Seaside Town.
	Community Addition - We will ensure that each new major development adds positively to the community through appropriate amenity contributions and/or other community benefits.

4.0 BACKGROUND

The subject property was incorporated into the Town in 1999, at which time it was given commercial zoning, OCP land use designation and DPA. As a condition of rezoning at that time, a restrictive covenant was registered on the title of the subject property which secured a future ALR buffer prior to construction.

Since then, development permits were issued for construction of phase 1 (the Shopper’s Drug Mart building) in 2008 and phase 2 in 2009 (everything else west of the of the Guthrie Road access), whereas phase 3 (proposed Lot 2) has remained vacant.

4.1 Pre-Application Public Notification

The applicant has conducted the Town-required pre-application community consultation, which included public notification and an open house meeting. Details from the pre-application community consultation are provided in the November 15, 2023 Planning Report on RZ OCP 23-4.

4.2 The November 15, 2023 Regular Council Meeting

The November 15, 2023 Planning Report on RZ OCP 23-4 is an introductory staff report that was presented to Council at the November 15, 2023 Regular Council Meeting (RCM). That report considered Official Community Plan (OCP) amendment consultation requirements (in accordance with section 475 of the Local Government Act) and outlined consultation that took place in accordance with Town procedures and applicable legislation and provided Council an opportunity for to consider if it is sufficient².

At that meeting, Council resolved to:

- Add the Food Policy Council to the referral list of organizations for the subject applications (motion #2023.387);
- Post a notice of the proposed Official Community Plan Amendment on the Town's bulletin board and website, until replaced by a notice of public hearing (motion #2023.388);
- Post the November 15, 2023 Planning Report on RZ OCP 23-4 on the Town website (motion #2023.388); and
- Add an option for online participation when a public hearing is scheduled for development application RZ OCP 23-4 (motion #2023.389).

4.3 Subdivision Application

The applicant has engaged with the Canada Housing and Mortgage Corporation (CMHC) to establish financing for the proposed development. As a condition of financing, CMHC requires that the development site be subdivided from the rest of the subject property³. A subdivision application has been received for the subject property, which is being processed by the Town's Approving Officer.

5.0 PROCESSING PROCEDURES

Section 477 of the *Local Government Act* requires that an OCP (or amendment) must be adopted by bylaw. Each reading of an OCP amendment bylaw must receive an affirmative vote of a majority of all council members.

Attachment 4 lists the processing steps for Council's concurrent consideration of the OCP Amendment, Rezoning and Development Permit applications. This application was submitted under the previous

² Comox Planning Procedures Bylaw 1780 section 6(5) "Based on the complexity of an Application for a Major Zoning Amendment, Council may require that a public Information Meeting in relation to a development proposal be conducted by staff." Public Information Meeting means a meeting conducted by Town staff to provide information about an Application under this bylaw and to obtain comments from the public prior to any statutory Public Hearing on the Application."

³ A subdivision is required in order for the owner to acquire funding from BC Housing, which has a maximum threshold of commercial floor area on the development site. The subdivision is necessary in order to reduce the commercial component of the development site to meet BC Housing requirements.

regime of the application approval process, which seeks Council approval for land use in tandem with approval of the proposed design via the Development Permit.

5.1 Standard Referrals

Standard processing of applications includes staff level referrals to affected local governments, provincial ministries, agencies, utility providers and affected First Nations. Following the referrals received prior to and included in the introductory staff report, staff have received six additional referral responses: the Comox Valley Food Policy Council (CVFPC), Comox Valley Regional District (CVRD)⁴, the Comox Valley Land Trust (CVLT), K'ómoks First Nation (KFN), Nav Canada and Transportation Canada have each provided comments regarding the proposal. **Attachment 5** lists the agencies Town staff sent referrals to, and responses received as of the date of this report.

KFN staff responded to the application referral with details regarding the procedure in the event that a 'chance find', or cultural heritage artifact is found during construction (**Attachment 5j**). The CVRD Electoral Areas Services Committee responded with a staff report that collated comments from the CVRD's Agricultural Advisory Planning Commission (AAPC), the Area B Advisory Planning Commission (APC), and various Regional District departments. The comments relate to drainage concerns, transportation (active, public & vehicular), housing need, the proposed agricultural interface and infrastructure capacity for services (**Attachment 5h**). The applicant has provided their comments, responding to the comments provided by referred external agencies, in **Attachment 6**.

5.2 Council Must Also Consider

5.2.1 Financial Plan and the Waste Management Plan

In accordance with section 477 of the *Local Government Act*, after the First Reading of a proposed OCP amendment bylaw, Council must consider the proposed bylaw in conjunction with the current Financial Plan and the waste management plan.

After the First Reading of a proposed OCP amendment bylaw, Council must do the following in the indicated order:

1. consider the proposed bylaw in conjunction with the current Financial Plan and the Waste Management Plan⁵ (**Attachment 8**);
2. hold a public hearing (with an option for online participation);
3. In addition to the requirements above, Council may consider the proposed bylaw in conjunction with any other land use planning and with any social, economic, environmental or other community planning and policies that Council considers relevant; and
4. Council may adopt the proposed bylaw at the same meeting at which the bylaw passed third reading.

Staff are of the opinion that the proposed OCP amendment will result in a net benefit to the Town when considering revenue minus costs and will not provide significant impact to the Comox Strathcona, 2012 Solid Waste Management Plan.

⁴ The referral response from CVRD raises questions relating to servicing, stormwater management and transportation, which are discussed in section 8.0 of this report.

⁵ The current 2012 Regional Solid Waste Management Plan is in the process of being renewed, initiated in 2022 with the goal to finalize it by 2025.

5.2.2 Housing Needs Report

When amending OCP statements and land use designations respecting residential development required to meet housing needs, the Local Government Act (s.473) obliges Council to consider its recent housing needs report and the housing information, on which the report is based. The most recent Town of Comox Housing Needs Report – Data Results was completed in May of 2020 (**Attachment 7**). The 2020 Report analyzed a variety of demographic, income, economic, housing and other data. In general, the data is in alignment with the assumptions and projections of the current OCP, for example the population growth between 2006 to 2016 was 10%, as projected.

The CVRD is currently in process of updating the 2020 report for the Town. Preliminary results indicate that between 2016 and 2021:

- The estimated “gap” between the anticipated supply (based on past construction starts trend) and projected demand could be 200 units for 2024 and 600 units by 2028.
- Population growth slowed to approximately 6% and is expected to further slow in the next two decades
- The Town is aging faster than anticipated in the OCP - the 65 to 84 years old cohort grew 19%.
- In terms of anticipated housing units need, the overall demand is projected to increase slightly more than the population projection, due to declining household sizes.
- About 77% of local households own their dwelling; 23% rent.
- In general, compact and affordable housing, rental units, housing suitable for seniors and family dwelling units are all expected to be in high demand in the future.

The CVRD referral response (**Attachment 5h**) highlighted the need for more purpose-built rental housing in the region.

5.2.3 Housing Accelerator Fund (HAF) Agreement

The proposed development aligns with the Anderton Corridor/Guthrie Road Transit Oriented Development Housing Initiative in the Town’s approved Housing Accelerator Fund (HAF) agreement. It is anticipated to have a significant impact on the Town’s ability to achieve the HAF Housing Supply Growth Target of additional 801 permitted units in addition to existing averages over the 3-year term of HAF. Of the additional 801 HAF units, the Town has agreed to strive to issue Building Permits for 304 additional medium and high-density residential housing units over and above the Town average of 235 (78 annually) multi-family units by March 31, 2027.

If building permits are issued for all 206 units prior to the HAF deadline in by March 2027, this alone can assist the Town achieve 38% of the combined average and net new medium and high-density residential housing units required to achieve full HAF funding.

5.2.4 Municipal Long Term Costs and Revenue

The majority of the infrastructure required for this project will be owned and maintained by property owner. The result is that there will be relatively little ongoing asset and maintenance costs. The following financial analysis is meant to provide high level information and is subject to high variability. Council should also keep in mind that these costs only represent the direct asset costs of the development and does not consider the impact of additional cars on the Town’s roads, pressure on

total water, sewer, and storm capacity, administrative burden, and recreation needs resulting from the proposed development.

Costs

Asset	Yearly Cost	Life Span	Yearly Depreciation
Irrigation	\$10,000	20 years	\$500
Water Meter Assembly	\$70,000	20 years	\$3,500
Street trees (via SRW)	\$14,000	50 years	\$280
Irrigation (of street trees)	\$10,000	25 years	\$400
Silva cells	\$19,500	80 years	\$244
Non-tree street plantings (via SRW)	\$8,500	25 years	\$340
Total	\$37,600	N/A	N/A

Revenue⁶

Type of Unit	Per-unit Yearly Tax Revenue to Town ⁷	Total Revenue ⁸
Dwelling (Apartment)	\$949 (2024)	\$10,877
CRU	\$4,228 (2024)	\$48,473
Total		\$59,350

As shown in the table above, this project will bring in significantly more tax revenue than it will cost in direct asset maintenance costs.

6.0 OCP IMPLICATIONS

Official Community Plan Land Use Designation: Commercial: Neighbourhood

The proposal requires an amendment to the OCP for proposed Lot 2 (Lot 1 would remain unchanged). The amendment is twofold: changing the land use designation and the Development Permit Area (DPA). The table below demonstrates proposed changes as they apply to the proposed lots on the subject property:

	Proposed Lot 1 (the existing shopping centre)		Proposed Lot 2 (the development site)	
	From	To	From	To
Amending the Land Use Designation	Commercial: Neighbourhood	No Change	Commercial: Neighbourhood	Mixed-Use Commercial-Residential
Amending the DPA map	DPA: #5 (Commercial: Neighbourhood & Tourist)	No Change	DPA: #5 (Commercial: Neighbourhood & Tourist)	DPA #4 (Mixed-Use Commercial-Residential)

⁶ Based on estimated number and types of units; subject to change.

⁷ Using \$450,000 for residential and \$475,000 for commercial spaces, as per estimates from 495 Aspen Road.

⁸ First 10 years assuming 3% increase annually.

The proposed land use designation for would change from commercial use to mixed use (commercial on the ground floor and residential above).

The proposed development includes some changes to the parking lot and open areas in Lot 1, including construction of a new garbage staging enclosure, in addition to the proposed development on Lot 2. However, as demonstrated in the table below, the proposed works on Lot 1 are exempt from the DPA #5, whereas the proposed development on Lot 2 would be subject to DPA #4 (Mixed-Use Commercial-Residential) and the design guidelines therein.

Development Permit Areas	Development Exempt from DPA?	
	Lot 1	Lot 2
DPA #5 Commercial: Neighbourhood and Tourist (existing DPA to be retained for Lot 1)	Exempt under sections 3.5.4(B), 3.5.4(C), & 3.5.4(D).	N/A
DPA #4 Mixed Use: Commercial-Residential (proposed DPA for Lot 2)	N/A	Not exempt

The proposed OCP amendments would change the allowable land use of proposed Lot 2 from commercial to mixed use (commercial on the ground floor and residential above).

6.1 OCP Regional Context Statement

Section 447 of the Local Government Act requires that an official community plan includes a regional context statement, which identifies the relationship between the official community plan and regional matters and is consistent with the rest of the official community plan.

The proposed land use designation and inclusion of the subject properties within development permit area #4 Mixed Use: Commercial-Residential is consistent with the regional context statement (**Attachment 9**).

6.2 Community Values and Planning Goals

The table below demonstrates how the proposed development aligns Community Values and Planning Goals identified in the OCP.

Community Value	Planning Goals	Proposed Development
Long term sustainability	Provide for high quality compact housing forms with good access to transit and non-vehicular transportation forms	The proposed development has a relatively high density (for Comox), is located in proximity to transit ⁹ and encourages cycling ¹⁰ .

6.3 Conformance to DPA #4 Mixed Use: Commercial-Residential

In addition to the proposed amendments to the OCP, a Development Permit for proposed DPA #4 Mixed Use: Commercial-Residential is also required for Lot 2. Subject to adoption of a proposed

⁹ The proposed development site is located within 160 m of a bus route.

¹⁰ The proposed development exceeds the required provision of enclosed bicycle parking, conveniently located for tenants.

rezoning bylaw and resolution of **Schedule 1 Outstanding Items** in relation to Development Permit guidelines, staff is of the opinion that the proposed development generally complies with the applicable DPA #4 guidelines.

However, there remain a few aspects of the proposed development that have not addressed certain design guidelines, which are discussed below.

6.3.1 The Guthrie Road Streetscape/Front Yard Depression (Not Supported By Staff)

The proposed development includes a sunken patio at the front yard of the lot between the building and the planting beds for street trees and extends around to the east side of the building (**Attachment 3**, sheets A271-A274). The depression does not address DPA #4 Mixed Use: Commercial-Residential design guidelines in the or OCP policies promoting age-friendly and pedestrian-oriented development.

The CD 31 zone (provided as Schedule "1" of **Attachment 2**) prohibits a depression in the development frontage¹¹. The applicant may apply for a development variance permit to vary the regulation in the CD 31 zone if they wish to pursue the sunken patio frontage design, in which case the variance would be presented Council as a standalone DVP application.

6.3.2 Surface Residential Parking Spaces

Two residential parking spaces are proposed to be located within the surface parking lot (of Lot 2); However, Design Guideline 3.4.5.7(4) encourages¹² residential parking to be provided either below the building or below ground. Considering that a) the proposed building would not be stratified and that parking would be managed and assigned to tenants by the owner as they see fit, and that b) Design Guideline 3.4.5.7(4) is, for all intents and purposes, optional unless directed otherwise by Council, there are no concerns about the proposed surface residential parking spaces. There are no zoning regulations that require residential parking for the proposed development to be located underground.

6.3.3 Building Height and Massing

The proposed building would be six storeys high and its primary massing is L-shaped, with the narrow end of the "L" facing Guthrie Road. To help reduce the impression of massing from the street, the building is proposed to be set back 3.48 m from the front lot line at the ground level, and the 2nd storey would be set back 7.4 m. On the 3rd to 5th storeys the balconies would be 5.5 m from the front lot line, and the 6th (top) storey would be set back by almost 7.0 m. The maximum building height is 24.0 m in the CD31 zone, and the building is proposed to be 23.1 m high (to the roof).

Development Permit Guideline 3.4.5.3(1) emphasizes that buildings should reflect the surrounding context in building orientation, scale, form, and setbacks.

Development Permit Guideline 3.4.5.3(1)

Building orientation, scale, form, height and setbacks should, where practical, reflect characteristics consistent with the surrounding context

¹¹ Section 831.13[3] of the CD31 zone provided as Schedule "1" of **Attachment 2**.

¹² As per DPA #4 Mixed Use: Commercial Residential, the term "encourage" indicates that compliance with the guideline may, at the discretion of the Council, be required as a condition of issuance of a development permit.

The proposed development lies in an established arterial commercial neighborhood, with exceptions including a townhouse complex across Guthrie Road and single-family residential lots further south along Anderton Road. At six storeys, the massing of the building is not consistent with most neighbourhoods in Comox. Although the proposed development is generally compatible with existing adjacent commercial buildings, it contrasts with adjacent ALR lands and nearby low-density residential areas. The ALR buffer functions to mitigate the impact of the interface between those uses through separation and masking (discussed further in section 7.6 of this report); however, the proposed building height highlights the importance of quality design along the street frontage. The proposed building step-backs at the front of the building help to mitigate massing impacts but should be applied in tandem with quality frontage design at the ground level (discussed further in section 6.3.1 of this report).

Other higher-density developments have been constructed in Comox in recent years that included buildings higher than 3-stories¹³, and new zoning for apartment buildings (RM6.1 apartment zone) proposed via development application RZ OCP 23-2 accommodates up to 6 stories where the development. Part of the reason for the low building heights in the Town are height restrictions imposed by the airport and the Department of National Defense (DND). However, DND has indicated that a program for endorsing developments that conflict with their maximum building height policies is currently underway.

The application was referred to CFB Comox (19 Wing), NAV CANADA and Transport Canada, who responded with temporary approval¹⁴ of the proposed development (**Attachments 5I & 5m**), as part of their examination of Comox Airport Zoning Regulations (which otherwise limits the height of structures in certain areas of the town to 9 m). Considering that the above-noted federal agencies approved the proposed height, and no higher, the proposed CD31 zone (Schedule "1" of **Attachment 2**) prohibits the parts of the building that are normally permitted to encroach above maximum allowable height (via the definition of 'height' in the Zoning Bylaw) to be higher than 83.36 Canadian Geodetic Vertical Datum (i.e. 24 m as measured from average grade)¹⁵.

6.3.4 Pedestrian-Vehicle Conflicts Along the Primary Pedestrian Route

Prior to the pre-application consultation open house (and prior to submitting an application), the applicant had originally presented staff with a different site layout that demonstrated the drive-thru lane directly abutting Guthrie Road. However, that site design was rejected because of the visibility and prominence of the drive-thru as seen from the street. As a result, the site design was updated to integrate the drive-thru more centrally in the site (and under the building) as shown in the applicant's submission (**Attachment 3**, sheet A202).

¹³ There is a 4-storey (11.8 m high) mixed use building located 235 m south of the subject property at 618 Anderton Road which was constructed in 2019. 630 m to the west of the subject property is a 4-storey (13.5 m high) residential and mixed use buildings were constructed in 2021 at 2200 Murrelet Drive and 695 Aspen Road. In downtown Comox, a 5-storey (17.8 m high) residential building at 1700 Balmoral Avenue was constructed in 2020.

¹⁴ Approval by NAV CANADA is valid for 18 months, and will require renewal by the applicant on July 17, 2025.

¹⁵ Via section 831.8[2] of the CD31 zone provided as Schedule "1" of **Attachment 2**.

DPA #4 Design Guideline 3.4.5.6(3) states that “safe, convenient pedestrian routes for all units should be provided from the unit to an abutting street”. However, the current site layout results in higher pedestrian and vehicle interactions. Staff will explore options to mitigate this issue with the applicant.

6.4 Proposed Amenities

The OCP does not call for amenities for development in the proposed *Mixed-Use Commercial-Residential* land use designation, however, the OCP generally identifies amenities that may be exchange for density or other incentives, at Council’s discretion.

The applicant offers the following amenities, for the benefit of the greater Comox community, with proposed development:

- Rental tenure for all 205 dwelling units (market rental);
- 20 adaptable dwelling units (9.7% of the 205 proposed dwelling units);
- A mix of dwelling unit sizes and types to address a range of housing needs;
- Two fully-functional level 2 EV-charging stations;
- Electrical capacity and ducting for future EV chargers in 65 residential parking spaces (30% of the proposed parking provision); and
- Soundproofing.

The applicant has not proposed constructing to an energy step code that is higher than the current minimum¹⁶ and has requested that the \$763,327.00 Affordable Housing Contribution be waived for the proposed development. Aside from the rental tenure, which is secured through the proposed CD31 zone, the proposed amenities would be secured via restrictive covenants (**Schedule 1 Outstanding Items**, items 6c[i-v]).

6.4.1 Rental Housing

The residential component of the building (205 dwelling units) is proposed to be 100% market rental, which the owner proposes to manage directly. The proposed CD31 zone (**Attachment 2**, Schedule “1”), includes *residential rental tenure* as a condition of use for all dwelling units, which would also be defined in the Zoning Bylaw (discussed further in section 8.1 of this report). The commercial units are proposed to remain unstratified (leased to commercial tenants) but their tenure would not be restricted through zoning.

The OCP states that “rental housing should be given priority wherever feasible¹⁷” and it includes a suite of policies that address the need to increase the supply of rental tenure in Comox¹⁸. The OCP also encourages developments to “include a range of appropriate and affordable housing options for older people...¹⁹”

Although none of the proposed rental dwelling units are proposed to be provided at below-market rates, the CVRD referral response (**Attachment 5h**) “indicates that there is a need for more subsidized and non-market rental housing in the Comox Valley. and that “ensuring that a certain proportion are

¹⁶ Comox Building Bylaw 1472 and the current BC Building Code require that the proposed building be constructed to Step 3 of the BC Energy Step Code.

¹⁷ Section 2.1.2.1 of the OCP.

¹⁸ Section 2.1.2.3 of the OCP.

¹⁹ Section 2.2.5.4 of the OCP.

rented at below-market rates is necessary in order to support Comox Valley residents during the housing crisis". As such, the CVRD referral response "strongly encourage[s] the Town to require the provision of affordable housing units or contributions per the Town's Affordable Housing Amenity Contribution Policy". A discussion of the Affordable Housing Amenity Contribution Policy is provided in section 6.5 of this report.

6.4.2 Adaptable Dwelling Units

The applicant proposes to provide 10% of the residential dwelling units (i.e. 20 dwelling units) as adaptable units. The intent of adaptable dwelling units is to construct dwelling units that are more easily convertible to accessible standards, without the need for what would otherwise be costly and invasive renovations. Dwelling units identified as adaptable are required to construct to the specifications identified in the Zoning Bylaw under Section 5.20 Special Needs Housing Standards – Adaptable Housing.

The BC Building Code is expected to update its requirements for adaptable dwelling units, coming into effect on March 10, 2025. The update is anticipated to increase the number of adaptable dwelling units required for the proposed development.

The OCP encourages special needs housing in exchange for additional density²⁰. The proposed adaptable dwelling units would be secured by a restrictive covenant (**Schedule 1 Outstanding Items**, Item 6c[i]).

6.4.3 Dwelling Unit Mix

The proposed rental dwelling units would be provided as a variety of unit size: 36 of the proposed dwelling units (17.6%) are studio apartments, 11 (5.4%) are two-storey loft apartments, 110 units (53.6%) are 1-bedroom apartments; 39 units (19%) are 2-bedroom apartments (15 of which include a den); and nine units (4.4%) are 3-bedroom apartments. The proposed unit mix would be secured within 10% of the proposed proportions by a restrictive covenant (**Schedule 1 Outstanding Items**, Item 6c[ii]).

The CVRD referral response (**Attachment 5h**) reflects the demand for a range of dwelling unit sizes to address the wide range of housing needs for residents of the Comox Valley.

6.4.4 Level 2 EV Charging Stations

The applicant proposes to provide two Level 2 EV charging stations in the surface parking lot. The proposed Level 2 EV charging stations would be secured by a restrictive covenant (**Schedule 1 Outstanding Items**, Item 6c[iii]).

6.4.5 Electrical Capacity and Ducting for future Residential EV Charging Stations

The applicant proposes to provide 30% of the residential parking provision (providing 65 underground parking spaces) with electrical capacity and ducting to accommodate future Level 1 EV charging stations²¹ in the

²⁰ Section 2.1.2.3(c) of the OCP.

²¹ The proposed electrical capacity and ducting do not constitute "EV Ready" parking spaces. The industry standard for EV-ready parking spaces include an electrical panel (connected to a main switchboard, which is connected to a main transformer) and energized electrical outlet (upon which a charging station can be installed

underground residential parking area. The proposed parking spaces with electrical ducting and capacity are unable to charge electrical vehicles but avoid costly electrical renovations to provide a source of power for EV charging. As drivers adopt EVs, charging stations can be installed into the energized outlets at these parking spaces to provide access to EV charging. The proposed electrical capacity and ducting would be secured by a restrictive covenant (**Schedule 1 Outstanding Items**, Item 6c[iv]).

Other recent multi-family development applications presented to Council for consideration included a larger provision of conduits and electrical capacity to accommodate future installation of electrical vehicle chargers²², however the applicant declined to provide greater than 30%, indicating that their target market for the tenants of rental dwellings do not have a high adoption rate of electric vehicles and that provision of electrical capacity and ducting for an additional 50% of residential parking spaces would result in higher rents without the benefit to their tenants.

6.4.6 Soundproofing

The applicant proposes sound attenuation for dwelling units in accordance with and exceeding OCP policies²³. Proposed sound attenuation (for windows and exterior wall assemblies of dwelling units located within 35 m of Guthrie Road) would be secured by a restrictive covenant (**1 Outstanding Items**, Item 6c[v]).

6.5 Request To Waive The Affordable Housing Contribution (Not Supported By Staff)

OCP policy 2.1.2.3(f) states, “The Town may consider an affordable housing amenity for rezoning applications for four or more principal residential buildings” and Affordable Housing Amenity Contribution Policy CCL-069 (**Attachment 10**), which directs staff to seek developer contributions for Affordable Housing as an amenity at time of zoning amendment, was adopted in 2022. The policy states that, as part of an amenity negotiation for rezoning, the Town will seek a developer contribution rate of \$50 per square metre for rental development²⁴ but also that rate may be adjusted in consideration of a) the impact of the proposed development on the community and b) the size of the proposed units.

The Town’s practice is to collect the Affordable Housing Contribution for developments that include a rental component. In the case of the proposed development, the rate of \$50 per square metre of residential rental floor area would come to a total contribution of approximately \$763,327.00. Provision of the Affordable Housing Contribution is included as item 4 in **Schedule 1 Outstanding Items**.

However, the applicant has submitted a letter requesting the Affordable Housing Contribution be waived for the proposed development (**Attachment 11**). As such, the **Alternative Recommendation** is

in the future). In order for the spaces to be considered “EV Ready”, they would need to include connected electrical panels and energized outlets, which are not proposed in this case.

²² The applicant for the proposed development at 2077 Hector Road and 941 Aspen Road (i.e. High Street) has committed to providing conduits and electrical capacity approximately 80% of parking stalls (unspecified charging level).

²³ Sections 2.1.1.3(n) & 2.1.4.3(j) of the OCP.

²⁴ Based on the interior floor area of each residential unit, including stairwells and elevator shafts but excluding vehicle parking areas, class II bicycle parking spaces and cellars.

provided for Council to optionally move to waive the provision of the Affordable Housing Contribution and remove it as an Outstanding Item.

6.6 The Urban-Agricultural Interface

The subject property abuts ALR lands in the CVRD to the north and east. The OCP objective identified in Section 2.1.9.2 (1) is to “protect the agricultural land base and marine foreshore for food production and encourage future growth that is environmentally sustainable”. In addition, OCP policy 2.1.9.3 (e) states:

“Any development occurring adjacent to land designated Agricultural on Map 1- Land Use Designations shall be required to incorporate an appropriate landscape buffer to mitigate conflict between agricultural and non-agricultural uses:

- i. The landscape buffer will either be in accordance with the recommendations of a Professional Agrologist based on an assessment of a specific development proposal or be compatible with the Landscape Buffer Specifications of the ALC;
- ii. The buffer design will address interface issues based on existing and anticipated land use with the objectives of providing visual separation and minimizing trespass and where appropriate litter, dust, pesticide and herbicide drift but accommodating wildlife movements; and
- iii. Agricultural best management practices to minimize dust, pesticide and herbicide drift will be encouraged due to the limited ability of a physical separation to alleviate these issues and the negative impact of a buffer over 10 metres in width on the viability of adjacent residential and industrial areas.”

In addition, the Zoning Bylaw includes screening (masking and separation) regulations²⁵ for lots that abut ALR lands.

Although an ALR buffer has been established²⁶ and maintained in accordance with the ALR buffer covenant along the north lot line of the subject property (on Lot 1), it has not been maintained around the proposed development site (Lot 2). **Attachment 12** includes a table that provides clarification about the various elements of the proposed ALR buffer, and whether they address zoning regulations and requirements in the existing ALR buffer covenant (discussed in section 6.6.1 of this report).

Because the ALR Buffer aims to protect agricultural lands from development impacts, in part by restricting activity within its bounds, the applicant proposes several types of fencing in and around the ALR buffer (Sheet L-01 of the Applicant’s Submission provided in **Attachment 3**). Locked gates with key access for residents of the building and maintenance personnel would secure the ALR buffer area and discourage its use by the general public.

In their referral response (**Attachment 5h**), the CVRD Agricultural Advisory Planning Commission (AAPC) commented that CVRD Farmland Protection Development Permit Area guidelines (which are not applicable to the proposed development) likewise encourage a 15 m landscaped buffer between

²⁵ Section 8.13 of the Zoning Bylaw.

²⁶ The ALR buffer was established via DP 07-7 (for the Shoppers Drug Mart building) and upheld via DP 08-10 (for the remaining existing buildings on the subject property).

development and lands in the ALR, and that the proposal is supported because of the buffer as well as the applicant's commitment to including an acknowledgement by tenants of the impacts of living next to active farm operations .

6.6.1 ALR Buffer Covenant

There is an existing Restrictive Covenant charge on the title of the subject property²⁷, that requires the owner to install and maintain a 15 m-wide landscape buffer along the north and east boundaries of the lot, where it abuts ALR lands. This covenant restricts use within the buffer area to managing and maintaining the buffer only, and includes specific landscaping requirements²⁸ (Schedule "A" of the existing ALR Buffer Covenant is provided on **Attachment 13**). The specifications establish an outer strip of trees closest to the lot line (the masking layer), and an inner layer of plantings (the innermost 7 metres of the buffer) that is mainly for separation of uses and preventing trespass.

The applicant has proposed a new design for the required ALR buffer that does not comply with the covenant's specifications. A comparison of the proposed design elements against the ALR covenant and zoning regulations is provided on **Attachment 12**.

- A 73.4 m² portion of outdoor amenity area for tenants of the building programmed for garden plots (located in the northwestern area of the buffer on Lot 2);
- gravel pathway and benches (located within the inner strip of the buffer);
- bollard lighting and underground electrical conduits (located along pathway);
- low split-rail fencing (to separate the inner strip of the buffer, which would contain the proposed passive recreational uses, from the masking layer of trees);
- stair encroachments (located at grade on the north elevation, projecting up to 1.4 m into the buffer);
- upper-storey balcony encroachments (located at the 2nd to 5th storey on the north elevation, projecting up to 0.61 m into the buffer);
- drainage infrastructure, including a stormwater swale, underground pipes carrying drainage from parking lots (to the swale) and to an underground holding tank (from the swale, for storm water overflow) and an oil/grit separator;
- A maintenance path (located in the outer layer of the buffer abutting the lot line); and
- The planting plan (which also requires a variance to the Zoning Bylaw that is discussed in section 7.3 of this report).

Of the proposed design elements noted above, the ones that would support active and passive recreational uses (e.g., walking, sitting) in the inner layer of the ALR buffer are contrary to the original intent of the buffer to restrict access. The applicant proposes to limit the impacts of activity in the buffer by restricting public access through fencing and controlled-access gates, discussed further in Section 6.6 of the report.

As discussed in section 8.5 and 9.0 of this report, the proposed landscaping in the ALR buffer is not compatible with the proposed underground stormwater infrastructure, and will need to be updated prior to adoption of bylaws. If the updated landscaping is not in conformance with zoning, then an

²⁷ Registered via Development Permit DP 07-7, which permitted construction of the Shopper's Drug Mart building in 2007.

²⁸ Specifications for buffer landscaping in the covenant are taken directly from a 1993 ALC.

additional variance would be required and likewise may not comply with the existing ALR buffer covenant (as an additional deviation from the original intent of the buffer).

The existing Restrictive Covenant is proposed to be discharged and replaced with a new one that reflects the proposed changes to the ALR buffer.

6.6.2 Nuisance Impacts Originating from Agricultural Operations

The adjacency of the proposed development to active farming operations would result in noise, odours and other nuisance impacts on the residents of the proposed development. An easement would be registered on the title of Lot 2 to allow the noise, odours and other nuisances originating from the abutting ALR lands to have 'right of passage' over proposed Lot 2. The easement, which is included in **Schedule 1 Outstanding Items** as Item 6b(vi), would be registered as a covenant so that the Town remains a signatory (and therefore cannot be discharged without Council approval).

In order to protect residents from the impacts of adjacent active farming operations, as well as those from the fronting arterial road (Guthrie Road), idling vehicles in the proposed loading bay and drive-thru lanes, and smoke from wood-burning stoves originating from dwellings on adjacent lots in the CVRD²⁹, the applicant proposes that every dwelling unit would be provided with openable windows (and a sliding glass door for units with a balcony), but would also include an interface to activate a ventilation system for times when the windows need to be closed. A restrictive covenant is included in **Schedule 1 Outstanding Items**, item 6c(vi), to secure the ventilation system and to ensure that it is provided and that each dwelling unit would include a panel to activate climate controls.

The referral response from CVRD indicates that the Agricultural Advisory Planning Commission (AAPC) supported the proposed development, in part because the applicant agreed to require tenants to acknowledge in writing the adjacent agricultural uses and their impacts. As such, a covenant securing that agreement is proposed by the applicant (**Schedule 1 Outstanding Items**, item 6c[vii]).

6.6.3 Outdoor Tenants' Amenity Area and Garden Plots in the ALR Buffer

OCP policy 2.1.9.3(f) states that "the Town will encourage urban agriculture including the development of backyard gardens as an accessory use on land zoned for residential use, community gardens and orchards as an accessory use on land zoned for multifamily housing including social housing and on municipal open space, where appropriate."

As demonstrated on figure 2 (below), about half of the proposed 137 m² enclosed outdoor amenity area at the northwest corner of the building³⁰ would be located within the ALR buffer, but more than a 15 m from existing ALR lands. The portion of the amenity area that encroaches into the buffer would be programmed for urban gardening (providing garden plots, compost bins and a garden shed, whereas the remaining portion would include a barbeque, tables and seating). The amenity area is proposed to be hard-surfaced³¹ and therefore would not be landscaped in accordance with the existing

²⁹ CVRD permits wood-burning stoves as a primary heating source for dwellings.

³⁰ There is also an outdoor tenants' amenity area located on the roof of CRU 9 at the front of the lot (accessed from 2nd storey of the building).

³¹ Sheet L-07 of the Landscape Plan (**Attachment 3**) indicates that patio surface treatments are 'to be determined'.

ALR Buffer covenant. However, although the portion of the amenity area containing the garden plots is located in the ALR buffer area as defined in the ALR buffer covenant, it is located 15 m from any current ALR lands.

However, considering that that garden plots are proposed within a portion of the ALR buffer that is more than 15 m from existing ALR lands, additional garden plots that are located within 15 m of the ALR would not be appropriate (with respect to policy 2.1.9.3 [f]). As such, the proposed CD31 zone prohibits gardens plots community gardens use in the rear or east side setbacks (section 831.2[6] of the CD31 zone, included as Schedule "1" of **Attachment 2**). Because the outdoor amenity area and garden plots are located more than 15m from a lot line abutting ALR lands, a variance to the ALR masking regulations in the Zoning Bylaw (section 8.13) is not required.

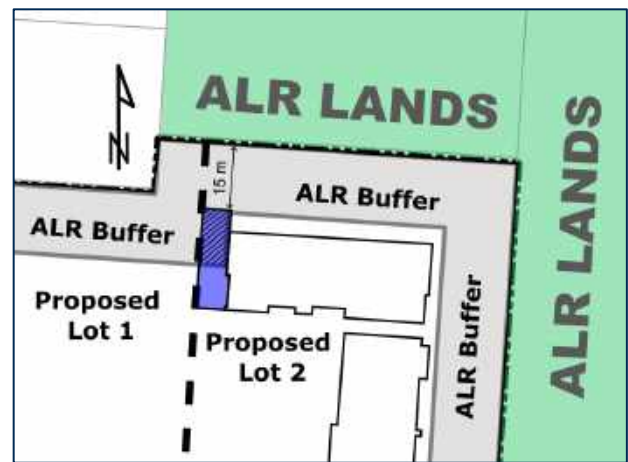


Figure 2: Proposed outdoor amenity area (shown in blue) is partially located in the ALR buffer (grey) but located more than 15m from ALR lands (green). The overlapping area is shown as blue hatched.

7.0 ZONING IMPLICATIONS

Current Zoning: C3.1 Arterial Commercial

The applicant proposes to rezone Lot 2 to a Comprehensive Development zone (CD 31) that accommodates the proposed development and the development site (proposed Lot 2). The following table demonstrates the proposed rezoning as it would apply to the proposed lots.

	Proposed Lot 1 (the existing shopping centre)		Proposed Lot 2 (the development site)	
	From	To	From	To
Rezoning	C3.1 Arterial Commercial	No Change	C3.1 Arterial Commercial	CD 31 Zone

Subject to resolution of the **Outstanding Items** contained in **Schedule A**, the proposed development is in compliance with the proposed CD zone regulations.

7.1 Residential Rental Tenure

In 2018, the province amended the Local Government Act³² to include new authority for local governments to enact zoning bylaws to require new multi-family housing to be developed as rental units and to ensure that existing rental housing is preserved as such. There are no existing rental-tenure zones in Comox; however, as noted in section 5.2.2 of this report, 23% of Comox residents are renters.

To secure the residential portion of the proposed development as rental, a condition of the 'dwelling unit' use is included in the proposed CD 31 zone requiring that residential rental tenure shall apply

³² Introducing section 481.1 of the Local Government Act.

(**Attachment 2**, Schedule "1", section 831.2[5]). Supplemental to this condition, the following definition for 'residential rental tenure' would be added to the Zoning Bylaw (**Attachment 2**, Section 2C):

Occupancy of a dwelling unit that is located in the residential portion of a mixed-use or a multi-family residential building and:

- (1) governed by a tenancy agreement that is subject to the Residential Tenancy Act (BC), as may be amended from time to time; or
- (2) A tenancy in which the landlord is the Town of Comox, BC Housing Management Commission or a non-profit society incorporated under the Societies Act (BC), as may be amended or replaced from time to time, where the society's objectives include the provision of rental housing.

7.2 Drive-Thru Use Zoning Amendment

The proposed development includes a drive-thru restaurant-café with the queuing lane looping, at-grade, underneath a portion of the building. One or more menu boards with speakers would be located in the covered area and a drive-thru service window is proposed at the rear of CRU #9.

However, section 5.11 of the Zoning Bylaw prohibits drive-thru use throughout the Town, except for properties in the C3.1 Arterial Commercial zone. The subject property is zoned C3.1, however, once the property is subdivided and Lot 2 is rezoned, Lot 2 would no longer be permitted to include a drive thru. Therefore, in addition to the rezoning, a zoning text amendment to Section 5.11 of the Zoning Bylaw would be required to allow the drive-thru. This zoning amendment is included in **Attachment 2** as section 2.D.

Drive-thru uses are typically not compatible with residential uses, nor are they encouraged along the urban-agricultural border. However, the existing C3.1 zone of the subject property permits drive thru services and the applicant has requested that that it continue to be permitted in the proposed development.

7.3 ALR Masking Variance

As the subject property is located adjacent to ALR lands, the proposed CD 31 zone includes setback and screening regulations (section 831.11[4] of the CD31 zone, included as Schedule "1" of **Attachment 2**), referring to Section 8 of the Zoning Bylaw. Section 8.13 of the Zoning Bylaw includes regulations for separating and masking properties from abutting ALR lands. However, the proposed ALR buffer is not fully in compliance with the mandatory plantings and spacing of plantings that are required by the ALR masking regulations, and a proposed maintenance along the property line is not proposed to be screened, so variances to the Zoning Bylaw would be required.

Development Variance Permit application DVP 23-2 seeks to vary sections 8.13(1)(b), (f) and (j) of the Zoning Bylaw for Lot 2:

- Section 8.13(1)(b) requires that, within 8.0 m of a lot line that abuts ALR lands, trees and shrubs should be provided as species represented in Appendix B.1³³ of the Zoning Bylaw; however, the Landscape Plan includes other native species (in addition to those included in Appendix B.1), on

³³ Appendix B.1 of the Zoning Bylaw is a brief list of native and non-native species that are considered to be appropriate for masking ALR lands.

the rationale that more native plantings would grow faster, survive better, require less irrigation and pest management and provide habitat for wildlife.

- Section 8.13(1)(f) of the Zoning Bylaw includes required spacing of 1.0 m between shrubs, whereas the applicant's Landscape Architect recommends greater separation (1.76 m for screening shrubs and 1.2 m for inhibiting shrubs³⁴) to ensure survivability and a continuous vegetative cover within 5-10 years.
- Section 8.13(1)(j) requires that pedestrian pathways must be masked in accordance with ALR masking requirements, whereas a 1.5 m maintenance path is proposed in the ALR buffer directly abutting the lot line shared with ALR lands, without screening.

Conditions for the proposed variances to ALR Masking and Separation regulations are provided in **Schedule 2 Development Variance Permit Conditions**.

7.4 Parking Variances for Lot 2

The applicant proposes 215 parking spaces for the development, including 177 underground residential spaces³⁵, plus two surface residential spaces, 11 residential visitor spaces³⁶, and 25 commercial spaces³⁷ located in the surface parking lot. Although the required residential and commercial spaces are met, the Zoning Bylaw requires 51 residential visitor spaces, leaving a shortfall of 40 spaces.

Although off-site parking is accommodated under section 6.6 of the Zoning Bylaw, it is not applicable to parking spaces required for dwelling units (including visitor parking). Therefore, a variance is required.

Development Variance Permit application DVP 23-2 seeks to vary section 6.6(1) of the Zoning Bylaw for Lot 2. Section 6.6(1) requires that, if all required parking spaces on a commercial or RM4.1-zoned lot cannot be provided on the same parcel, required parking spaces may be located on another parcel within 250 m of the parcel that the space is intended to serve; does not include parking spaces required for dwelling units. The applicant seeks to locate residential visitor parking (which is required for dwelling units) on the adjacent shopping centre parking lot on proposed Lot 1. It is noted that the proposed zone is neither solely commercial nor RM4.1, however the OCP considers mixed use development as commercial development.

Conditions for this variance are in **Schedule 2 Development Variance Permit Conditions**, section 3. To facilitate this arrangement in accordance with section 6.6(2) of the Zoning Bylaw, the applicant proposes a legal agreement to share the use of 40 surplus spaces from Lot 1, located within 39.0 m of Lot 2, for residential visitor parking (demonstrated on **Attachment 14**). The Town would be a party to this agreement to ensure it cannot be discharged without Council consent. The agreement is included in **Schedule 1 Outstanding Items**, as item 6b(iii).

³⁴ Anti-trespass plantings; thorny plants that discourage access.

³⁵ Including ten accessible spaces and three small car spaces.

³⁶ Including three parallel parking spaces and eight small car spaces.

³⁷ Including two accessible spaces and 23 small car spaces.

Lot 1 currently has 39 surplus parking spaces³⁸, but the applicant proposes to increase that amount to 41 spaces³⁹. The proposed parking provision is shown in the following table, which demonstrates (in blue) that, of the 255 parking spaces required for Lot 2, 215 are provided on-site, and (in gold) the remaining 40 are provided off-site (to be shared with Lot 1). Shared spaces on Lot 1 would have signage indicating the arrangement with Lot 2.

With 205 dwelling units proposed, at least 26 units would lack parking⁴⁰. However, since the building is not stratified, parking spaces can be flexibly distributed or rented to tenants by the owner or property manager. The development is within 160 m of a transit stop and offers a surplus of secure cycling facilities, providing alternative transportation options for tenants.

7.5 Parking and Screening Variances for Lot 1

Because the proposed subdivision line jogs to the west near the front of the subject property to accommodate services from Guthrie Road, the new lot line is proposed in close proximity to six existing parking spaces on Lot 1 that are located between the TD Bank and the Guthrie Road access driveway (as shown on **Attachment 15**).

The proposed subdivision line creates a situation where those existing parking spaces become non-compliant to the minimum required 1.5 m setback, as well as a regulation for required screening for off-street parking areas that are visible from a street.

Development Variance Permit application DVP 23-2 seeks to vary section 6.11(3)(a), to reduce the minimum required setback for the subject parking spaces from 1.5 m to 0 m, and section 8.8, to not require on-site screening of the subject parking spaces from the adjacent street (though screening of the parking spaces would be provided on the abutting Lot 2).

Conditions for this variance are in **Schedule 2 Development Variance Permit Conditions**, Section 2. A legal agreement to secure screening on Lot 2 of the subject parking spaces on Lot 1 is included in **Schedule 1 Outstanding Items**, item 6c(viii).

8.0 ROAD, SIDEWALK, STREET TREE, WATER, SANITARY SEWER, STORM SEWER IMPLICATIONS

Items required by Engineering and Public Works would be secured by a restrictive covenant (**Schedule 1 Outstanding Items**, Items 5a-n).

8.1 Roads

- Guthrie Road is classified as an arterial road. No off-site upgrades to Guthrie Road are required.
- No concerns from Ministry of Transportation and Infrastructure (**Attachment 5k**)
- The CVRD referral response encourages the development of a protected multi-use path along Guthrie Road. Currently, Guthrie Road includes a painted bicycle lane and a pedestrian sidewalk. The CVRD references the Comox Valley Active Transportation Network Plan in reference to the

³⁸ Based on the required parking provision of 121 spaces via DP 08-10 and 160 spaces constructed (39 surplus) at the time of stage 2 development.

³⁹ 41 surplus parking spaces on Lot 1 is achieved through 4 spaces removed to accommodate the proposed solid waste staging enclosure on Lot 1 and 6 proposed new spaces introduced into the existing parking area.

⁴⁰ A parking reduction of 26 residential parking spaces is granted to the proposed development via section 6.8 of the Zoning Bylaw, based on the provision of Class 2 bike parking spaces.

multi-use pathway. In reviewing the plan, it encourages the development of a bicycle accessible and walkable shoulder for the portion of Guthrie Road between Linshart Road and Nootka Street, not a protected multi-use path. The existing sidewalk and bike lane meets the requirements of the proposed improvements identified in the CVRD's Active Transportation Network Plan.

- Figure 8 of the applicant's Transportation Impact Study⁴¹ indicates that an average if 20% of traffic originating from the proposed development would access the development from Anderton Road north80% going east or west on Guthrie Road or southbound on Anderton Road.

8.2 Street Tree

- Street trees fronting Guthrie Road are to be contained within a SRW with silva cell structures. Drainage of silva cells is to connect to the development storm system. A separate Town irrigation service is to be supplied for irrigation of the trees.

8.3 Water

- An analysis of the existing Town of Comox water system by the Town's contractor is required, at the applicant's expense. Any upgrades necessitated by this review will form a condition of development and shall be constructed at the applicant's expense.
- Existing 250 mm watermain on Guthrie Road will need to be extended to the front of the property to facilitate a separate water service connection.
- Meter vaults must be accessible from the public road network and be contained within the road right-of-way or a SRW dedicated to this purpose. The meter vault shall not be installed within the driveway access to the subject property.

8.4 Sanitary Sewer

- An analysis of the existing Town of Comox sanitary system by the Town's contractor is required, at the applicant's expense. Any upgrades necessitated by this review will form a condition of development and shall be constructed at the applicant's expense.
- Onsite sanitary sewer lift station will require registration of a restrictive covenant for the long term maintenance.

8.5 Storm Drainage System

- The subject property drains into Brooklyn Creek which is protected by the Anderton Corridor Servicing Study Volume 1 Storm Management Plan. The Stormwater management plan will need to be updated to reflect the servicing report revisions.
- On-site storm water management system design should be verified and certified that it is safe and suitable for the subject property and that the development of the lands will have no negative impacts to any surrounding water wells. This certification must be performed by a Professional Geotechnical Engineer or Geoscientist.
- On site storm water management system design to be verified and certified that the development and proposed system will not have any negative impacts to the surrounding ALR lands. This is to be reviewed and accepted by a Professional Agrologist.

⁴¹ Available on-record at Comox Town Hall and provided on the Town of Comox website (Development → Planning → Application Notices → OCP amendment and rezoning at 1966 Guthrie Road; or <https://www.comox.ca/node/6873>.) until the proposed bylaws are given 3rd reading

- MOTI acceptance of the SWMP is required for the use of their roadside ditch.
- ALR restrictive covenant required to be changed to accommodate the infiltration system and on-site underground stormwater detention facility within the set back area.
- Registration of a restrictive covenant for the long-term maintenance of the storm infiltration system and on-site underground stormwater detention facility including the oil /grit separator(s) is required.
- Based on the Landscape Plan and the applicant's proposed civil drawings, there appears to be a potential conflict between trees proposed in the ALR buffer area and the underground stormwater infrastructure, which would result in tree roots causing damage to the stormwater works. Either the Landscape Plan or the Stormwater Management Plan (**Schedule 1 Outstanding Items**, items 3c and 5c, respectively) should be amended to avoid the conflict.

8.6 Landscaping and Rainwater

- The CVRD referral requests that the Town ensure that this development does not result in an increase in volume or change in timing of the flows from pre-development conditions. The CVRD also strongly encourage Comox to ensure that all rainwater and stormwater is managed onsite in ways that account for climate change and increasingly frequent and severe storm events.
- All stormwater will be managed on site as per the requirements of Anderton Corridor Servicing Study Storm water Management Plan. The system design is to be verified and certified that the development and proposed system will not have any negative impacts to the surrounding ALR lands and no negative impacts to any surrounding water wells. This will be reviewed and accepted by a Professional Agrologist and Professional Geotechnical Engineer or Geoscientist.

8.7 Public Transportation

- Town of Comox has been actively upgrading the bus shelters as opportunity arises with the consultation of BC Transit and CVRD.

9.0 Existing and Proposed Trees

The Town of Comox Tree Retention Policy CCL-067 (**Attachment 16**) seeks to create a tree canopy comprised of feature trees and mid-level trees to provide a continuous streetscape. The proposed development includes four street trees along the Guthrie Road frontage⁴² to establish a continuous streetscape canopy (as the trees mature) and facilitate passive heating and cooling for pedestrians, as stated in Council policy.

In order to ensure establishment and provide community benefits of increased canopy coverage in this area of town, a statutory right-of-way (SRW) along the Guthrie Road frontage to ensure that the street trees are under the Town's maintenance program and oversight⁴³. In addition, silva cells (located wholly on the private property) would be required (**Schedule 1 Outstanding Items**, item 3f) to accommodate adequate root growth under the public frontage of the proposed development.

The ability to suitably accommodate canopy street trees along the street frontage would be impeded by the treatment of grade in the front yard that is proposed in the applicant's submission (**Attachment 3**)

⁴² Street trees are required in accordance with DPA #4 Mixed-Use Commercial-Residential design guidelines.

⁴³ The SRW would also avoid the need for an encroachment agreement for the off-site silva cells (in the Guthrie Road right of way) and Highway Use Permits every time works involving the silva cells are required.

and discussed further in section 6.3.1 of this report; the regulation in the CD31 zone⁴⁴ prohibiting significant grade changes along the property frontage avoids the need for the silva cells to be located under the municipal sidewalk (and associated encroachment agreement for drainage into the private stormwater system) due to the proposed suboptimal conditions for street trees, and avoids potential property damage and early tree replacement due to conflicts with the retaining wall in approximately 30 years.

Landscaping for the entrance median bed on Lot 1 (across from the garbage staging area) would be secured by a restrictive covenant⁴⁵ (**Schedule 1 Outstanding Items**, item 6c[ix]). This bed features larger Douglas fir trees. It is important to ensure all trees on and off site have adequate soil volume, sufficient light and the necessary growing space to reach full maturity and provide maximum benefits to the community. As such, soil volumes for this area should be provided, to verify the viability of the proposed landscaping there (**Schedule 1 Outstanding Items**, as item 3a).

The proposed landscape design features a diverse array of native plants in the ALR landscape buffer, with a high proportion of canopy coverage. The proposed development features a drainage swale and underground stormwater infrastructure in the ALR buffer. An updated landscaping plan with suitable plants for the edge of the swale is required (included in **Schedule 1 Outstanding Items**, as item 3b). As noted in sections 6.6.1 and 8.5, the proposed landscape plan includes trees and vegetation that are not appropriate near underground stormwater infrastructure, which are required to be replaced with suitable landscaping (**Schedule 1 Outstanding Items**, as item 3c).

MK/RP/SA/EH/CP/RN

Schedules

1. Outstanding Items
2. Development Variance Permit Conditions
3. Development Permit Conditions

Attachments:

1. Comox OCP Amendment Bylaw 1850.43
2. Comox Zoning Amendment Bylaw 1685.10
3. Applicant's Submission
4. Application processing steps
5. Referral list and responses (updated)
6. Applicant's responses to comments received from external agencies
7. 2020 Housing Needs Report
8. 2012 Solid Waste Management Plan
9. Regional Context Statement
10. Affordable Housing Amenity Policy CCL 069
11. Applicant's Letter Requesting to Waive the Affordable Housing Contribution
12. Proposed ALR Buffer vs. the existing ALR Buffer Covenant & Zoning
13. Existing ALR Buffer Landscaping Specifications (Schedule "A" of ALR Buffer Covenant FB150103)
14. Proposed Off-Site Parking Plan
15. Lot 1 Parking Spaces Requiring Variance
16. Tree Retention Policy CCL-067

⁴⁴ Section 831.13[3] of the CD31 zone, provided as Schedule "1" of **Attachment 2**

⁴⁵ A covenant is necessary to secure offsite landscaping because a Development permit is not required for the proposed landscaping, parking spaces and garbage enclosure on Lot 1.

SCHEDULE 1

OUTSTANDING ITEMS

Outstanding Items to be resolved prior to adoption of proposed Bylaws 1685.10 and 1850.43 and approval of Development Permit DP 23-5

Provide a complete set of revised Development Permit drawings in accordance with items 1, 2 and 3, below, with details and statements that are internally consistent, dimensioned, labelled with applicable revision number and date, and remove any superseded information from the drawings.

1. Architectural Drawings

Provide an updated Architectural set reflecting:

- a. Grading of the front yard that is in compliance with section 831.13(3) of the proposed CD31 zone.

2. Parking Plan

Provision of an updated parking plan reflecting:

- a. a notation that all proposed curbs are barrier curbs and in conformance with Section 6 of the Zoning Bylaw.

3. Landscape drawings

Provide an updated Landscape Plan reflecting the following items:

- a. Demonstrate proposed soil depth for all planting beds where a tree is proposed, including new landscaping on Lot 1 that is reflected on the Landscape Plan (to the satisfaction of Parks staff);
- b. New swale-appropriate plantings around the swale edge in the ALR buffer (to the satisfaction of Parks staff);
- c. Replace landscaping in proximity to the underground stormwater infrastructure in the ALR buffer with suitable landscaping (to the satisfaction of Engineering staff);
- d. Coordination with civil drawings; and
- e. Coordination with updated architectural drawings as required via Outstanding Items, Item 1a.
- f. Demonstrate Silva cells on the private property

4. Affordable Housing Contribution

Provision of the affordable housing contribution, in accordance with Affordable Housing Amenity Contribution Policy CCL-069.

5. Engineering & Public Works

Provide the following items:

- a. An analysis of the existing Town of Comox water system by the Town's contractor is required, at the applicant's expense. Any upgrades necessitated by this review will form a condition of development and shall be constructed at the applicant's expense.
- b. An analysis of the existing Town of Comox sanitary system by the Town's contractor is required, at the applicant's expense. Any upgrades necessitated by this review will form a condition of development and shall be constructed at the applicant's expense.
- c. An updated stormwater management plan that reflects the servicing report revisions.

- d. Confirmation via Geotech Engineer or geoscientist that the on-site proposed on-site stormwater management system is safe and suitable for the subject property and surrounding wells.
- e. On site storm water management system design to be verified and certified that the development and proposed system will not have any negative impacts to the surrounding ALR lands. This is to be reviewed and accepted by a Professional Agrologist.
- f. MOTI acceptance of the SWMP is required for the use of their roadside ditch.
- g. Street trees, in accordance with the landscape plan
- h. silva cell structures located under the private development frontage abutting street trees
- i. Drainage of silva cells into the development (pvt) storm system.
- j. A separate Town irrigation service, for irrigation of street trees
- k. Extension of the existing 250 mm watermain on Guthrie Road to the front of the property to facilitate a separate water service connection .
- l. an onsite sanitary sewer lift station and long-term maintenance.
- m. on-site underground stormwater detention facility and long-term maintenance.
- n. an on-site infiltration system and oil/grit separator in the ALR buffer and long-term maintenance for both.

6. Legal Documents

Registration of the following legal documents on the title of proposed Lot 2 prior to adoption of proposed Bylaws 1685.10 and 1850.43.

- a. Discharge and replacement of the existing Section 219 restrictive covenant for the ALR Buffer, to accommodate the proposed items and uses in the ALR buffer and the proposed planting specifications.
- b. Registration of SRW, easement and other legal agreements, for the provision of the following:
 - i. SRW for street trees and non-tree plantings abutting the street on private property along Guthrie Road;
 - ii. SRW providing access to on-site meter vaults (in the event that they are not located in the Guthrie Road right-of-way or on-site along the front lot line). The SRW would be registered as a blanket SRW over all of Lot 2, with the opportunity to amend the agreement following construction with a more refined SRW plan).
 - iii. a shared parking and reciprocal access agreement with Lot 1, providing access to 40 parking spaces on Lot 1 lot by visitors of Lot 2 residents. This easement is required as a covenant so that the Town remains a signatory to the agreement.
 - iv. easement between Lot 1 and Lot 2 for legal access over the driveways of each others lot. This easement is required as a covenant so that the Town remains a signatory to the agreement.
 - v. shared waste collection agreement & easement covenant to accommodate the proposed off-site garbage staging area. This agreement is required as a covenant so that the Town remains a signatory to the agreement.
 - vi. easement between Lot 2 and abutting ALR properties, to allow the noise, odours and other nuisances originating from the abutting ALR lands to have 'right of passage' over proposed Lot 2. This easement is required as a covenant so that the Town remains a signatory to the agreement.
- c. Registration of Section 219, restrictive covenants for the provision of the following:
 - i. construction of minimum 20 units meeting Comox Zoning Bylaw 1850, Section 5.20 Special Needs Housing Standards – Adaptable Housing;
 - ii. Dwelling mix that does not vary more than 10% from the dwelling unit mix proposed via Planning Report RZ/OCP 23-4, DP-5, DVP 23-2 dated August 7, 2024 (studio apartments

- 17.6% of the total number of dwellings, two-storey loft apartments 5.4%, 1-bedroom apartments 53.6%, 2-bedroom apartments 19.0% & 3-bedroom apartments 4.4%);
- iii. two Class II EV charging stations, as indicated on the Site Plan;
 - iv. electrical capacity and ducting to at least 65 underground parking spaces to facilitate the future extension of wiring for Class I EV chargers;
 - v. sound attenuation for all residential units within 35 metres from Guthrie Road that meets Canada Mortgage and Housing Corporation (CMHC) Standards, in accordance with Town of Comox, Acoustical Standards for Residential Developments, a report prepared by Wakefield Acoustics, dated October 30, 2014
 - vi. ventilation system, including climate control panels in each unit;
 - vii. acknowledge the nuisance impacts from abutting working and to require that each rental or lease agreement for a residential or commercial unit in the building include a section that requires the tenant to acknowledge in writing the differences that living next to an active farm can bring (e.g. noise, odour, etc. through regular operations, such as the use of manure or burning debris).
 - viii. Screening on Lot 2 of parking spaces on Lot 1 that abut the proposed lot line; and
 - ix. Proposed landscaping on Lot 1;
 - x. All items listed under section 5 of Schedule 1 Outstanding Items of Planning Report RZ/OCP 23-4, DP 23-5, DVP 23-2 dated August 7, 2024.

SCHEDULE 2

DEVELOPMENT VARIANCE PERMIT CONDITIONS

1. This development variance permit is to facilitate construction of a six-storey mixed-use building containing 205 residential rental dwelling units, 1,400 m² of commercial space in nine CRUs on the ground floor and underground parking, in general compliance with drawings in

Attachment 1;

2. The following bylaws are hereby varied for proposed Lot 1:

(a) *Comox Zoning Bylaw 1850:*

- (i) To add as section 6.11(3)(c) the following:

Notwithstanding section 6.11(3)(a), existing parking spaces may be located closer than 1.5 m of the east lot line of the subject property.

- (ii) To add as section 8.14 the following:

Notwithstanding section 8.8, existing parking spaces that are located within 0.5 m of the east lot line of the subject property are not required to be concealed from view by screening.

3. The following bylaws are hereby varied for proposed Lot 2:

(b) *Comox Zoning Bylaw 1850:*

- (i) Section 6.6(1) to change the current wording

From:

In the Commercial, Marine, Industrial, Park, Institutional, and RM4.1 zones, if all required parking spaces cannot be provided on the same parcel, required parking spaces may be located on another parcel within 250 metres of the parcel that the space is intended to serve; does not include parking spaces required for dwelling units.

To:

In the CD31 zone, if all required parking spaces cannot be provided on the same parcel, required residential visitor parking spaces may be located on parcel that is adjacent to the parcel that the space is intended to serve.

(ii) Section 8.13(1)(b) to change the current wording:

From:

Materials: The 8.0 metre strip immediately adjacent to the ALR parcel shall comprise a double row of deciduous and coniferous trees (or just coniferous), a triple row of trespass inhibiting shrubs, and a single row of screening shrubs in conformance with Appendix B1 and existing trees are to be retained where possible;

To:

Materials: The 8.0 metre strip immediately adjacent to the ALR parcel shall comprise a double row of deciduous and coniferous trees (or just coniferous), a triple row of trespass inhibiting shrubs, and a single row of screening shrubs including types that are represented in Appendix B1 and existing trees are to be retained where possible;

(iii) Section 8.13(1)(f) to change the current wording

From:

Spacing: Trees shall be spaced a distance of 8m or less; and all shrubs shall be spaced 1.0 meters apart; with the ultimate result that there is a continuous vegetative cover within 5 to 10 years of growth;

To:

Spacing: Trees shall be spaced a distance of 8m or less; and all shrubs shall be spaced up to a maximum of 1.8 meters apart; with the ultimate result that there is a continuous vegetative cover within 5 to 10 years of growth;

(iv) Section 8.13(1)(j) to change the current wording

From:

Notwithstanding section 8.3(1) driveways and pedestrian walkways shall not be excluded from the provisions of section 8.13 (1).

To:

Notwithstanding section 8.3(1) driveways and pedestrian walkways shall not be excluded from the provisions of section 8.13 (1), but a maintenance path directly adjacent to a lot line that abutting land within the Agricultural Land Reserve, and is no wider than 1.5 m and for the exclusive use by maintenance personnel, is excluded from the provisions of section 8.13 (1).

SCHEDULE 3

DEVELOPMENT PERMIT CONDITIONS

1. At time of Building Permit Issuance provision of a landscape security, based on 125% of the on-site and off-site landscaping and irrigation cost estimates;
 - a. The landscape security shall include the screening of the proposed garbage enclosure on Lot 1.
2. All development shall be in accordance with approved updated development and landscaping drawings;
3. All landscaping, shall be serviced with automatic watering system;
4. Issuance of any sign permits in conformance with Town of Comox Sign Bylaw No. 1197;
5. All new services shall be placed underground;
6. Except for cedar, all wooden landscape components shall be treated with paint, stain, or other preservative.
7. Provision of street trees on the private development frontage;
8. Fencing must be robust and minimum commercial grade;
9. Parking must be in compliance with Comox Zoning Bylaw 1850 (as varied by DVP 23-2):
 - a. Section 6.12(7) and (8) Surfacing and Curbing;
 - b. Visitor Parking, Small Car, Accessible and Commercial parking spaces shall be clearly marked in accordance with Section 6.12(10);
10. All exterior building and site lighting fixtures (including those in parking areas but excluding dwelling unit entrance, balcony and patio lighting) shall:
 - a. produce illumination levels in accordance with common engineering practices and standards;
 - b. minimize illumination of adjacent properties;
 - c. consist of full cut off/flat lens pole lighting or fully shielded wall lighting;
 - d. be arranged so rays of light are directed upon the parking, walking, or loading areas and not upon adjacent land or streets; and
 - e. excluding light fixtures that are incorporated into canopies, lighting of off-street parking and loading areas shall:
 - i. be so arranged that no part of any fixture is more than 4.5 m above finished ground level, and
 - ii. not overhang maneuvering aisles or loading spaces.

ATTACHMENT 1

COMOX OCP AMENDMENT BYLAW 1850.43

TOWN OF COMOX

BYLAW NO. 1685.10

A BYLAW TO AMEND COMOX OFFICIAL COMMUNITY PLAN BYLAW 1685

WHEREAS the Council of the Town of Comox has the authority under the provisions of the Local Government Act to amend the Official Community Plan Bylaw;

AND WHEREAS Council has considered the amendments in conjunction with its financial plan, any waste management plan that is applicable in the municipality or regional district and Town of Comox May 2020 Housing Needs Report Data Results including Summary Form;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. TITLE

This bylaw may be cited for all purposes as the "Comox Official Community Plan Amendment Bylaw 1685.10".

2. AMENDMENTS

Comox Official Community Plan Bylaw 1685 is hereby amended as follows:

- (1) Schedule "A" Part 5: Maps is hereby amended by:
 - (a) designating the portion of LOT A, SECTION 77, COMOX DISTRICT PLAN, VIP86498, shown shaded in Schedule "1", which is attached to and forms part of this bylaw, as within the following:
 - i. Map 1 – Land Use Designations, as Mixed Use: Commercial – Residential;
and
 - ii. DPA #4 – Mixed Use: Commercial – Residential.
 - (b) Removing the portion of LOT A, SECTION 77, COMOX DISTRICT PLAN, VIP86498, shown shaded in Schedule "1", which is attached to and forms part of this bylaw, from the shaded area on map DPA #4 - Commercial: Neighbourhood & Tourist.
- (2) Comox Official Community Plan Bylaw 1685 is further amended by making such consequential changes as are required to reflect the foregoing amendments,

including, without limitation, changes in the numbering and order of the sections of the bylaw.

3. DEFINITIONS

In this Bylaw, unless the context otherwise requires:

- (1) "Council" means the Council of the Town of Comox;

4. ADOPTION

(1) READ A FIRST time this day of , 2024

(2) READ A SECOND time this day of , 2024

(3) ADVERTISED A FIRST time this day of , 2024

(4) ADVERTISED A SECOND time this day of , 2024

(5) PUBLIC HEARING HELD this day of , 2024

(6) READ A THIRD time this day of , 2024

(7) ADOPTED this day of , 2024

MAYOR

CORPORATE OFFICER

BYLAW 1685.10

SCHEDULE "1"

SUBJECT PROPERTY



Subject Property:
1966 Guthrie Road
LOT A SECTION 77 COMOX DISTRICT PLAN VIP86498

ATTACHMENT 2

COMOX ZONING AMENDMENT BYLAW 1685.10

TOWN OF COMOX

BYLAW NO. 1850.43

A BYLAW TO AMEND COMOX ZONING BYLAW 1850

WHEREAS the Council of the Town of Comox has the authority under the provisions of the Local Government Act to amend the Zoning Bylaw;

NOW THEREFORE the Council of the Town of Comox, in open meeting assembled, enacts as follows:

1. TITLE

This bylaw may be cited for all purposes as the "Comox Zoning Amendment Bylaw, 1850.43".

2. AMENDMENTS

Comox Zoning Bylaw 1850 is hereby amended as follows:

- A. Table of Contents is amended by adding the following text under '830. CD30 COMPREHENSIVE DEVELOPMENT 30: ASPEN TOWNHOUSES':

831. CD31 COMPREHENSIVE DEVELOPMENT 30: HIGHLAND VILLAGE RENTAL APARTMENT MIXED USE

- B. Section 2.7(2) Penalties is amended by adding the following:

Column 1	Column 2	Column 3
Offence	Bylaw Section	Fine Amount
Unlawful projection into setback – CD 31 zone	5.12(12)	\$250.00
Unlawful use – CD 31 zone	117.1	\$250.00
Failure to comply with conditions – CD 31 zone	117.2	\$250.00
Unlawful accessory buildings – CD 31 zone	117.10	\$250.00

- C. Section 3.2 Definitions is amended by adding the following new definition:

Residential Rental Tenure

Occupancy of a dwelling unit that is located in the residential portion of a mixed-use or a multi-family residential building and:

- (1) governed by a tenancy agreement that is subject to the *Residential Tenancy Act* (BC), as may be amended from time to time; or
- (2) A tenancy in which the landlord is the Town of Comox, BC Housing Management Commission or a non-profit society incorporated under the *Societies Act* (BC), as may be amended or replaced from time to time, where the society's objectives include the provision of rental housing

- D. Section 4.1 Classification of Zones is amended by adding the following text under the Comprehensive Development Zones heading:

831. CD 31 – Highland Village Rental Mixed Use

- E. Section 5.11 Drive-Thru Services and Drive-Thru Restaurants is amended by replacing it with the following text:

Drive-Thru Services and Drive Thru Restaurants are prohibited in all zones except the C3.1 and CD 31 zones.

- F. Section 5.12 Projections into Required Setbacks is amended by adding a new subsection (12) with the following text:

In the CD 31 zone:

- (a) awnings, balconies, bay windows, canopies, chimneys, cornices, eaves, gutters, landings, leaders, ornamental features, pilasters, porches, sills, stairs, or sunshades may project up to 1.0 metres into a required front, rear and exterior – side setback.
- (b) In an interior side setback, only eaves may project up to 0.6 m.
- (c) In a rear setback:
 - a. Balconies may project up to 0.61 m into the rear setback; and

- b. Stairs between ground-storey residential patios and the rear yard that are no higher than 0.6 m may project up to 1.4 m into the rear setback.

- G. Schedule "A" is amended by adding as Section 831 the CD 31 zone, as shown in Schedule "1", which is attached to and forms part of this Bylaw;

- H. Adding Schedule "2" as "Appendix B2", which is attached to and forms part of this Bylaw.

- I. Schedule "B" (the Zoning Map) is amended by rezoning the portion of LOT A, SECTION 77, COMOX DISTRICT PLAN, VIP86498 shown shaded on Schedule "3" which is attached to and forms part of this Bylaw, from C3.1 Arterial Commercial to CD 31 Comprehensive Development 31.

- J. Comox Zoning Bylaw 1850 is further amended by making such consequential changes as are required to reflect the foregoing amendments, including without limitation changes in the numbering and order of the sections of the bylaw.

BYLAW 1850.43

SCHEDULE "1"

831. CD31 COMPREHENSIVE DEVELOPMENT 31: HIGHLAND VILLAGE RENTAL APARTMENT MIXED USE

For the purpose of Section 831, Areas A, B, and C are as shown in Appendix "B2"

831.1 Permitted Uses:

In the CD31 zone, the following uses are permitted and all other uses are prohibited:

- (1) Accessory structures and uses, excluding buildings and outside storage;
- (2) Apartment dwellings
- (3) Artist studios
- (4) Childcare facilities
- (5) Dental clinics
- (6) Denturist labs
- (7) Dwelling units
- (8) Financial institutions
- (9) Grocery Store
- (10) Home occupations
- (11) Libraries
- (12) Locksmiths
- (13) Medical clinics
- (14) Museums
- (15) Neighbourhood Pub
- (16) Offices
- (17) Personal service establishments
- (18) Pet grooming
- (19) Recreational cannabis retail stores
- (20) Restaurants
- (21) Restaurant - Coffee Shops
- (22) Restaurant - Lounges
- (23) Retail stores
- (24) Small appliance repair shops, processing and packaging of food or beverage products, or establishments that repair or assemble electronic equipment which
 - (i) have a total non-retail floor area not in excess of 150 square metres,
 - (ii) retail directly from the premises, and
 - (iii) have the retail area extending the full width of the premises and located adjacent to the primary pedestrian entrance

831.2 Conditions of Use:

- (1) Apartment dwelling units shall not be located below the ground floor;
- (2) With the exception of the north- and east-facing loft dwelling units in Area C, apartment dwelling units shall not be located on the ground floor;
- (3) Permitted uses other than apartment dwellings shall not be located above the ground floor;
- (4) All permitted uses shall be located within a portion of a building, completely enclosed by exterior walls, except for restaurants, child care facilities, produce stalls, landscape material, accessory structures and accessory uses; and
- (5) Residential rental tenure shall apply to all dwelling units on the lot.
- (6) Community Gardens shall not be permitted within the rear or side interior (east) setbacks.

831.3 Density:

Density shall not exceed 205 dwelling units.

831.4 Parcel Area:

Parcel area shall not be less than 1.0 ha.

831.5 Parcel Frontage:

Parcel frontage shall not be less than 70 metres.

831.6 Parcel Depth:

Parcel depth shall not be less than 145 metres.

831.7 Parcel Coverage:

Parcel coverage shall not exceed 36%.

831.8 Height and Storeys:

- (1) Height shall not exceed 24.0 metres.
- (2) Notwithstanding the definition of 'height' in section 3, no portion of the building, including chimneys, elevators, roof access stairways, water tanks and associated screening, mechanical equipment and associated screening, church spires, belfries, monuments, flag poles, ham radio towers, telecommunication modules, television or radio reception aerials or hose towers shall be located above 83.36 Canadian Geodetic Vertical Datum.

831.9 Required Setbacks:

- (1) Front
Front setback shall not be less than:
 - (i) 3.4 metres for a building or above-ground structure;
 - (ii) 1.2 m for an underground parking structure
- (2) Rear
Rear setback shall not be less than 15.0 metres.
- (3) Side – interior (east)
East interior side setback shall not be less than 15.0 metres.
- (4) Side – interior (west)
West interior side setback shall not be less than:
 - (i) 0 m for structures typically associated with a residents' outdoor amenity area;
 - (ii) 2.75 metres to a retaining wall for underground vehicle access; and
 - (iii) 6.0 m to a building.
- (5) Other:
Notwithstanding section 831.9 (2) and (4), building setbacks from lot lines shared with the Agricultural Land Reserve shall be not less than 15.0 metres.

831.11 Screening

The following shall be screened in accordance with Section 8:

- (1) Garbage compounds;
- (2) Off-street parking and loading areas;
- (3) Above ground utility boxes and utility transformers; and
- (4) Abutting Agricultural Land Reserve (ALR) lands,.

831.12 Off-Street Parking and Loading

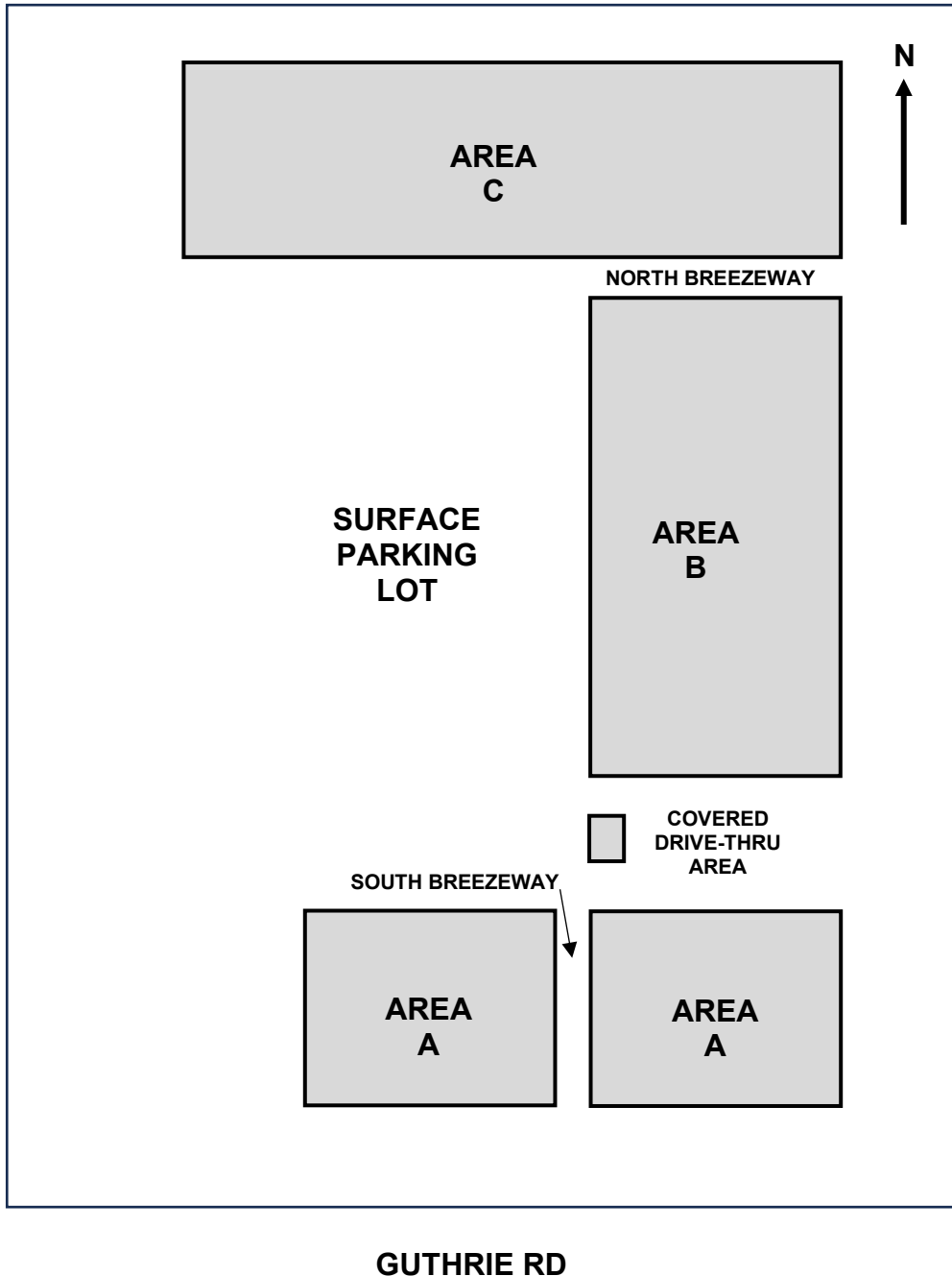
- (1) Off-street parking and loading shall be provided in accordance with Section 6;

831.13 Other Requirements:

- (1) Overhead wiring shall not be permitted on a parcel. All new services on a parcel shall be placed underground.
- (2) Unoccupied open spaces shall be fully and suitably landscaped with landscape material.
- (3) No portion of the front yard (between the building in Area A and the fronting street) shall:
 - (i) be more that 0.3 m below the existing grade of the abutting municipal sidewalk.
 - (ii) contain steps up or down.

BYLAW 1850.43
SCHEDULE "2"

Appendix "B2"
Building Areas at Ground Level



BYLAW 1685.10

SCHEDULE "3"

SUBJECT PROPERTY



Subject Property:

1966 Guthrie Road

LOT A SECTION 77 COMOX DISTRICT PLAN VIP86498

ATTACHMENT 3
APPLICANT'S SUBMISSION



Aug 31st, 2023

DESIGN RATIONALE

1966 Guthrie Road Mixed Use, Comox, BC

Project: #21005

The Design team of this project respectfully acknowledges that the land on which we propose to build these homes is on the Unceded traditional territory of the K'òmoks First Nation, the traditional keepers of this land.

WA Architects Ltd.

| PRINCIPALS

Barry Weih
ARCHITECT AIBC, AAA, SAA, LEED® AP

Neil Banich
DESIGN DIRECTOR

Joel Smith
ARCHITECT AIBC, AAA, SAA

David Echaiz-McGrath
ARCHITECT AIBC, AAA, SAA

| STUDIOS

VAN

950 — 1500 W Georgia Street
Vancouver, BC V6G 2Z6
604.685.3529

VIC

104 — 3212 Jacklin Road
Victoria, BC V9B 0J5
250.391.4933

wa-arch.ca

We are building communities.

Site Context

This proposed mixed-use residential development is located at the intersection of Anderton Road and Guthrie Road in the Township of Comox. Located between agricultural lands (ALR) along the North and East property lines and Highland Village shopping centre immediately to the West, it is a short distance from many of the Township's shops, recreational facilities, businesses, schools and parks. Multi-family and single family lots populate the areas South and South West of this property. It has a great walkability score and is immediately adjacent to dedicated bike lanes and public transit. This makes it an ideal location for residential development.

Land Use

The current use of the lot is commercial. There are 4 existing CRUs on Anderton roadside (Shoppers, Subway, Bank, Medical / Care buildings), and a vacant area on the other side. Our development takes place in the vacant area and offers a mixed use residential commercial program. A subdivision is proposed to make a clear separation between the existing and the proposed programs.

Adequate infrastructure

With the help of a team of qualified professional, we established that the existing utilities, routes, and walkways were suitable for our proposal.

Project Statistics and Programming

Highland Village Residence will consist of one six (6) storey mix-use building containing 205 purpose-built rental units over 15,000 sf of ground floor commercial space. This development proposes approximately 140,000 sf of residential area, comprised of 18% Studio, 54% 1-bedroom, 12% 2-bedroom, 7% 2-bedroom plus den, 4% 3-bedroom, and 5% Live/Work Lofts. Parking for both commercial and residential components will be provided through a combination of surface and below-grade structured parking. A 15m (49 ft) landscaped buffer separates the proposed building from the agricultural lands along the North and East facades.

The organization of commercial and residential uses was carefully considered. Commercial retail use was located along Guthrie Road for street-front activation while the upper residential floors are set back to visibly reduce massing and keep the frontage at a pedestrian scale. The ground floor residential components would be located along the quieter North side of the development away from traffic.

We chose to support an active lifestyle by encouraging bike-use and showcasing two large secure bicycle centres prominently adjacent to the main residential lobby. No more small bicycle storage in the furthest corner of an underground parkade.



Commercial and residential uses are further separated by a wide breezeway connecting the residential lobby with the lush, landscaped buffer along the East property line. Entrance to the underground parkade is located at the North West corner of the building adjacent to one of the building's indoor and outdoor amenity spaces.

To encourage a café or coffee shop, a prominent corner retail space along Guthrie was equipped with drive-through service and an outdoor patio space. The drive-through was strategically located to minimize visual impact to the neighbourhood and future residence of this development.

A generous 15m landscape strip along the North and East sides of the property provides a significant buffer to shield agricultural lands from this proposed development. This landscaped buffer is divided into two zones; a 6m (20ft) restricted area and a 9m (29 ft) zone designed with an undulating gravel path connecting Guthrie to the North facing live/work loft units. We have provided 2,450 sq. ft of outdoor amenity space that will accommodate BBQ areas and a community garden. Additional indoor and outdoor amenities are provided on the second floor above the commercial units along Guthrie Road.

Amendment and Variances requested.

Parking Amendment (Toc Zoning bylaw 1850 – 6.15)

The proposal meets the residential 177 permanent residential parking stalls in a secured underground parking, using the bylaw reduction of 28 stalls with providing all the minimum bike stalls required. To meet the residential parking visitor need, the proposal offers 11 surface parking stalls, and is asking for an amendment for the 40 remaining stalls to be able to share with existing retail complex in parking excess, through a parking covenant. The proposal meets the commercial/retail parking requirements with 27 surface parking stalls and one loading stall.

Building height Variance (Toc Zoning bylaw 1850 – 305.8)

To accommodate a six-storey mixed use development, our proposed max building height is 24m and would require a variance. The proposed building height is limited to 10m in the current zoning.

Servicing Variance (ToC Subdivision & Development servicing Bylaw 1261)

We worked closely with a qualified team of civil engineers to determine that the existing utilities were suitable for both existing and proposed programs. Our proposal is requesting a variance to allow the existing servicing to be shared between the two sites after subdivision.

Form and Character

To be consistent with the Land Use change, our proposal requires a change of Development permit zone from DPA #5 (commercial: neighbourhood) to DPA #4 (Mixed use: commercial / residential). Our design complies to the DPA #4 requirement.

We chose a West-coast Contemporary aesthetic with large varying sizes of balconies, generous roof overhangs and a modern palette of colours and material. From the early stages of design, we recognize the need to reduce the visual massing of this proposed structure. We started with a series of solid ground floor elements (commercial arches) that would march along the length of the building creating solid and void spaces. This served to emphasize the commercial programming, create a covered shopping experience, and separate the upper residential use from the predominantly commercial pedestal.



By stepping the second level back from the brick base, it allowed us visual separation of the upper floors and provided a continuous privacy screen for the residential units above. This technique was further utilized at either end of the building to present a pedestrian scale façade and reduce the overall size of the building along Guthrie Road and at the indoor amenity space along the North end of the building.

By introducing white residential blocks of varying width and height, we were able to breakdown the length of the building and create relief along its façade. Inset stacked balconies juxtaposed against cantilevered random ones served to further create the illusion of separate buildings and a unique skyline running across the length of this structure. The white element only breaks through the brick commercial base to the ground where we identify the residential character of lobby. The top floor is also set back to further reduce the impact of the six floors.

We identified any area we could to add planters and landscape nodes so as to provide visual interest and break up the continuity of the surface parking areas. A round-about driveway with central planter and tree identifies and separates the residential portion of this development from the commercial.

The design team held our first Public Information Meeting back in January 25th, 2021 and a second one in March 29th, 2023. We heard your comments and have responded as follows:

Views, Noise and Privacy Concerns – being surrounded by ALR and landscape buffer along two sides and a commercial development and Guthrie Road on the other two has allowed for significant separation between this proposed development and all residential developments around. Additionally, we have orientated the building massing so that the smallest cross section of the building faces directly onto Guthrie Road.

Parking and Traffic Concerns – We are working with WATT Consulting Group to analyze parking and traffic requirements/patterns and provide alternate means (bicycle use, public transit) to reduce the impact of the added density.

Protection of Environment/Farm/Water – We are working closely with Grade Consulting Inc. (Civil Engineers) and Mystic Woods Landscape Design to have a comprehensive storm management strategy to retain and control rainwater runoff and ensure the development service demands on the township's infrastructure meets all required levels and rates.

Community Benefits amenities

20 units (10% of the total proposed) have been designed to meet the city adaptable housing standards. An Affordable housing letter is submitted with the application to outline the measures provided. This project is also being design along Step 3 Sustainability requirements to ensure we meet responsible environmental mandates.

Regional growth context strategy

This community is growing. Studies and reports show the need to construct many new homes in the coming years to meet the demand of a vibrant economically strong centre, including the workers and families of CFB 19 Wing Comox who are challenged to find appropriate housing close to the Base. We welcome your feedback and look forward to working with you and the Township to create homes and places of business.

WA Architects Ltd.

21005 : HIGHLAND VILLAGE

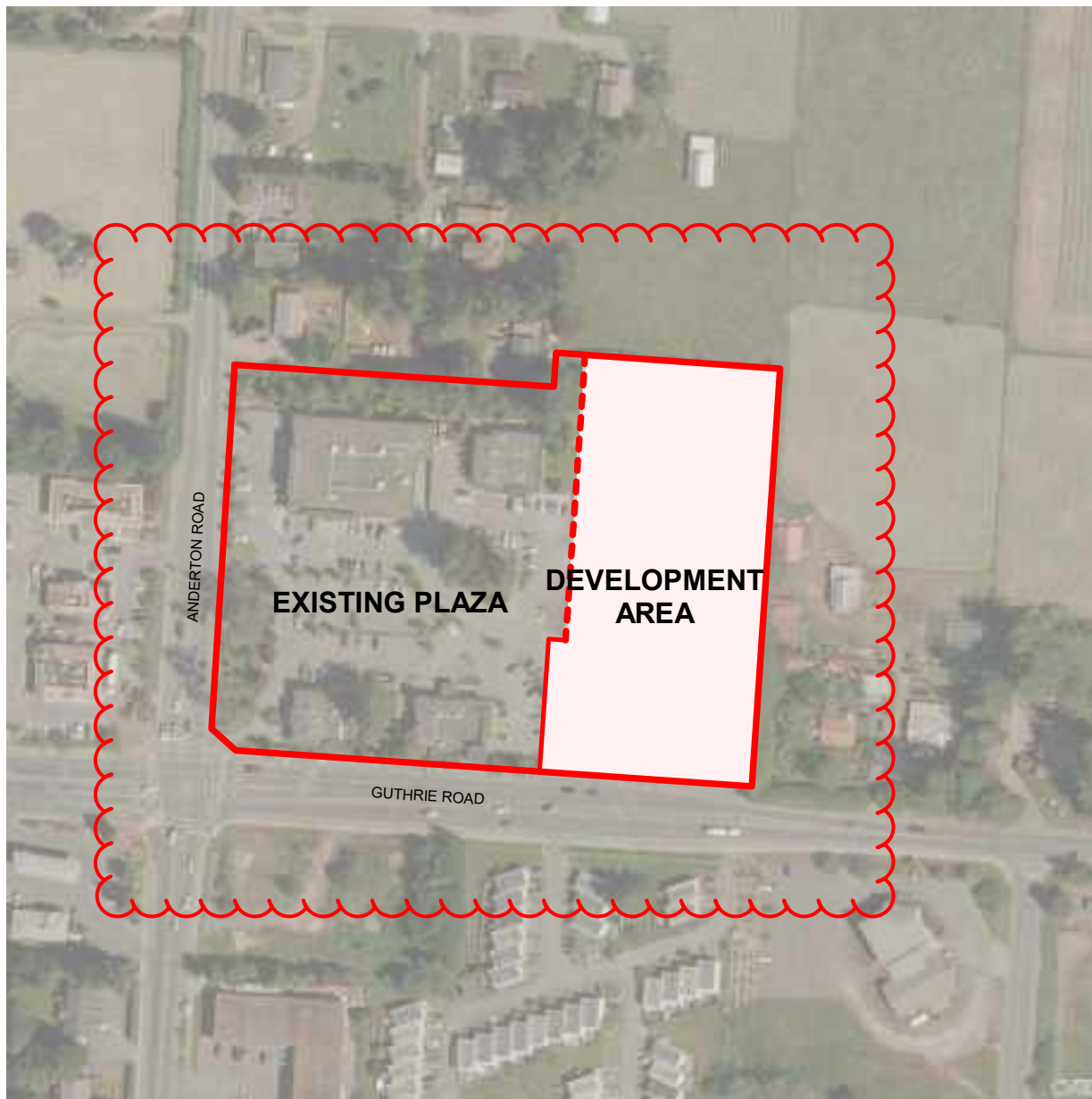
ISSUED FOR SUBDIVISION, OCP AMENDMENT, REZONING, VARIANCES AND DP RESUBMISSION - JULY 23rd, 2024

ADDRESS: 1966 GUTHRIE ROAD, COMOX, BC V9M 3X7

LEGAL DESCRIPTION: Lot A, SECTION 77, COMOX DISTRICT PLAN VIP 86498



SOUTH-WEST PERSPECTIVE VIEW



CONTEXT PLAN

NTS

SHEET LIST

REF NUMBER	SHEET NAME	REVISION
ARCHITECTURAL SHEET LIST		
A00x	CONTEXT	
A001	COVER	5
A002	SITE DETAILS	5
A003	EXISTING SURVEY	5
A101	SITE PLAN	5
A201	PARKADE P1 FLOOR PLAN	5
A202	LEVEL 1 FLOOR PLAN	5
A203	LEVEL 2 FLOOR PLAN	5
A204	LEVEL 3-6 TYPICAL FLOOR PLAN	5
A205	LEVEL 6 FLOOR PLAN	5
A206	LEVEL ROOF PLAN	5
A271	FRONTAGE PATIO	5
A272	FRONTAGE PATIO SECTION & 3D VIEW	5
A273	FRONTAGE PATIO RENDERS	5
A274	FRONTAGE PATIO RENDERS	5
A290	LIGHTING PLANS	5
A291	LIGHTING PLANS	5
A292	LIGHTING ELEVATIONS	5
A293	LIGHTING ELEVATIONS	5
A301	ELEVATIONS	5
A302	ELEVATIONS	5
A303	ELEVATIONS	5
A304	ELEVATIONS	5
A305	COMPARATIVE ELEVATIONS	5
A306	MATERIAL BOARD	5
A401	BUILDING SECTION	5
A402	BUILDING SECTION	5
A403	BUILDING SECTION	5
A501	RENDERS	5
A502	RENDERS	5
A503	RENDERS	5
A504	RENDERS	5
A505	RENDERS	5

REF NUMBER	SHEET NAME	REVISION
LANDSCAPE SHEET LIST		
L1	MASTER PLAN	
L2	PLANTING PLAN	
L3	DETAILS PLAN	
L4	ROOFTOP AMENITY PLAN	
L5	TREE RETENTION AND REMOVAL PLAN	

REF NUMBER	SHEET NAME	REVISION
CIVIL SHEET LIST		
C100	SITE SERVICING PLAN	
C200	SITE GRADING PLAN	
C300	PRE-DEVELOPMENT PLAN	
C301	POST-DEVELOPMENT PLAN	
C400	STANDARD DETAILS	
C401	STANDARD DETAILS	

DIRECTORY

ARCHITECT

WA ARCHITECTS
950 - 1500 WEST GEORGIA STREET
VANCOUVER, BC
CONTACT: DAVID ECHAIZ-MCGRATH
TEL: 604-685-3529

CLIENT

AVTAR PROPERTIES MANAGEMENT CORP,
3409 - 13496 CENTRAL AVE, SURREY,
BC, V3T 0K2
CONTACT: NORMAN LAUBE + INDER SAINI
TEL: 778-953-1653

SURVEY

BENNETT - LAND SURVEYING (COASTAL) LTD.
152 CLIFF STREET,
NANAIMO, BC V9R 5E7
CONTACT: DOUG HOLME
TEL: 250-754-5518

LANDSCAPE

MYSTIC WOODS LANDSCAPE DESIGN
PO BOX 19
GALIANO ISLAND B.C. VON 1PO
CONTACT: CORINNE MATHESON
TEL: 250-286-1327

CIVIL

GRADE CONSULTING INC.
201A, 10639 - 124 STREET,
EDMONTON, AB T5N 1S5
CONTACT: MIKE SHANKARUK
ABDULLAH RABI
TEL: 780-920-0685

ARBORIST

MUMBY'S AGRICULTURE
CONTACT: VERNA MUMBY
TEL: 250-218-6951

TRAFFIC

WATT CONSULTING GROUP
302 - 740 HILLSIDE AVENUE,
VICTORIA, BC V8T 1Z4
CONTACT: NADINE KING
TEL: 250-410-1058

GEOTECHNICAL

GEOPACIFIC CONSULTANTS
2ND FLOOR - 3351 DOUGLAS STREET,
VICTORIA, B.C. V8Z 3L4
CONTACT: NATHAN ANDERSON
TEL: 604-439-0922

SUMMARY OF CHANGES REQUESTED WITH DEVELOPMENT			
LEGAL DESCRIPTION	LOT A SECTION 77 COMOX DISTRICT PLAN VIP86498		
OCP AMENDMENT:			
ITEM	EXISTING	PROPOSED	
LAND USE	COMMERCIAL-NEIGHBOURHOOD	MIXED USE-COMMERCIAL RESIDENTIAL	
DPA ZONE	DPA #5 COMMERCIAL-NEIGHBOURHOOD/TOURIST	DPA #4 MIXED USE-COMMERCIAL/RESIDENTIAL	
REZONING:			
ZONING	C3.1 ARTERIAL COMMERCIAL ZONE	CD-COMPREHENSIVE DEVELOPMENT	
OFF STREET RESIDENTIAL VISITOR AMENDMENT	REQUIRED 51 (0.25 PER DWELLING UNIT)	PROPOSED 8 WITH 40 SHARED WITH EXISTING RETAIL COMPLEX THROUGH A SHARED PARKING COVENANT	T-OC ZONING BYLAW 1850 - ITEM 6.15
DVP VARIANCE:			
HEIGHT AND STOREY	REQUIRED 10m MAX	PROPOSED 24m MAX	T-OC ZONING BYLAW 1850 - ITEM 305.6
UTILITIES SERVICING MULTIPLE...	REQUIRED 1 SERVICING PER LOT	PROPOSED - COMBINED SERVICING	T-OC SUBDIVISION & DVPT SERVICING BYLAW - ITEM 1261

PROJECT STATS		
CIVIC ADDRESS	LEGAL DESCRIPTION	ZONING
1966 Guthrie Road, Comox, BC V9M 3X7	Lot A, Section 77, Comox District Plan VIP 86498	CD-COMPREHENSIVE DEVELOPMENT

SITE AREA			
SQUARE FEET	SQUARE METRES	PARCEL FRONTAGE	PARCEL DEPTH
112533.67 ft²	10454.72 m²	74.73 m	147.94 m

BUILDING DATA - FOR BYLAW AMENDMENT						
BUILDING HEIGHT - MAXIMUM	MAX. DENSITY	FRONT SETBACK - REQUIRED	SIDE SETBACK - REQUIRED	REAR SETBACK - REQUIRED	ALR - REQUIRED	BYLAW REFERENCE
24.00 m	1.7	17,000.00 m2/ha	2.50 m	6.00 m	7.50 m	15 m SECTION 8.13 To be Rezoned

BUILDING DATA						
BUILDING HEIGHT - PROPOSED	DENSITY - PROPOSED	FRONT SETBACK - PROPOSED	SIDE SETBACK - PROPOSED	REAR SETBACK - PROPOSED	ALR - PROPOSED	BYLAW REFERENCE
23.09 m	1.59	16,175.48 m2/ha	3.48 m	17.42 m	18.04 m	15 m To be Rezoned

TOTAL FLOOR AREA		GROSS FLOOR AREA - EXCLUSION		GROSS FLOOR AREA	
Floor Area	Total Floor Area (m2)	Exclusion Areas	Total Floor Area (m2)	Total Floor Area (m2)	FSR
PARKADE P1	6930.14 ft²	PARKADE P1	1197.23 ft²	111.23 m²	
	6496.72 m²	GARAGE	1415.71 ft²	131.52 m²	
		MECH ELEC	6659.21 ft²	6184.48 m²	
		P1 PARKING	747.99 ft²	69.49 m²	
		STORAGE	6930.14 ft²	6496.72 m²	
LEVEL 2	35160.82 ft²	LEVEL 1-1 COMM.	2146.05 ft²	199.95 m²	
	3266.55 m²	BIKE CENTRE	1498.44 ft²	138.44 m²	
LEVEL 3	31459.43 ft²	OUTDOOR AMENITY	360.44 ft²	33.49 m²	
	2922.68 m²	PRIVATE OUTDOOR AREA	4002.93 ft²	371.88 m²	
LEVEL 4	31459.43 ft²	LEVEL 2	3669.53 ft²	340.91 m²	
	2922.68 m²	OUTDOOR AMENITY	3669.53 ft²	340.91 m²	
LEVEL 5	31459.43 ft²	Total Area	77660.86 ft²	7206.92 m²	
	2922.68 m²				
LEVEL 6	27396.56 ft²				
	2548.22 m²				
254862.99 ft²	23677.51 m²				

PARCEL COVERAGE			
LOT DESCRIPTION	LOT AREA	BUILDING FOOTPRINT	PERCENT COVERAGE
EXISTING PARCEL	16565.91m² (178313.97ft²)	2911.10m² (31,334.81ft²)	17.57%
PROPOSED PARCEL	10454.72m² (112533.67ft²)	3830.04m² (38073.45ft²)	34.72%

UNIT MIX					
WA_AnR_UnitType	Area per Unit	# of Units	PERCENTAGE	Total Area	Total Area - Metric
A - STUDIO					
A1	342 ft²	36	17.56%	12319.02 ft²	1144.47 m²
B - 1 BED					
B1	638 ft²	36	17.56%	22991.16 ft²	2135.02 m²
B1 - ADAPTABLE UNIT	638 ft²	20	9.76%	12767.31 ft²	1186.12 m²
B2	650 ft²	30	14.63%	19731.80 ft²	1833.14 m²
B2*	650 ft²	20	9.76%	13005.83 ft²	1208.28 m²
B3	795 ft²	2	0.98%	1589.82 ft²	147.70 m²
B4	631 ft²	1	0.49%	631.49 ft²	58.67 m²
B5	701 ft²	1	0.49%	700.68 ft²	65.10 m²
C - 2 BED					
C1	982 ft²	8	3.90%	7856.30 ft²	728.78 m²
C2	908 ft²	4	1.95%	3630.24 ft²	337.26 m²
C3	860 ft²	4	1.95%	3441.48 ft²	319.72 m²
C4	917 ft²	4	1.95%	3668.87 ft²	340.69 m²
C5	820 ft²	3	1.46%	2461.25 ft²	228.66 m²
C6	876 ft²	1	0.49%	875.68 ft²	81.35 m²
D - 2 BED + DEN					
D1	<varies>	15	7.32%	16783.65 ft²	1466.35 m²
E - 3 BED					
E1	1171 ft²	4	1.95%	4683.12 ft²	435.08 m²
E2	1214 ft²	2	0.98%	2427.22 ft²	225.50 m²
E3	1131 ft²	2	0.98%	2261.45 ft²	210.10 m²
E4	1105 ft²	1	0.49%	1105.09 ft²	102.67 m²
F - LOFT					
F1 - LOFT	643 ft²	10	4.88%	6431.93 ft²	597.55 m²
F2 - LOFT	831 ft²	1	0.49%	830.96 ft²	77.20 m²
Total Area: 205					
Total Area: 205				138783.37 ft²	12893.40 m²
RESIDENTIAL DENSITY: 205 UNITS/1.05ha = 195.24 UNITS/ha				100.00%	

AMENITY AREAS				COMMERCIAL UNITS GROSS AREA	
WA_AnR_UnitType	DESCRIPTION	SQUARE FEET	SQUARE METRES	UNIT TYPE	TOTAL UNIT TYPE AREA (SF)
INDOOR AMENITY					
MULTI-PURPOSE ROOM	AMENITY	594.29 ft²	55.21 m²	CRU 1	1538.88 ft²
INDOOR AMENITY	AMENITY	1050.36 ft²	97.88 m²	CRU 2	1538.33 ft²
OUTDOOR AMENITY					
OUTDOOR AMENITY	OUTDOOR AMENITY	1644.64 ft²	152.79 m²	CRU 3	1653.82 ft²
OUTDOOR AMENITY	OUTDOOR AMENITY	1494.44 ft²	138.84 m²	CRU 4	1653.82 ft²
OUTDOOR AMENITY	OUTDOOR AMENITY	1494.44 ft²	138.84 m²	CRU 5	1653.82 ft²
OUTDOOR AMENITY	OUTDOOR AMENITY	3139.08 ft²	291.63 m²	CRU 6	1653.45 ft²
OUTDOOR AMENITY	OUTDOOR AMENITY	3139.08 ft²	291.63 m²	CRU 7	1654.16 ft²
OUTDOOR AMENITY	OUTDOOR AMENITY	3139.08 ft²	291.63 m²	CRU 8 (CAFE)	2337.65 ft²
GRAND TOTAL				CRU 9	15069.63 ft²

FOR CAR & BICYCLE PARKING DATA, PLEASE SEE SITE PLAN (PAGE A101)

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OWNER/CLIENT:



GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	23.08.31
NO.	ISSUE	Y/M/D

SEAL:



CONSULTANT:

2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V9B 0J5
804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:

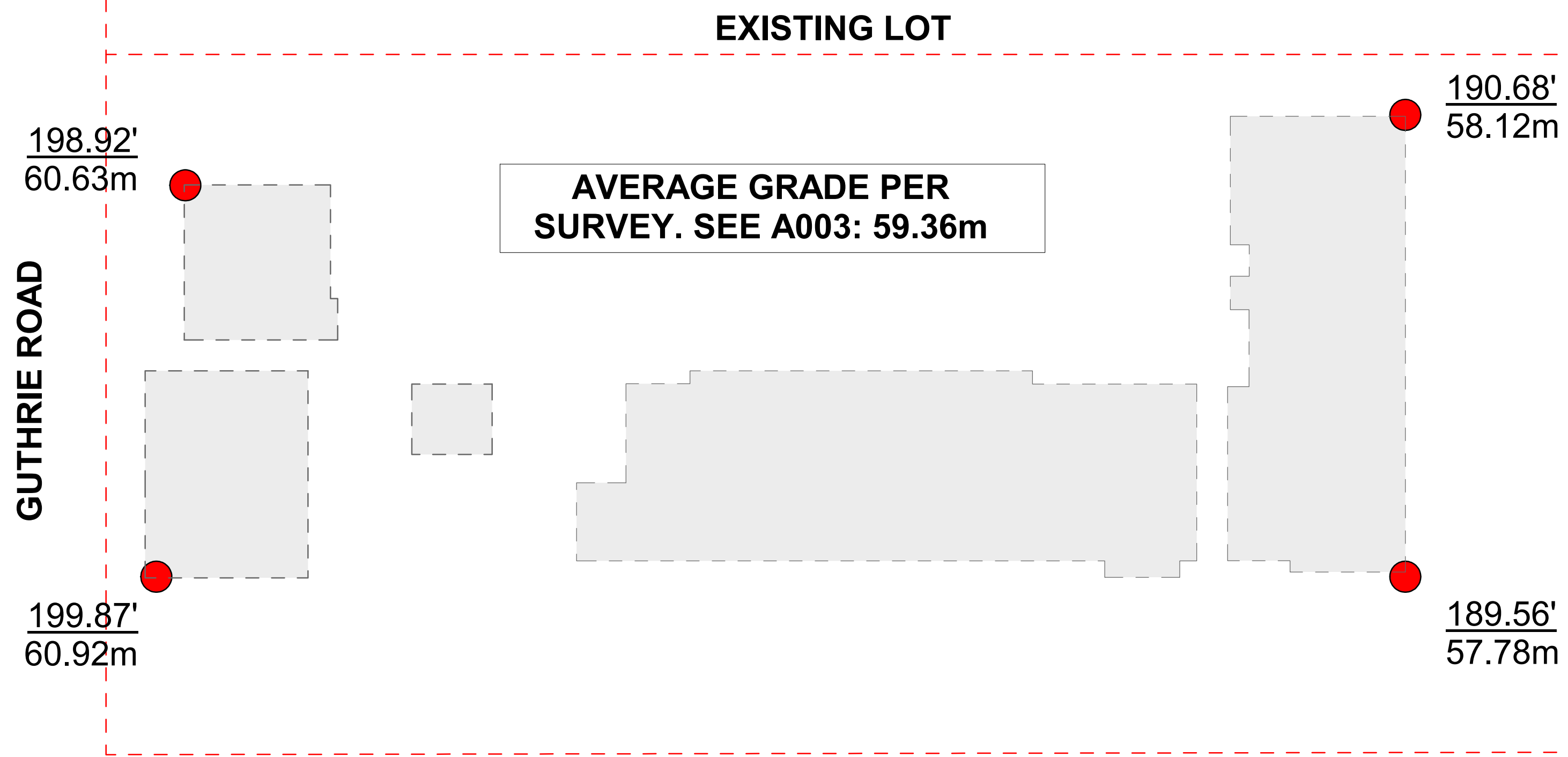
COVER

PROJECT NO: 21005 DRAWN BY: SD / WY

SCALE: 12" = 1'-0" REVIEW BY: AE

DWG NO:

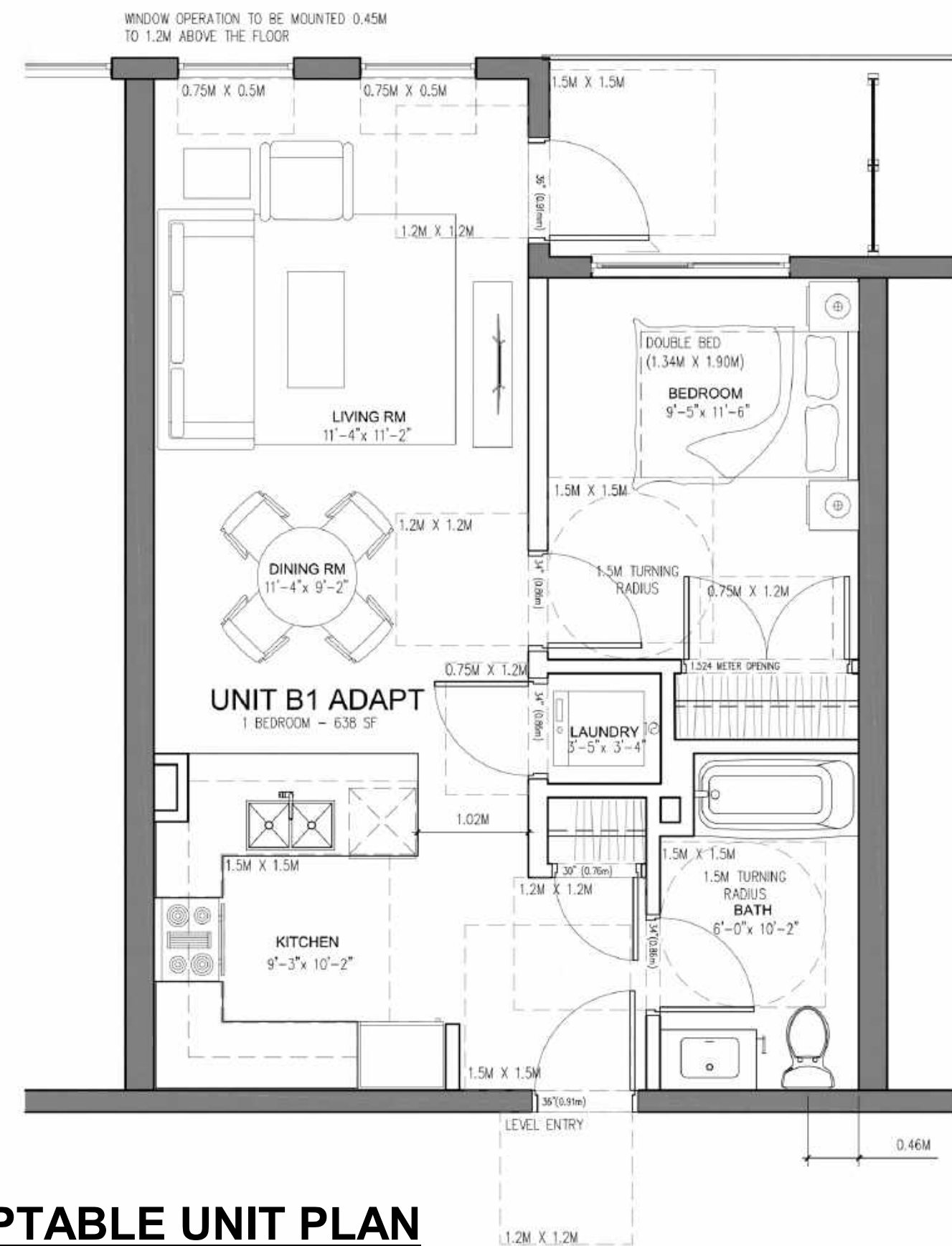
A001



AGRICULTURAL LAND RESERVE

AVERAGE GRADE CALC.

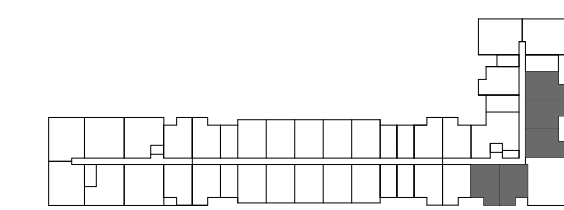
SCALE: 1:400



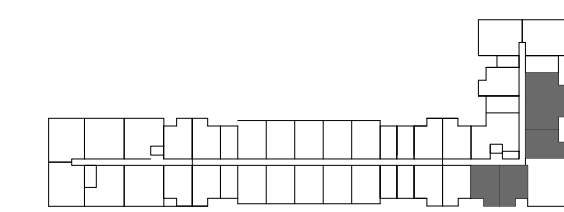
ADAPTABLE UNIT PLAN

SCALE: 1:50

ADAPTABLE UNIT KEYPLAN

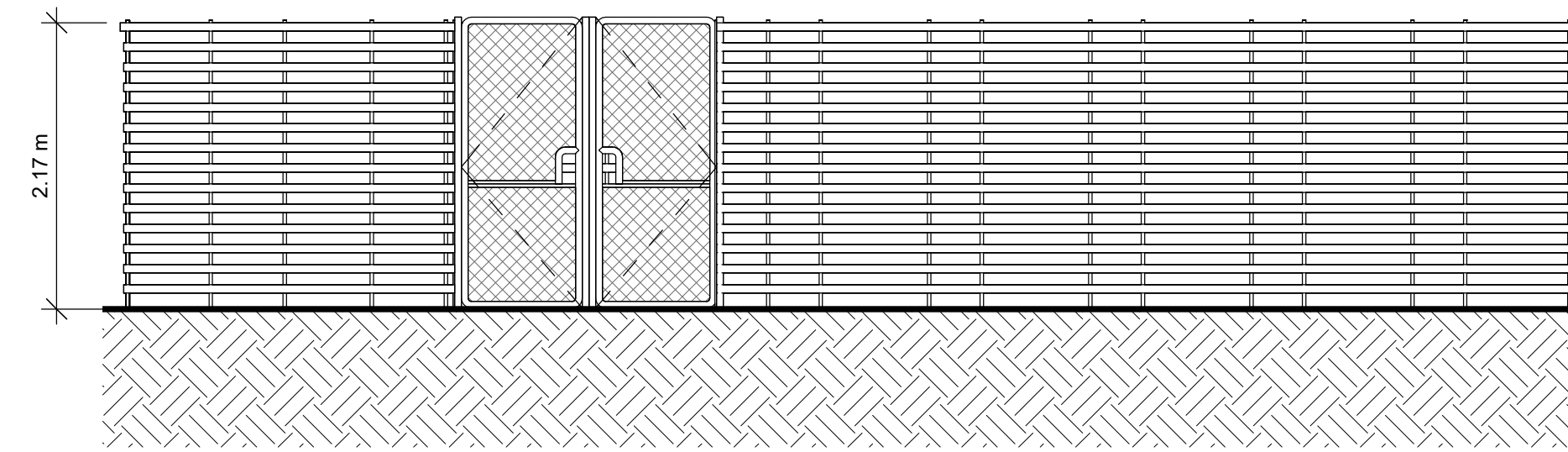


LEVEL 2



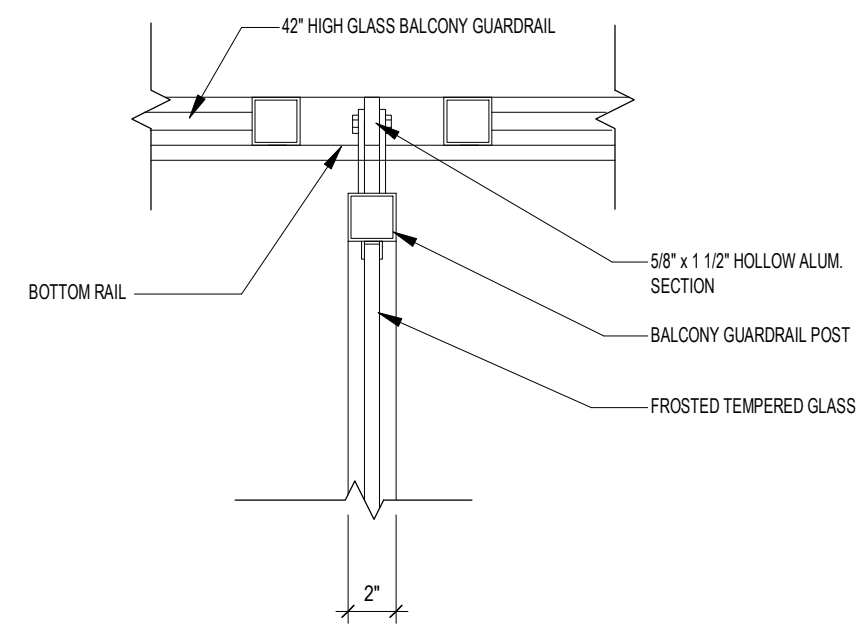
LEVEL 3-5

AGRICULTURAL LAND RESERVE



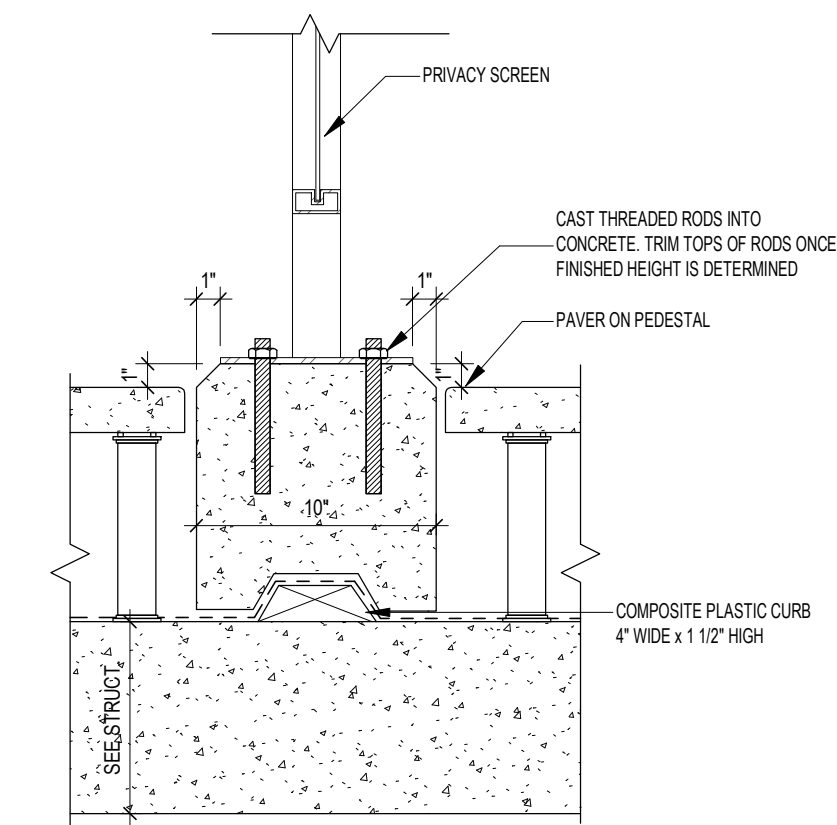
5 GARBAGE ENCLOSURE ELEVATION

REFER: A201
SCALE: 1/4" = 1'-0"



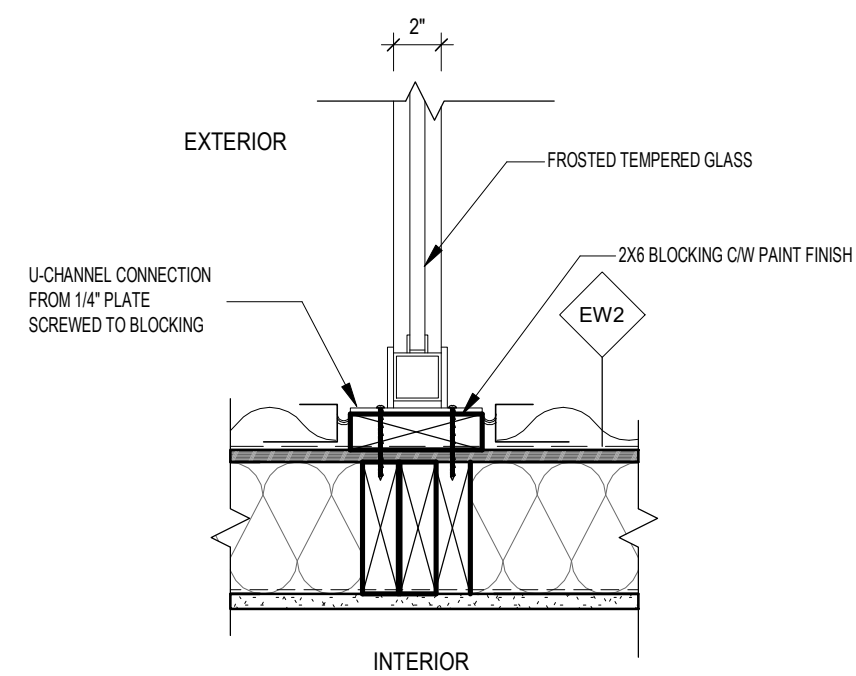
3 PRIVACY SCREEN RAILING CONNECTION

SCALE: 1 1/2" = 1'-0"



2 PRIVACY SCREEN MOUNTING DETAIL - CURB

SCALE: 1 1/2" = 1'-0"

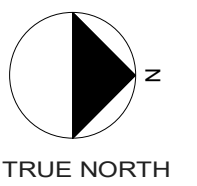


1 PRIVACY SCREEN WALL CONNECTION

SCALE: 1 1/2" = 1'-0"

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NORTH ARROWS:



OWNER/CLIENT:



GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	23.08.31

SEAL:



CONSULTANT:

2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:

SITE DETAILS

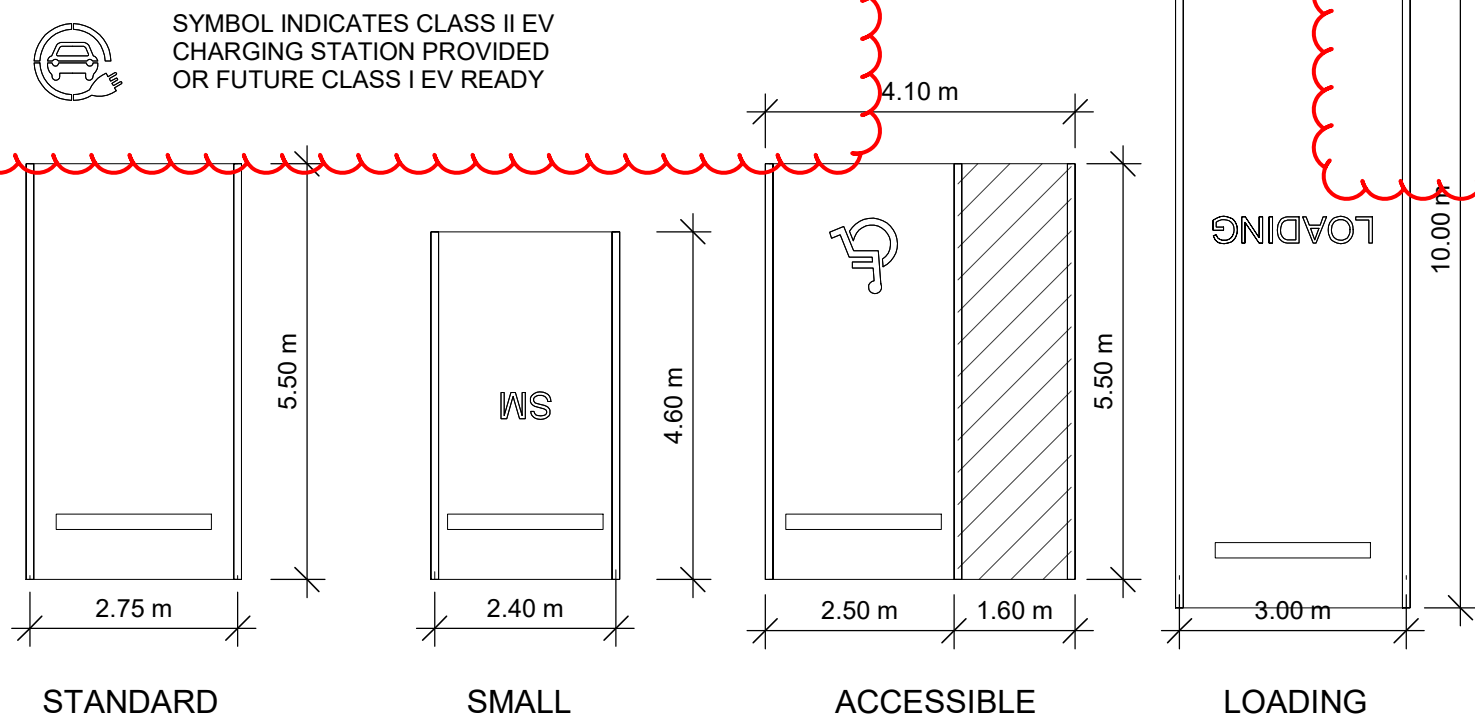
PROJECT NO: 21005 DRAWN BY: SD / WY

SCALE: As indicated REVIEW BY: AE

DWG NO:

A002

CAR PARKING LEGEND



REQ. CAR PARKING & BICYCLE PARKING SPACE

Table 1: Use Areas

Input the areas for each use in the appropriate orange cell.

Use	Area	Vehicle		Bicycle	
		Standard	Visitor	Requirements	Bicycle Class II
Dwelling, apartment	205 unit(s)	0.25 per unit	0.25 per unit	0.25 per unit	0.25 per unit
Retail Store less than 500 m ² gross floor area (excluding convenience stores and liquor stores)	1183.31 m ²	1 per 40m ²	1 per 125 m ² with a minimum of 2	1 per 125 m ² with a minimum of 2	1 per 125 m ² with a minimum of 2
TOTAL		225	51	50	50

Table 2: Vehicle Parking Space Reduction

Input the number of proposed bicycle parking spaces in the orange cells.

Proposed Bicycle Parking Spaces	Vehicle Parking Space Reduction		Total Reduction
	Class I	Class II	
Class I	12	28	40
Class II	311	28	339

Table 3: Accessible Parking Spaces

Accessible parking spaces are required in commercial zone.

Use	Minimum
Class I	1
Class II	2

LOT 1 - PHASE 1 & 2 - EXISTING PARKING CALCULATION

PROVIDED BY OTHERS. TABLE TAKEN FROM DP-08-10

Category	Count
Standard	229
Visitor	51
Overhead Vehicle	0
Total	280
Reduction ¹	28
Total Required	252
Restaurant	3
Overall Total	255

CAR PARKING PROPOSED

Table 4: Undergound Parking

Category	Count	Percentage
Accessible, 4x5.5m	6	2.33%
Accessible, 4x5.5m ELECTRIC	4	1.56%
Small, 2.4x4.6m	20	7.78%
Standard, 2.7x5.5m	104	40.47%
Standard, 2.7x5.5m ELECTRIC	61	23.74%
TOTAL	178	

BICYCLE PARKING PROPOSED

Table 5: Bicycle Parking

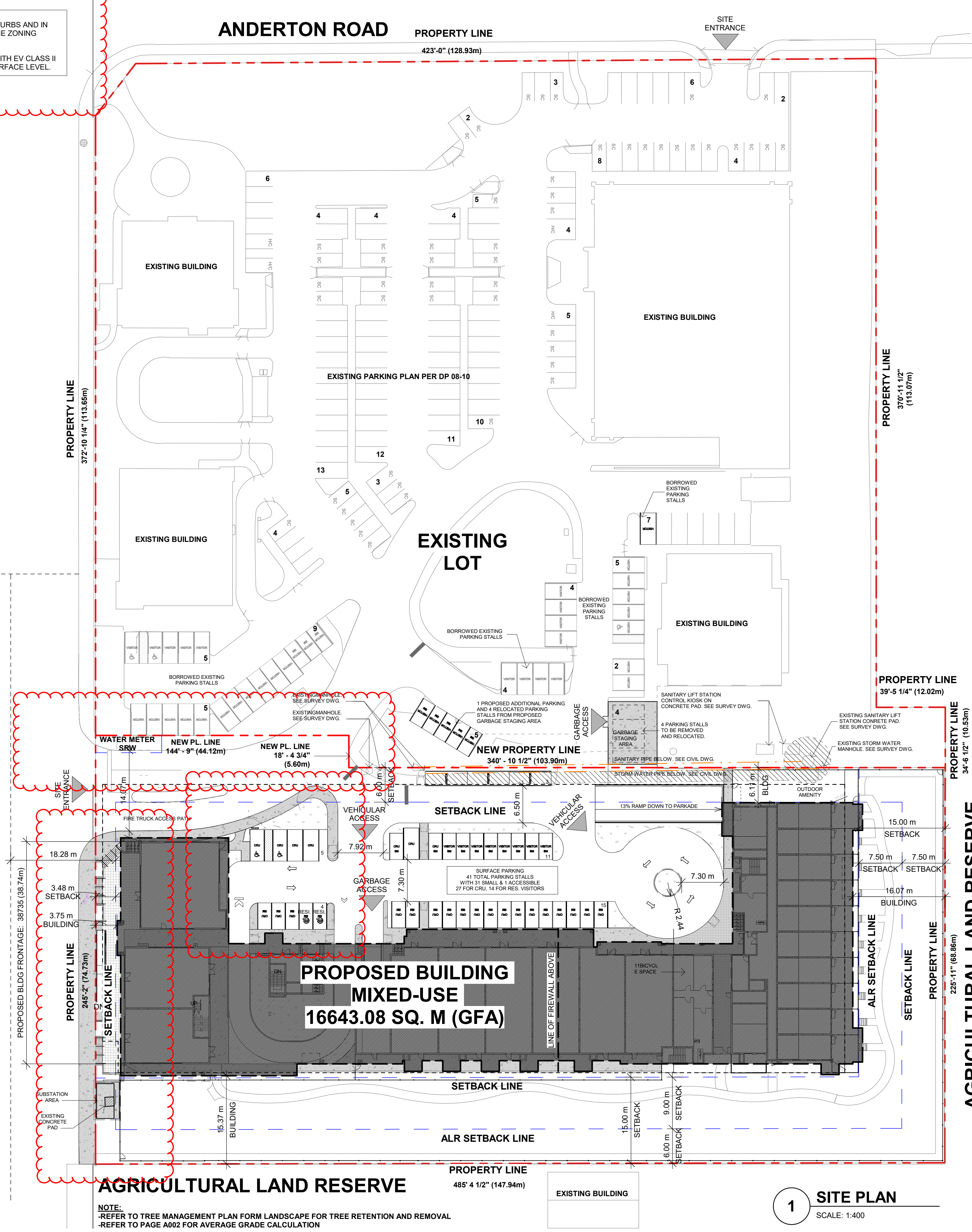
Category	Count
Class 1 - SURFACE	6
Class 1 - RESIDENTIAL	7
Class 1 - RETAIL	12
Class 2 - ENCLOSED	63
Class 2 - RESIDENTIAL - HORIZONTAL	1
Class 2 - CARGO BIKE	39
Class 2 - RESIDENTIAL - VERTICAL	18
Class 2 - RETAIL	113
TOTAL BICYCLE PARKING SPACE	125

MULTIFAMILY LOW DENSITY ZONING

COMMERCIAL ZONING

GUTHRIE ROAD

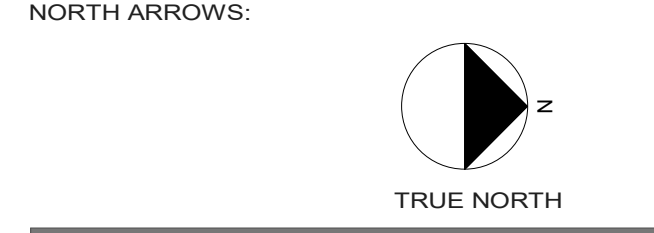
AGRICULTURAL LAND RESERVE



NOTE:
 -REFER TO TREE MANAGEMENT PLAN FORM LANDSCAPE FOR TREE RETENTION AND REMOVAL
 -REFER TO PAGE A002 FOR AVERAGE GRADE CALCULATION

1 SITE PLAN
 SCALE: 1:400

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OWNER/CLIENT:
AVTAR PROPERTIES

GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	23.08.31
NO.	ISSUE	Y/M/D



CONSULTANT:
 2024-07-23

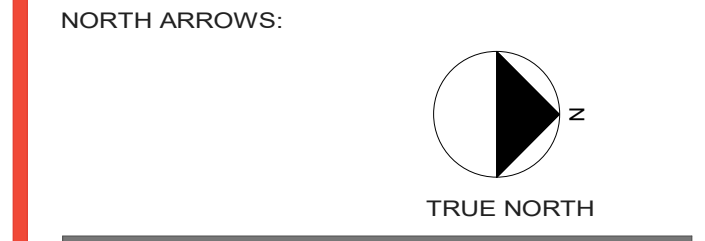
AGRICULTURAL LAND RESERVE

PROJECT NAME:
HIGHLAND VILLAGE

PROJECT ADDRESS:
 1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:
SITE PLAN

PROJECT NO: 21005
 SCALE: As indicated
 DRAWN BY: SD / WY
 REVIEW BY: AE
 DWG NO: A101



OWNER/CLIENT:
AVTAR PROPERTIES

GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/NVAR/DP RESUBMISSION	23.08.31
NO.	ISSUE	Y/M/D



CONSULTANT:
 2024-07-23

IWA ARCHITECTS

VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
 VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
 604.685.3529 | office@wa-arch.ca | wa-arch.ca

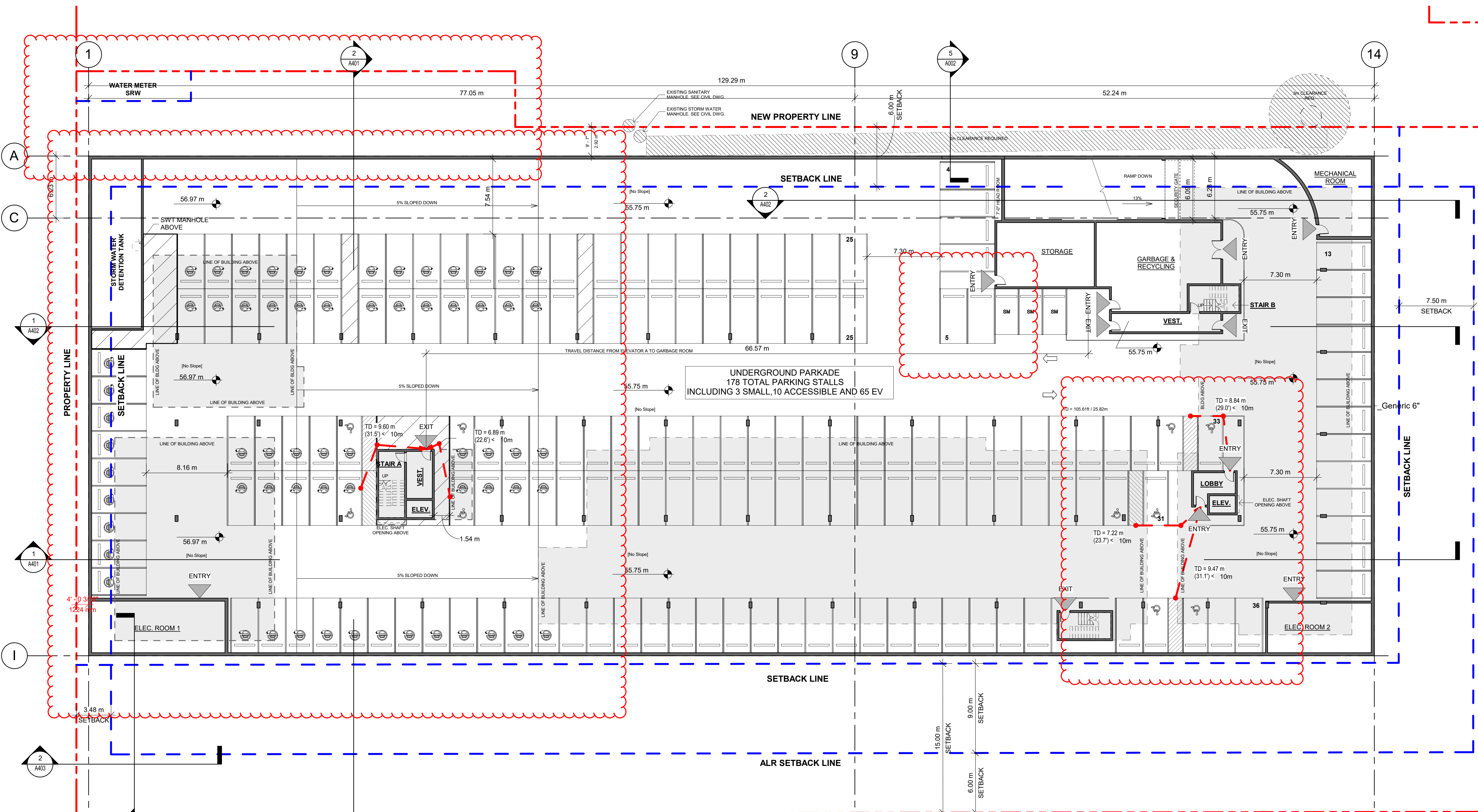
PROJECT NAME:
HIGHLAND VILLAGE

PROJECT ADDRESS:
 1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:
PARKADE P1 FLOOR PLAN

PROJECT NO: **21005** DRAWN BY: **SD / WY**
 SCALE: **As indicated** REVIEW BY: **AE**

DWG NO: **A201**



1 LEVEL P1
 SCALE: 1:200

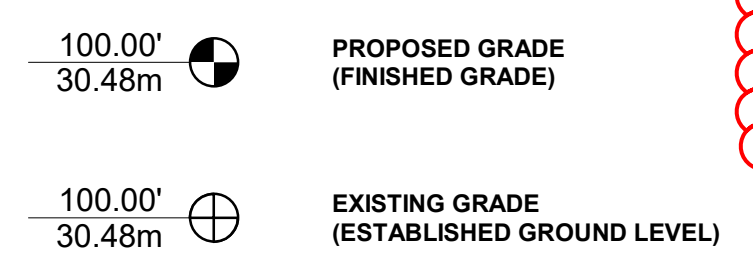
CAR PARKING PROPOSED

Category	Unit Type	Count	Area (sqm)	Percentage
RESIDENTIAL	Accessible, 4x5.5m	6	2.33%	
	Accessible, 4x5.5m ELECTRIC	4	1.56%	
	Small, 2.4x4.6m	3	1.17%	
	Standard, 2.75x5.5m	104	40.47%	
SURFACE PARKING	Standard, CRU, 2.75x5.5m	25	1.17%	
	Standard, CRU, 2.75x5.5m	25	1.17%	
RESIDENTIAL VISITOR	Small, Visitor, 2.4x4.6m	8	3.11%	
	Small, Visitor, Existing	3	1.17%	
	Standard, Visitor Parallel, 2.5x7.3m	11	4.22%	
RESIDENTIAL	Small, 2.4x4.6m ELECTRIC	2	0.78%	
	Small, 2.4x4.6m ELECTRIC	2	0.78%	
LOADING STALLS	Standard	1	0.39%	
	Standard	1	0.39%	
EXISTING PHASE BORROWED RESIDENTIAL VISITOR	Accessible Visitor, 4x5.5m	3	1.17%	
	Small, Visitor, 2.4x4.6m	5	1.96%	
	Small, Visitor, Existing	3	1.17%	
	Standard, Visitor, 2.75x5.5m	29	11.28%	
	Standard, Visitor, 2.75x5.5m	40	15.17%	
TOTAL CAR PARKING PROPOSED		287		

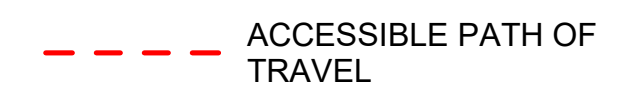
PROPERTY LINE

Unit Type	Area per Unit	Total Area - Metric
GARBAGE & RECYCLING	1197 ft ²	111.23 m ²
MECH / ELEC ROOM	541 ft ²	50.27 m ²
MECH / ELEC ROOM	875 ft ²	81.25 m ²
PI PARKING	6659 ft ²	618.45 m ²
STORAGE	748 ft ²	69.49 m ²

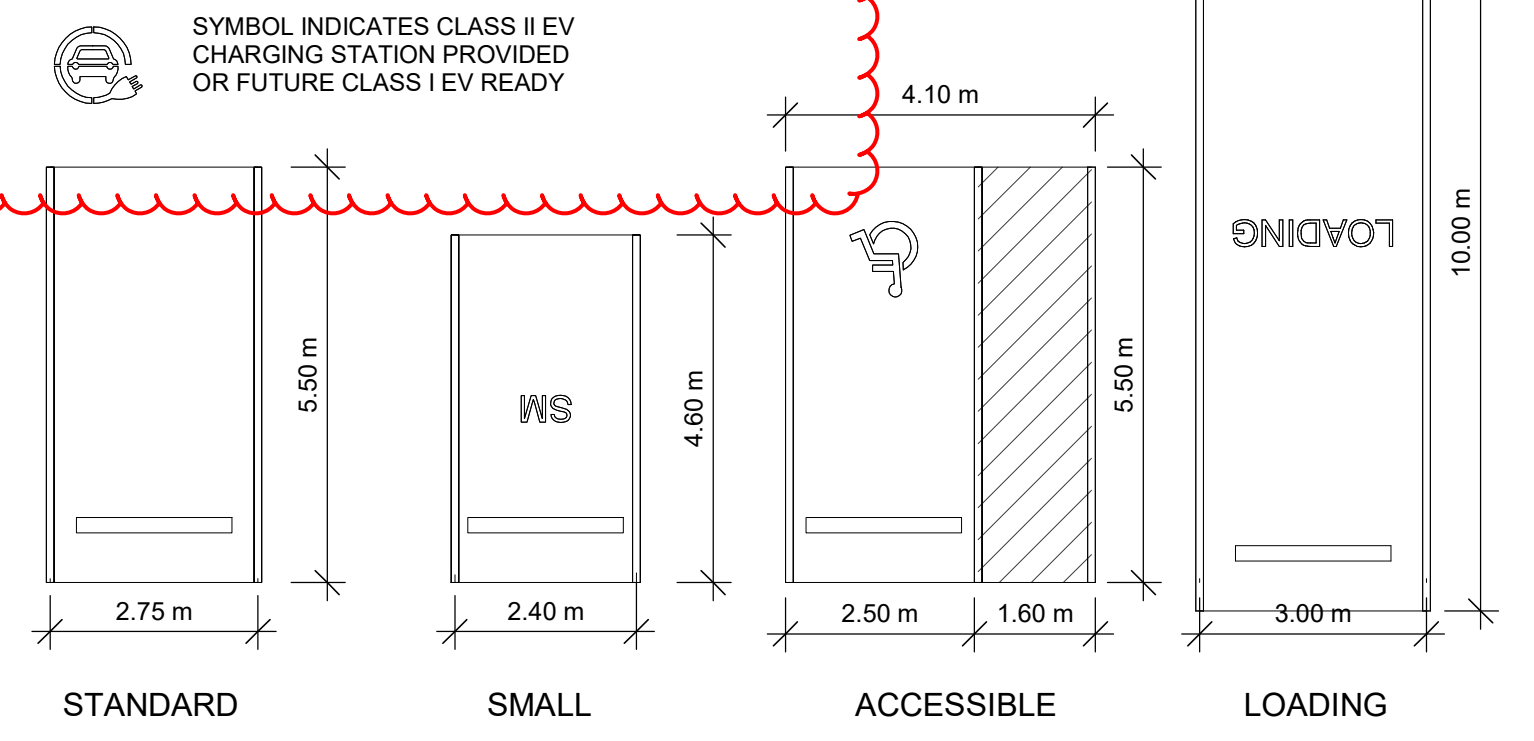
GRADE LEGEND



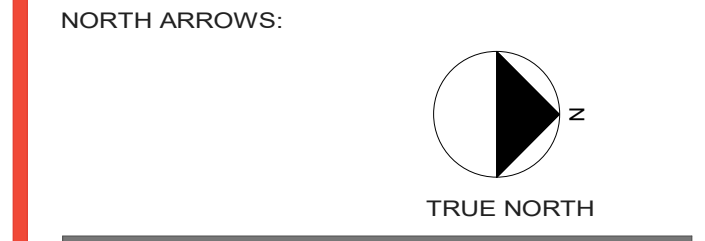
LEGEND



CAR PARKING LEGEND



NOTES:
 *PRE BYLAW 1850 - 6.8 REDUCTION OF 28 PARKING STALLS HAS BEEN APPLIED



OWNER/CLIENT:
AVTAR PROPERTIES

GENERAL NOTES:

NO.	ISSUE	DATE
5	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	23.08.31
1	ISSUE	Y/M/D

SEAL:

CONSULTANT:
2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
804.685.3529 | office@wa-arch.ca | wa-arch.ca

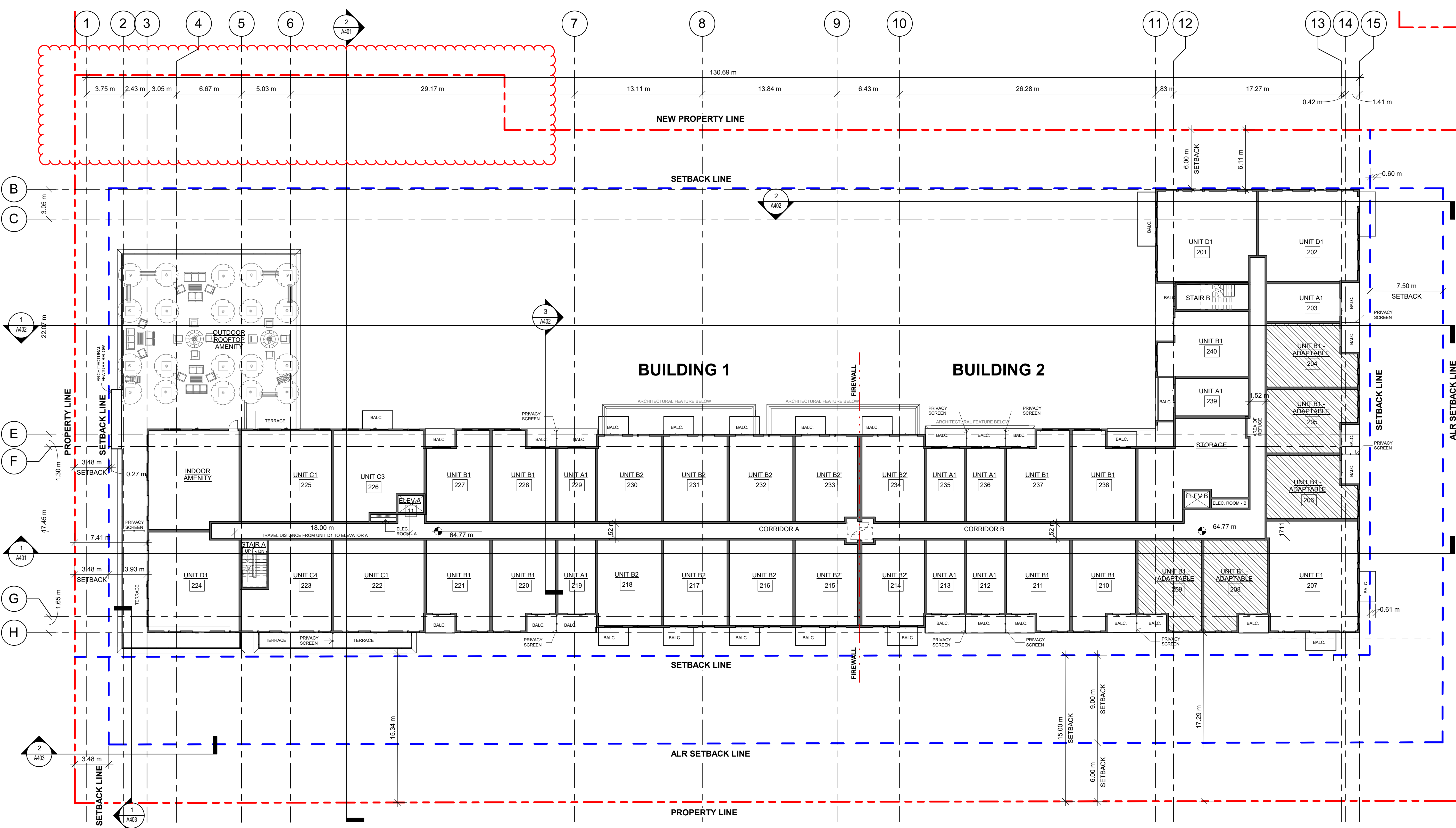
PROJECT NAME:
HIGHLAND VILLAGE

PROJECT ADDRESS:
1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:
LEVEL 2 FLOOR PLAN

PROJECT NO: 21005 DRAWN BY: WY
SCALE: As indicated REVIEW BY: AE

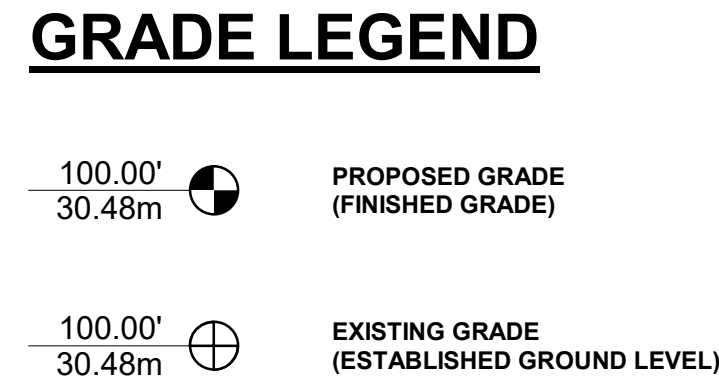
DWG NO: **A203**



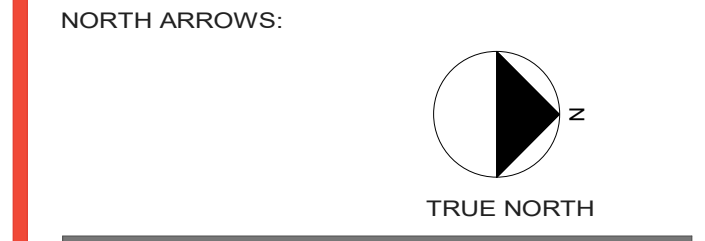
UNITS					
UnitType	Area per Unit	# of Units	PERCENTAGE	Total Area - Metric	Total Area - Imp
A - STUDIO					
A1	342 R ²	36	17.56%	12319 R ²	1144 m ²
B - 1 BED					
B1	638 R ²	36	17.56%	22981 R ²	2135 m ²
B1 - ADAPTABLE UNIT	638 R ²	20	9.76%	12767 R ²	1186 m ²
B2	658 R ²	30	14.63%	19732 R ²	1833 m ²
B2'	650 R ²	20	9.76%	13006 R ²	1208 m ²
B3	785 R ²	2	0.98%	1590 R ²	146 m ²
B4	631 R ²	1	0.49%	631 R ²	59 m ²
B5	701 R ²	1	0.49%	701 R ²	65 m ²
C - 2 BED					
C1	982 R ²	8	3.90%	7856 R ²	730 m ²
C2	908 R ²	4	1.95%	3632 R ²	337 m ²
C3	860 R ²	4	1.95%	3441 R ²	320 m ²
C4	817 R ²	4	1.95%	3269 R ²	304 m ²
C5	820 R ²	3	1.48%	2461 R ²	229 m ²

UNITS					
UnitType	Area per Unit	# of Units	PERCENTAGE	Total Area - Metric	Total Area - Imp
D - 2 BED + DEN					
D1	1059 R ²	7	3.41%	7363 R ²	683 m ²
D2	1054 R ²	8	3.90%	8431 R ²	783 m ²
E - 3 BED					
E1	1171 R ²	4	1.95%	4683 R ²	435 m ²
E2	1214 R ²	2	0.98%	2427 R ²	225 m ²
E3	1131 R ²	2	0.98%	2261 R ²	210 m ²
E4	1105 R ²	1	0.49%	1105 R ²	103 m ²
F - LOFT					
F1 - LOFT	643 R ²	10	4.85%	6432 R ²	599 m ²
F2 - LOFT	831 R ²	1	0.49%	831 R ²	77 m ²
Tota: 206					
		206	100.00%	138783 R ²	12893 m ²

AMENITY AND SERVICES - SECOND FLOOR		
UnitType	Area per Unit	Total Area - Metric
AMENITY		
INDOOR AMENITY	1050 R ²	97.58 m ²
CIRCULATION		
CORRIDOR A	1274 R ²	118.39 m ²
CORRIDOR B	1290 R ²	126.19 m ²
ELEVATOR A	69 R ²	6.39 m ²
ELEVATOR B	69 R ²	6.39 m ²
STAIR A	158 R ²	15.80 m ²
STAIR B	199 R ²	18.51 m ²
MECH ELEC		
ELEC. CL.	27 R ²	2.54 m ²
ELEC. CL.	46 R ²	4.31 m ²
OUTDOOR AMENITY		
OUTDOOR ROOFTOP AMENITY	3670 R ²	340.91 m ²
STR		
STORAGE	1006 R ²	93.47 m ²



1 LEVEL 2
REFER: A272
SCALE: 1:200



OWNER/CLIENT:
AVTAR PROPERTIES

GENERAL NOTES:

5	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	23.08.31
NO.	ISSUE	Y/M/D



CONSULTANT:
 2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
 VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
 804.685.3529 | office@wa-arch.ca | wa-arch.ca

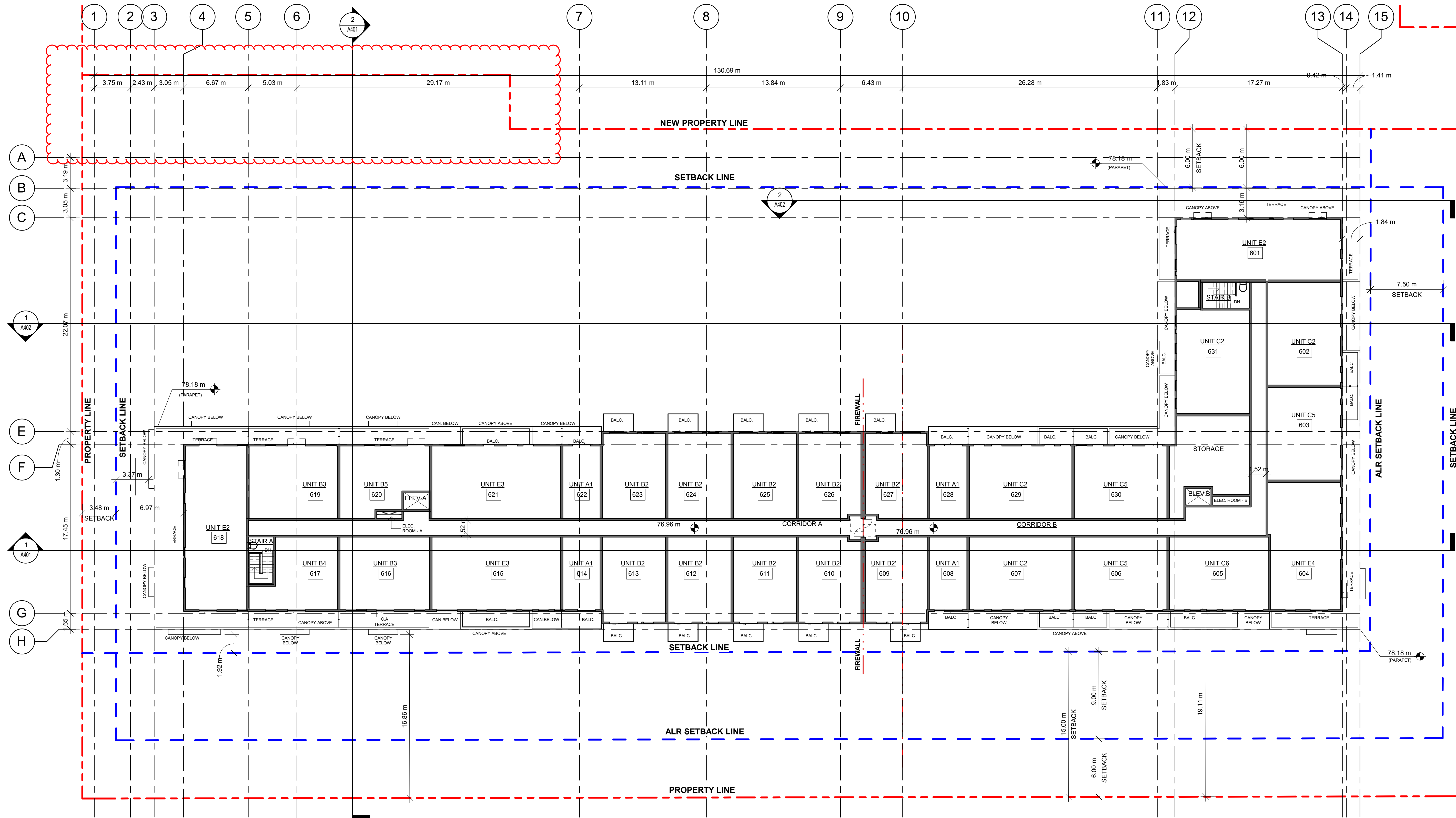
PROJECT NAME:
HIGHLAND VILLAGE

PROJECT ADDRESS:
 1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:
LEVEL 6 FLOOR PLAN

PROJECT NO: **21005** DRAWN BY: **WY**
 SCALE: **As indicated** REVIEW BY: **AE**

DWG NO: **A205**



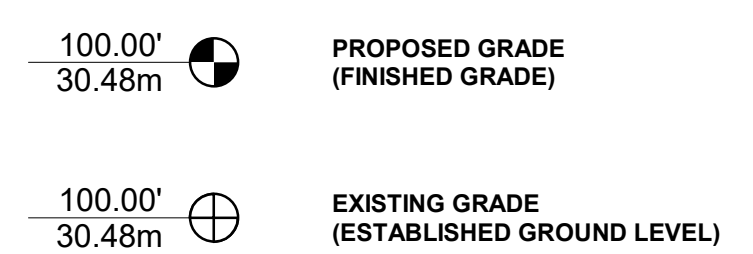
1 LEVEL 6
 REFER: A292
 SCALE: 1:200

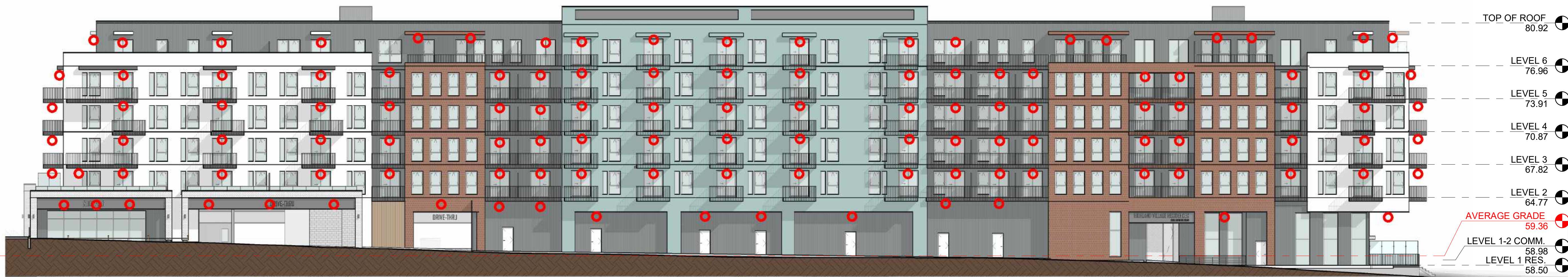
UNITS					
UnitType	Area per Unit	# of Units	PERCENTAGE	Total Area - Metric	Total Area - Metric
A - STUDIO					
A1	342 R ²	36	17.56%	12319 R ²	1144 m ²
B - 1 BED					
B1	638 R ²	36	17.56%	22981 R ²	2135 m ²
B1 - ADAPTABLE UNIT	638 R ²	20	9.76%	12767 R ²	1186 m ²
B2	658 R ²	30	14.63%	19732 R ²	1833 m ²
B2'	650 R ²	20	9.76%	13006 R ²	1208 m ²
B3	785 R ²	2	0.98%	1590 R ²	146 m ²
B4	631 R ²	1	0.49%	631 R ²	59 m ²
B5	791 R ²	1	0.49%	791 R ²	65 m ²
C - 2 BED					
C1	982 R ²	8	3.90%	7856 R ²	730 m ²
C2	908 R ²	4	1.95%	3632 R ²	337 m ²
C3	860 R ²	4	1.95%	3441 R ²	320 m ²
C4	817 R ²	4	1.95%	3269 R ²	304 m ²
C5	820 R ²	3	1.48%	2461 R ²	229 m ²

UNITS					
UnitType	Area per Unit	# of Units	PERCENTAGE	Total Area - Metric	Total Area - Metric
D - 2 BED + DEN					
D1	1059 R ²	7	3.41%	7363 R ²	683 m ²
D1	1054 R ²	8	3.90%	8431 R ²	783 m ²
E - 3 BED					
E1	1171 R ²	4	1.95%	4683 R ²	435 m ²
E2	1214 R ²	2	0.98%	2427 R ²	225 m ²
E3	1131 R ²	2	0.98%	2261 R ²	210 m ²
E4	1106 R ²	1	0.49%	1106 R ²	103 m ²
F - LOFT					
F1 - LOFT	643 R ²	10	4.89%	6432 R ²	599 m ²
F2 - LOFT	831 R ²	1	0.49%	831 R ²	77 m ²
Total: 205					
		205	100.00%	138783 R ²	12893 m ²

AMENITY AND SERVICES - SECOND FLOOR		
UnitType	Area per Unit	Total Area - Metric
AMENITY		
INDOOR AMENITY	1050 R ²	97.88 m ²
CIRCULATION		
CORRIDOR A	1274 R ²	118.39 m ²
CORRIDOR B	1290 R ²	128.10 m ²
ELEVATOR A	69 R ²	6.39 m ²
ELEVATOR B	69 R ²	6.39 m ²
STAIR A	150 R ²	15.80 m ²
STAIR B	199 R ²	18.51 m ²
MECH ELEC		
ELEC. CL.	27 R ²	2.54 m ²
ELEC. CL.	46 R ²	4.31 m ²
OUTDOOR AMENITY		
OUTDOOR ROOFTOP AMENITY	3670 R ²	340.91 m ²
STR		
STORAGE	1006 R ²	93.47 m ²

GRADE LEGEND





1 LIGHTING PLAN EAST ELEVATION

SCALE: 1 : 200



2 LIGHTING PLAN - NORTH ELEVATION

SCALE: 1 : 200

NOTES

1. DIAGRAMMATIC LIGHTING DRAWINGS ARE SHOWN FOR INTENDED FORM AND CHARACTER PURPOSES. DETAILED LIGHTING PLANS AND CODE COMPLIANCE DRAWINGS WILL BE ADDRESSED FOR BUILDING PERMIT APPLICATION. ALL FIXTURE TYPES, NUMBER AND LUMEN OUTPUT, AS WELL AS EMERGENCY LIGHTING REQUIRED BY CODE, TO BE CONFIRMED WITH ELECTRICAL ENGINEER FOR BUILDING PERMIT SUBMISSION.

2. LIGHT COLOUR TO BE BETWEEN 2500-3000K.

3. ILLUMINATION DIRECTION INTENDED TO BE GENERALLY LOW LEVEL AND POSITIONED TO REDUCE SPILL TOWARDS NEIGHBORING PROPERTY OR RESIDENTIAL UNITS. ACCESS / EXIT PATHS AND BUILDING SIGNAGE TO BE PRIORITIZED.

4. LIGHTING IMAGES ARE FOR REPRESENTATION OF DESIGN INTENT ONLY. EXACT FIXTURE TYPES WILL NOT BE FINALIZED UNTIL CLOSER TO BUILDING AND CONSTRUCTION ATAGE.

LIGHTING LEGEND

- BOLLARD LIGHTING - LOW LEVEL DOWNWARD FACING TO ILLUMINATE PEDESTRIAN PATHS AND SURFACE DRIVE ISLES
- SOFFIT LIGHTING - DOWNWARD FACING TO ILLUMINATE BUILDING ENTRANCES/BALCONIES
- ▶ EXTERNAL FLOOR MOUNTED ILLUMINARIES DIRECTED AT BUILDING SIGNAGE AND ADDRESS
- EXTERIOR STEP LIGHTING
- ✕ SCONCE LIGHTING - BACK FACING TO ILLUMINATE STREET BUILDING ENTRANCES AND COLUMNS



○ SOFFIT MOUNTED LIGHTING



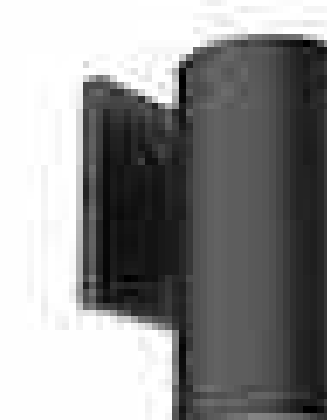
▶ FLOOR MOUNTED EXTERNAL LUMINARIES



● BOLLARD LIGHTING



— EXTERIOR STEP LIGHTING



✕ WALL MOUNTED SCONCE LIGHTING

SAMPLE LIGHT FIXTURES DO NOT REFLECT FINAL DESIGN

ACCORDANCE TO DESIGN GUIDELINE 3.4.5.2 (13)(A)

IN ACCORDANCE WITH DESIGN GUIDELINE 3.4.5.2(13)(A) AND 3.4.5.3(3) LIGHTING REQUIREMENTS, WE ARE PROPOSING LIGHT FIXTURES IN SIDEWALKS, EXTERIOR PATHS, BREEZEWAYS, UNIT BALCONIES, TERRACES, AND PARKING AREAS TO PROMOTE A SAFETY ENVIRONMENT AND EVENING USE.

OWNER/CLIENT:

GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/NAV/DP RESUBMISSION	24.02.16

SEAL:



CONSULTANT:

2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:

LIGHTING ELEVATIONS

PROJECT NO: 21005 DRAWN BY: FH

SCALE: As indicated REVIEW BY: AE

DWG NO:

A292

OWNER/CLIENT:



GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	23.08.31

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	23.08.31

SEAL:



CONSULTANT:

2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road, Comox, BC V9M 3X7

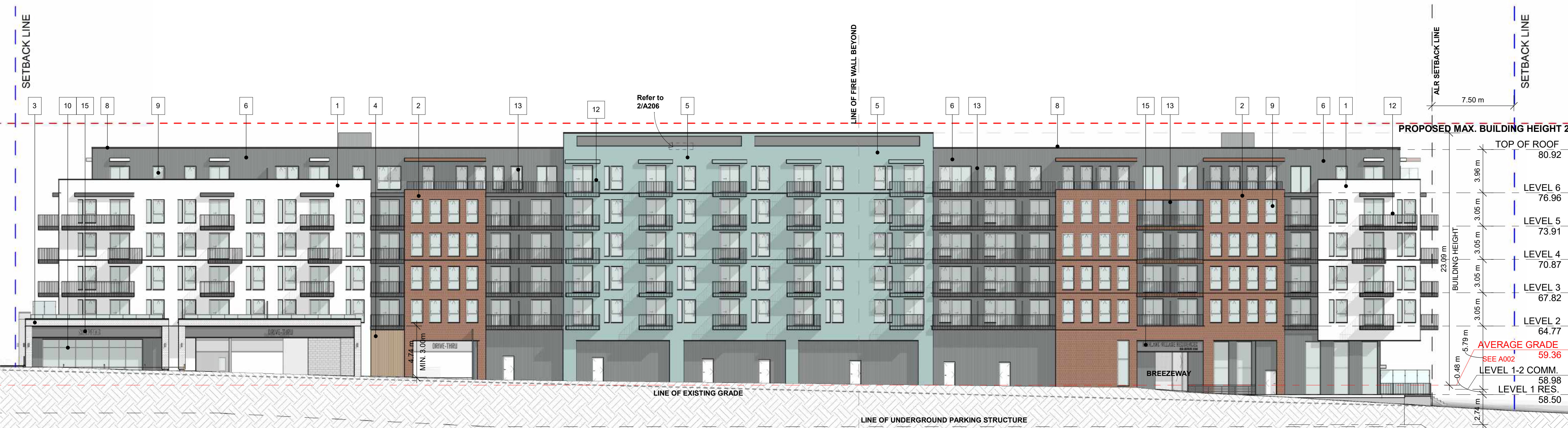
DRAWING TITLE:

ELEVATIONS

PROJECT NO: 21005 DRAWN BY: WY

SCALE: 1:200 REVIEW BY: AE

DWG NO: A301



- 1 CEMENTITIOUS FINISH - WHITE
- 2 BRICK VENEER - RUSTIC
- 3 BRICK VENEER - WHITE
- 4 VINYL PLANKS - VERTICAL WOOD LOOK
- 5 WOOD BOARD AND BATTEN - VERTICAL TEAL
- 6 GALVANIZED CORRUGATED METAL - HORIZONTAL GREY
- 8 METAL FLASHING - CHARCOAL
- 9 VINYL WINDOWS AND PATIO DOORS - NATURAL ALUMINUM
- 10 ALUMINUM STOREFRONT WINDOW WALL - NATURAL ALUMINUM
- 11 PARKADE GARAGE DOOR - CHARCOAL
- 12 ALUMINUM EXTRUDED PICKET GUARDRAIL - CHARCOAL
- 13 ALUMINUM EXTRUDED PRIVACY SCREEN - CHARCOAL WITH TEXTURED GLASS
- 14 METAL CANOPY - CHARCOAL
- 15 METAL SIGNAGE - NATURAL ALUMINUM
- 16 GLASS GUARDRAIL FRAME ALUMINUM EXTRUDED - CHARCOAL



VIEW LOOKING SOUTH WEST FROM ALR BUFFER TOWARDS THE LOFTS UNITS

OWNER/CLIENT:



GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	23.08.31
1	ISSUE	Y/M/D

SEAL:



CONSULTANT:

2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:

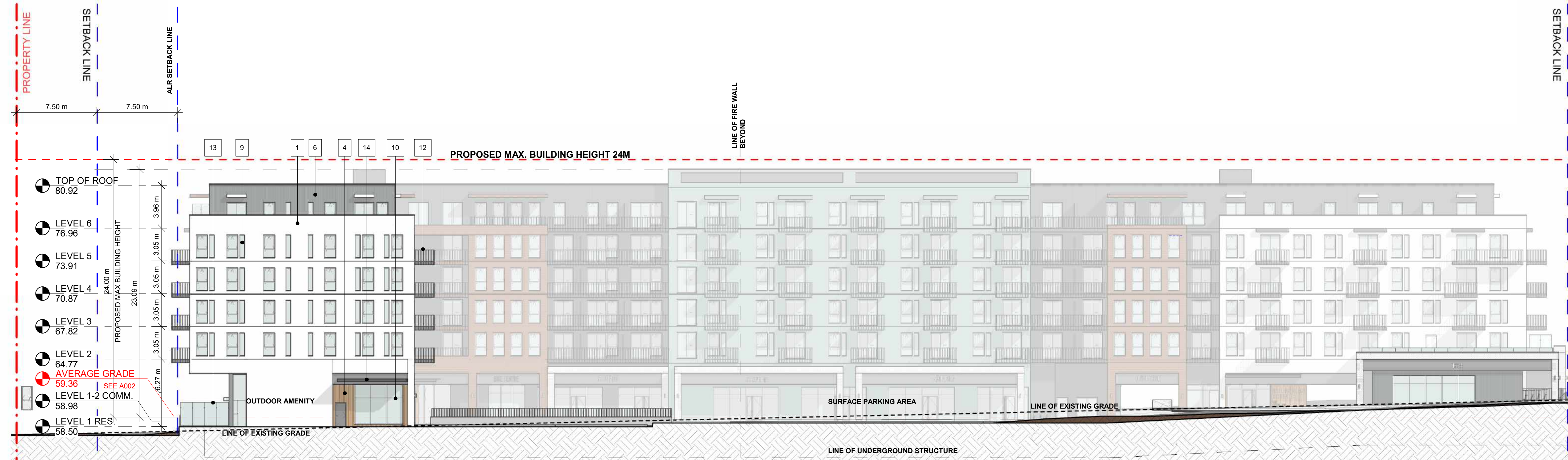
ELEVATIONS

PROJECT NO: 21005 DRAWN BY: WY

SCALE: 1:200 REVIEW BY: AE

DWG NO:

A302



1 WEST ELEVATION

REFER: A280
SCALE: 1:200

- 1 CEMENTITIOUS FINISH - WHITE
- 2 BRICK VENEER - RUSTIC
- 3 BRICK VENEER - WHITE
- 4 VINYL PLANKS - VERTICAL WOOD LOOK
- 5 WOOD BOARD AND BATTEN - VERTICAL TEAL
- 6 GALVANIZED CORRUGATED METAL - HORIZONTAL GREY
- 8 METAL FLASHING - CHARCOAL
- 9 VINYL WINDOWS AND PATIO DOORS - NATURAL ALUMINUM
- 10 ALUMINUM STOREFRONT WINDOW WALL - NATURAL ALUMINUM
- 11 PARKADE GARAGE DOOR - CHARCOAL
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- 13 ALUMINUM EXTRUDED PRIVACY SCREEN - CHARCOAL WITH TEXTURED GLASS
- 14 METAL CANOPY - CHARCOAL
- 15 METAL SIGNAGE - NATURAL ALUMINUM
- 16 GLASS GUARDRAIL FRAME ALUMINUM EXTRUDED - CHARCOAL



2 SOUTH ELEVATION

REFER: A280
SCALE: 1:200



VIEW LOOKING NORTH EAST ALONG GUTHERIE ROAD

OWNER/CLIENT:



GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	23.08.31
1	ISSUE	Y/M/D

SEAL:



CONSULTANT:

2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:

ELEVATIONS

PROJECT NO: 21005 DRAWN BY: SD / WY

SCALE: 1:200 REVIEW BY: AE

DWG NO:

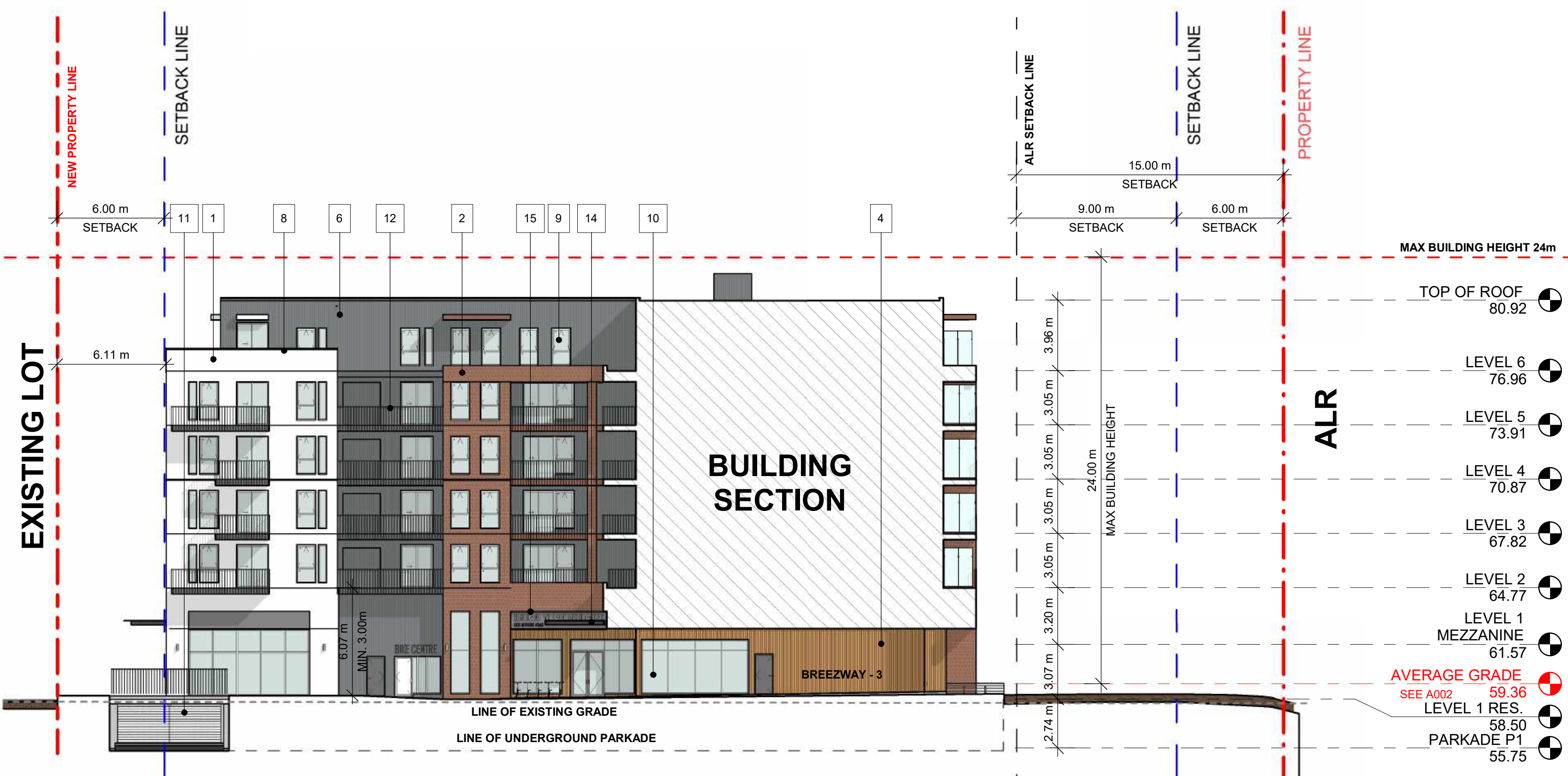
A303



1 PARTIAL WEST ELEVATION

REFER: A280
SCALE: 1:200

- 1 CEMENTITIOUS FINISH - WHITE
- 2 BRICK VENEER - RUSTIC
- 3 BRICK VENEER - WHITE
- 4 VINYL PLANKS - VERTICAL WOOD LOOK
- 5 WOOD BOARD AND BATTEN - VERTICAL TEAL
- 6 GALVANIZED CORRUGATED METAL - HORIZONTAL GREY
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- 12 ALUMINUM EXTRUDED PICKET GUARDRAIL - CHARCOAL
- 13 ALUMINUM EXTRUDED PRIVACY SCREEN - CHARCOAL WITH TEXTURED GLASS
- 14 METAL CANOPY - CHARCOAL
- 15 METAL SIGNAGE - NATURAL ALUMINUM
- 16 GLASS GUARDRAIL FRAME ALUMINUM EXTRUDED - CHARCOAL



2 PARTIAL SOUTH ELEVATION

REFER: A280
SCALE: 1:200



VIEW LOOKING AT THE RESIDENTIAL LOBBY ENTRANCE (NORTH VIEW)

OWNER/CLIENT:

GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	23.08.31

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZNVAR/DP RESUBMISSION	23.08.31

SEAL:



CONSULTANT:

2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
 VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
 804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road, Comox, BC V9M 3X7

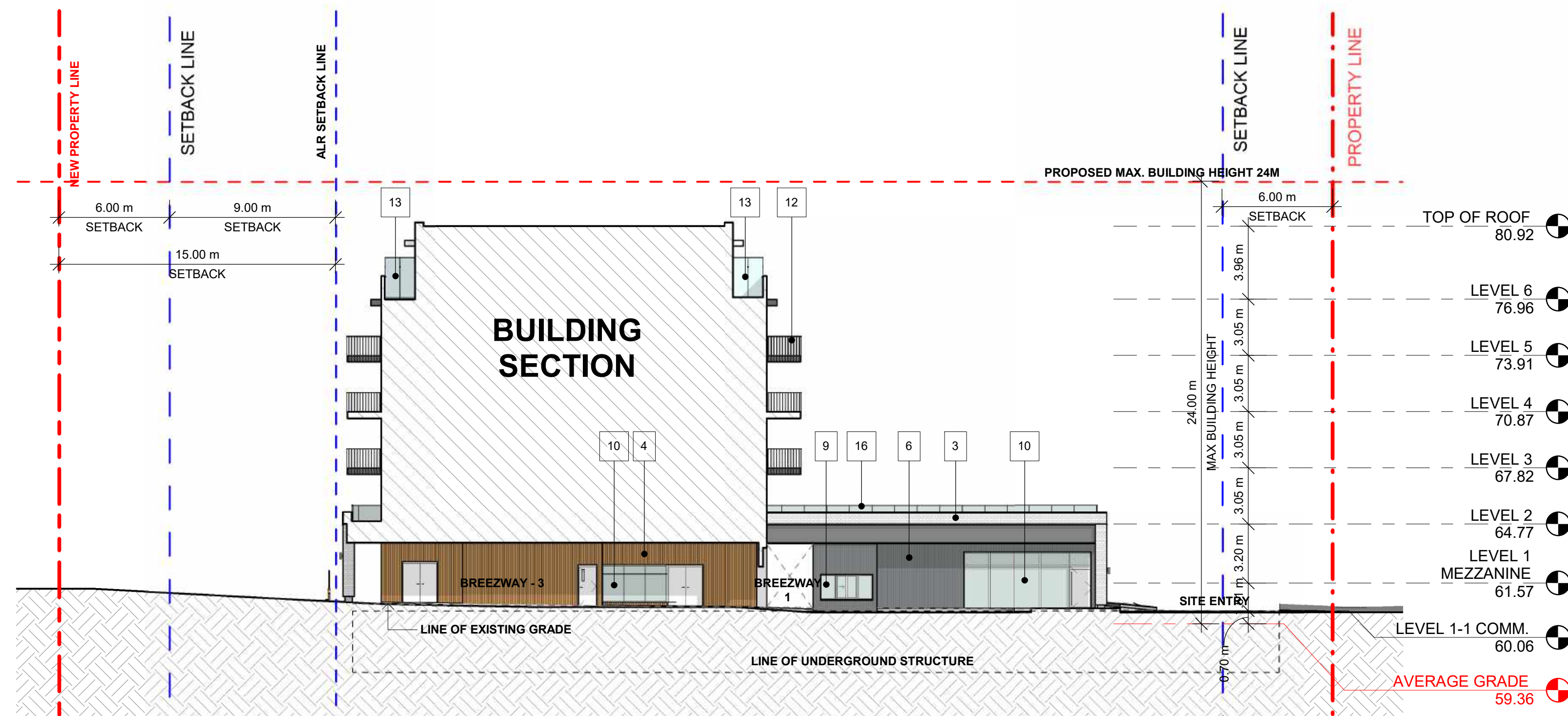
DRAWING TITLE:

ELEVATIONS

PROJECT NO: 21005 DRAWN BY: WY

SCALE: As indicated REVIEW BY: AE

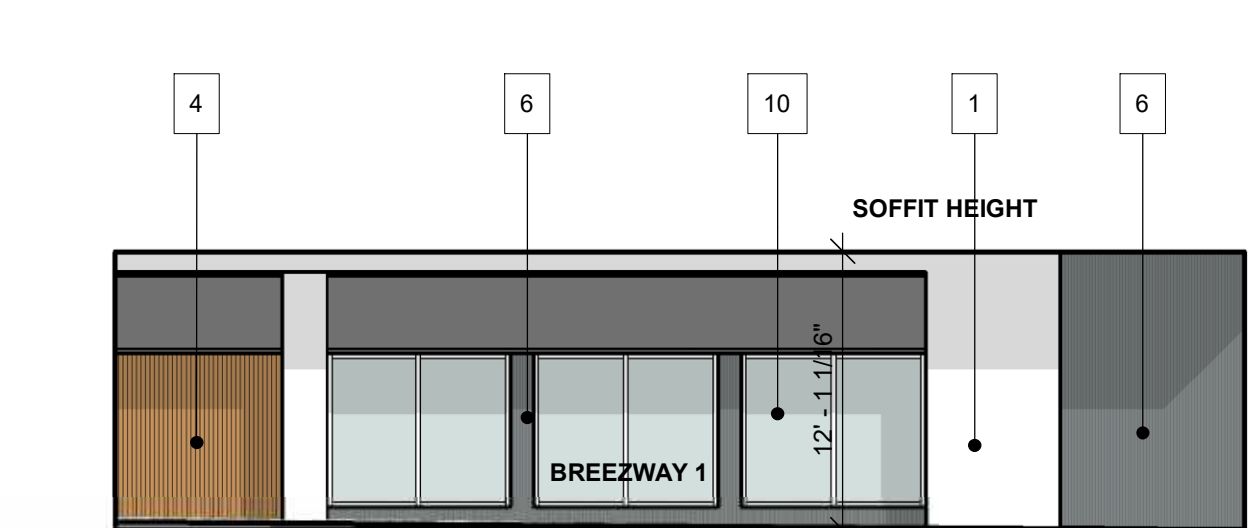
DWG NO: A304



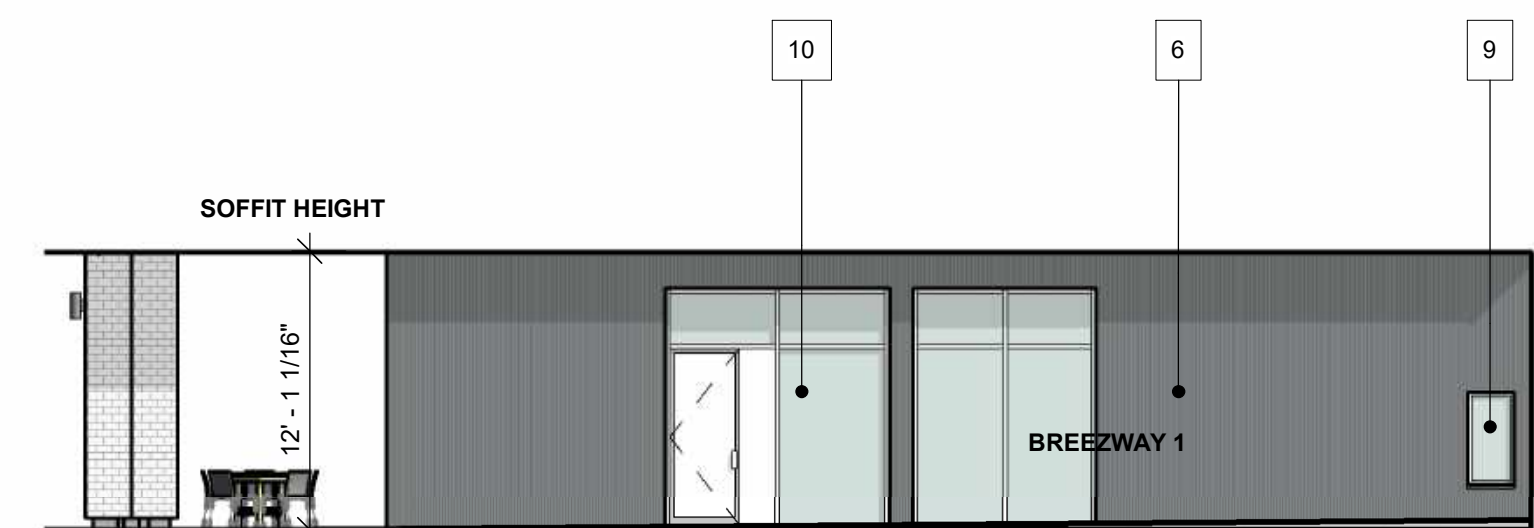
1 PARTIAL NORTH ELEVATION
 SCALE: 1 : 200

- 1 CEMENTITIOUS FINISH - WHITE
- 2 BRICK VENEER - RUSTIC
- 3 BRICK VENEER - WHITE
- 4 VINYL PLANKS - VERTICAL WOOD LOOK
- 5 WOOD BOARD AND BATTEN - VERTICAL TEAL
- 6 GALVANIZED CORRUGATED METAL - HORIZONTAL GREY
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- 15 METAL SIGNAGE - NATURAL ALUMINUM
- 16 GLASS GUARDRAIL FRAME ALUMINUM EXTRUDED - CHARCOAL

BREEZWAY INTERIOR ELEVATIONS

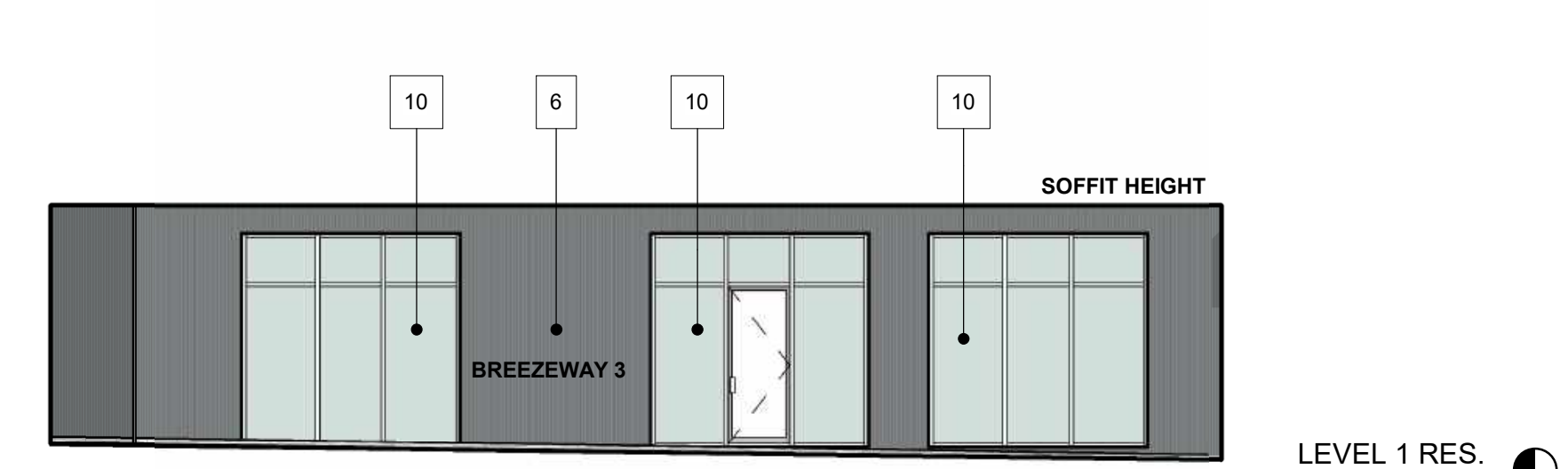


2 BREEZWAY 1 - WEST INTERIOR ELEVATION
 SCALE: 1 : 100



3 BREEZWAY 1 - EAST INTERIOR ELEVATION
 SCALE: 1 : 100

LEVEL 1-1 COMM. 60.06



4 BREEZWAY 3 - NORTH INTERIOR ELEVATION
 SCALE: 1 : 100

LEVEL 1 RES. 58.50

OWNER/CLIENT:

GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.04.05
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2	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	23.08.31
NO.	ISSUE	Y/M/D



CONSULTANT: 2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
 VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
 804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:
HIGHLAND VILLAGE

PROJECT ADDRESS:
 1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:
COMPARATIVE ELEVATIONS

PROJECT NO: 21005 DRAWN BY: WY
 SCALE: 1:200 REVIEW BY: AE

DWG NO: **A305**



2 SOUTH ELEVATION - RECESSED PATIO OPTION
 SCALE: 1 : 200



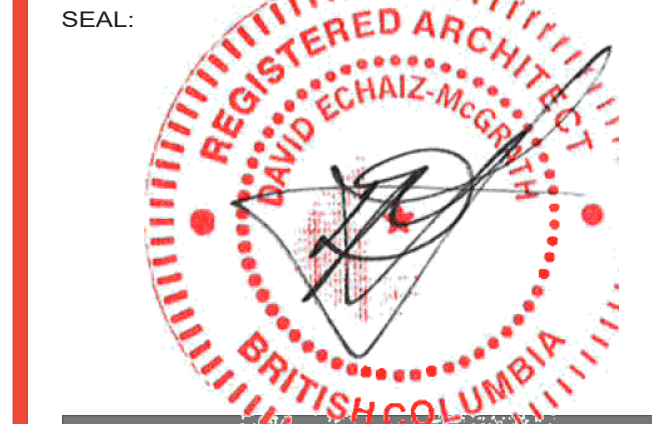
1 SOUTH ELEVATION - WITHOUT RECESSED PATIO OPTION
 SCALE: 1 : 200

OWNER/CLIENT:



GENERAL NOTES:

NO.	ISSUE	Y/M/D
5	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.07.23
4	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.04.05
3	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.02.16
2	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	24.01.19
1	ISSUE FOR SUB/OCP/RZ/INVAR/DP RESUBMISSION	23.08.31
NO.	ISSUE	Y/M/D



CONSULTANT: 2024-07-23



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
 VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
 804.685.3529 | office@wa-arch.ca | wa-arch.ca

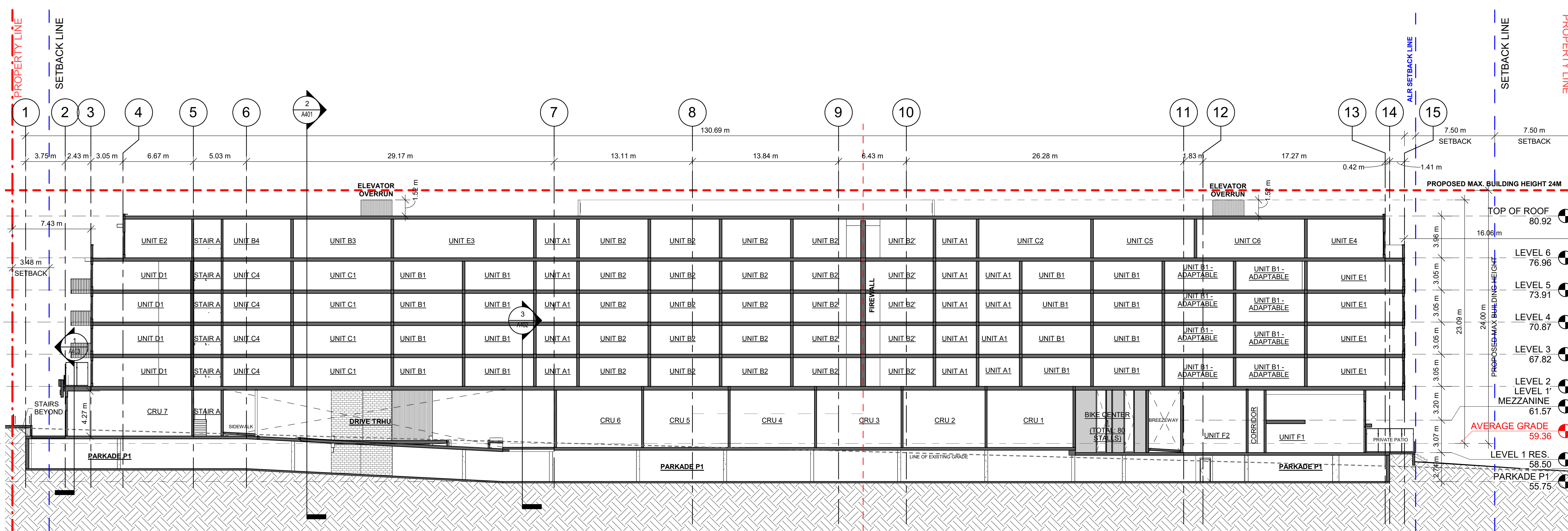
PROJECT NAME:
HIGHLAND VILLAGE

PROJECT ADDRESS:
 1966 Guthrie Road, Comox, BC V9M 3X7

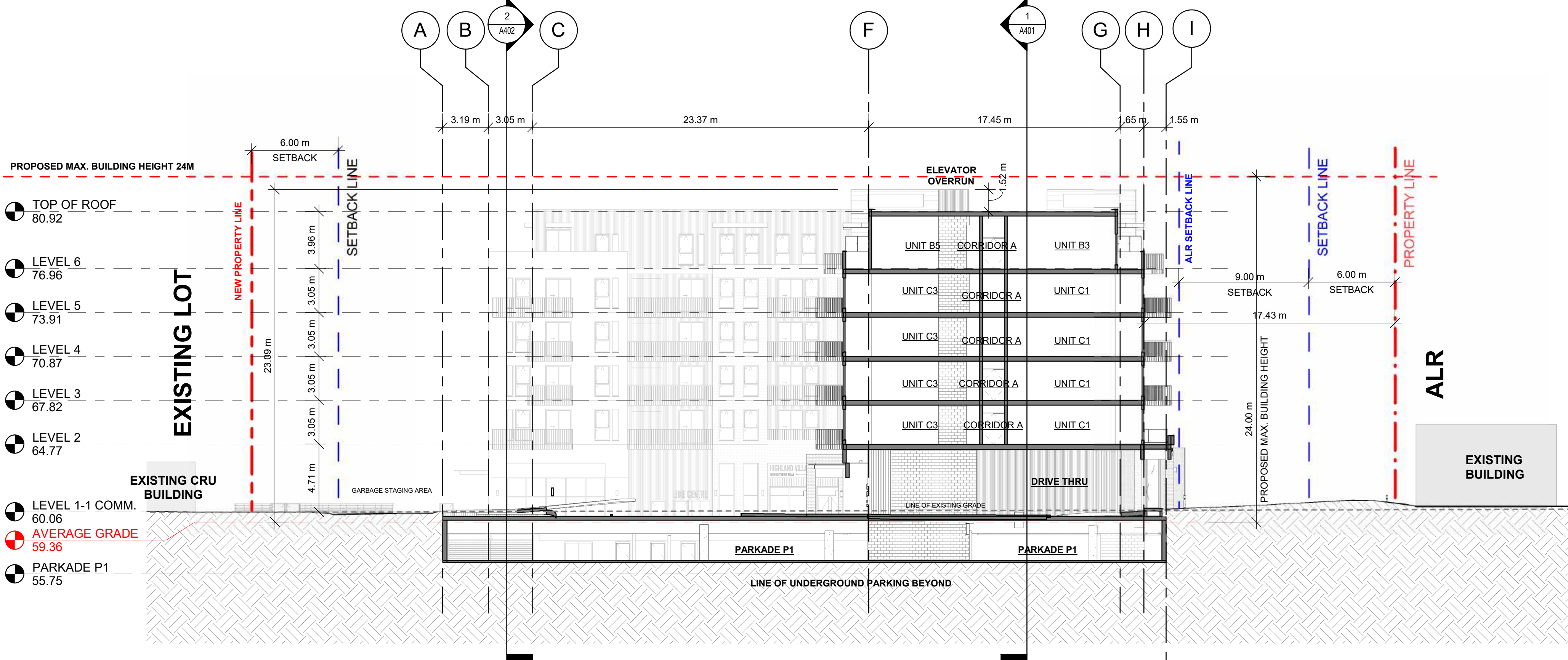
DRAWING TITLE:
BUILDING SECTION

PROJECT NO: 21005 DRAWN BY: WY
 SCALE: 1:200 REVIEW BY: AE

DWG NO: **A401**



1 SECTION 1 - LONGITUDINAL SECTION (N/S)
 SCALE: 1:200



2 SECTION 2 - CROSS SECTION (E/W)
 SCALE: 1:200

ALR - PROTECTION AREA - REMOVE ALL BLACKBERRY AND BROOM AND REPLANT WITH NATIVE DOUGLAS FIR FOREST HABITAT WITH LARGE AND MEDIUM SIZED NATIVE SHRUBS AND HERBACEOUS PLANTS

ALR - PROTECTION AREA - REMOVE ALL BLACKBERRY AND BROOM AND REPLANT WITH MEDIUM SIZED NATIVE SHRUBS AND HERBACEOUS PLANTS

GRAVEL PATH

PLANTINGS NOT IN THE ALR ZONE SUNNY AREAS

PLANTINGS NOT IN THE ALR ZONE SHADE AREAS

ASPHALT

BROOM FINISH CONCRETE

DECORATIVE PAVING TO BE DETERMINED

- PLANTS FOR PLANTERS ABOVE PARKADE - SHADY AREAS
- Sarcococca hookeriana humilis Dwarf Sweet Box
 - Polystichum munitum Sword Fern
 - Helleborus 'Royal Heritage' Mixed Lenten Rose
 - Athyrium filix-femina Lady Fern

ROCK BOULDERS FOR STRUCTURAL ELEMENT IN SHADY DRIVE THROUGH PLANTERS

- PLANTS IN APENITY AREA
- Lavandula angustifolia 'Hickote' English Lavender
 - Rosa David Austin 'Heritage' Fragrant Pale Pink Rose
 - Vaccinium cor. 'Northble' Blueberries
 - Viburnum tinus 'Spring Bouquet' Spring Bouquet Viburnum
 - Viburnum x burk 'Anne Russel' Fragrant White Viburnum

EXISTING TREES TO BE RETAINED

- PLANTS AROUND PARKING AREAS AND STREET SIDE
- STREET TREES
- Cornus betulus 'Frans Fontaine' Pyramidal European Hornbeam
 - Cornus nuttallii 'Eddies White Wonder' Eddies White Wonder Dogwood
 - Fagus sylvatica 'Dauyck Purple' Dauyck Purple Columnar Beech
 - Picea omorika 'Bruns' Brun's Serbian Spruce
 - Pinus mugo 'Pumili' Dwarf Mugo Pine
 - Vaccinium ovatum Evergreen Huckleberry
 - Rosa meidland 'Sevilland' Red Rose
 - Nepeta fassenii 'Walker's Low' Catmint
 - Calamagrostis x Karl Foerster Feather Reed Grass
 - Carex marroaii 'Ice Dance' Ice Dance Sedge

- OTHER PLANTS IN ALR BUFFER
- Oceanspray Holodiscus discolor
 - Saskatoon Berry Amelanchier alnifolia
 - Western Flowering Currant Ribes sanguineum
 - Coastal Mock Orange Philadelphus lewisii 'Gordianus'
 - Pacific Pink Rhododendron Rhododendron macrophyllum
 - Tall Oregon Grape Mahonia aquifolium
 - Evergreen Huckleberry Vaccinium ovatum
 - Red Huckleberry Vaccinium parvifolium
 - Nootka Rose Rosa nutkana
 - Snowberry Symphoricarpos albus
 - Gaultheria shallon Salal
 - Polystichum munitum Sword Fern

- PLANTS FOR BACK 5 ROWS OF ALR SCREENING
- Douglas Fir Pseudotsuga menziesii
 - Big Leaf Maple Acer macrophyllum
 - Bitter Cherry Prunus emarginata

ALR PLANTING BACK 2 ROWS MIX OF THE FIVE PLANTS BELOW

- Oceanspray Holodiscus discolor
- Saskatoon Berry Amelanchier alnifolia
- Western Flowering Currant Ribes sanguineum
- Coastal Mock Orange Philadelphus lewisii 'Gordianus'
- Pacific Pink Rhododendron Rhododendron macrophyllum

ALR PLANTING MIDDLE 3 ROWS MIX OF THE FIVE PLANTS BELOW

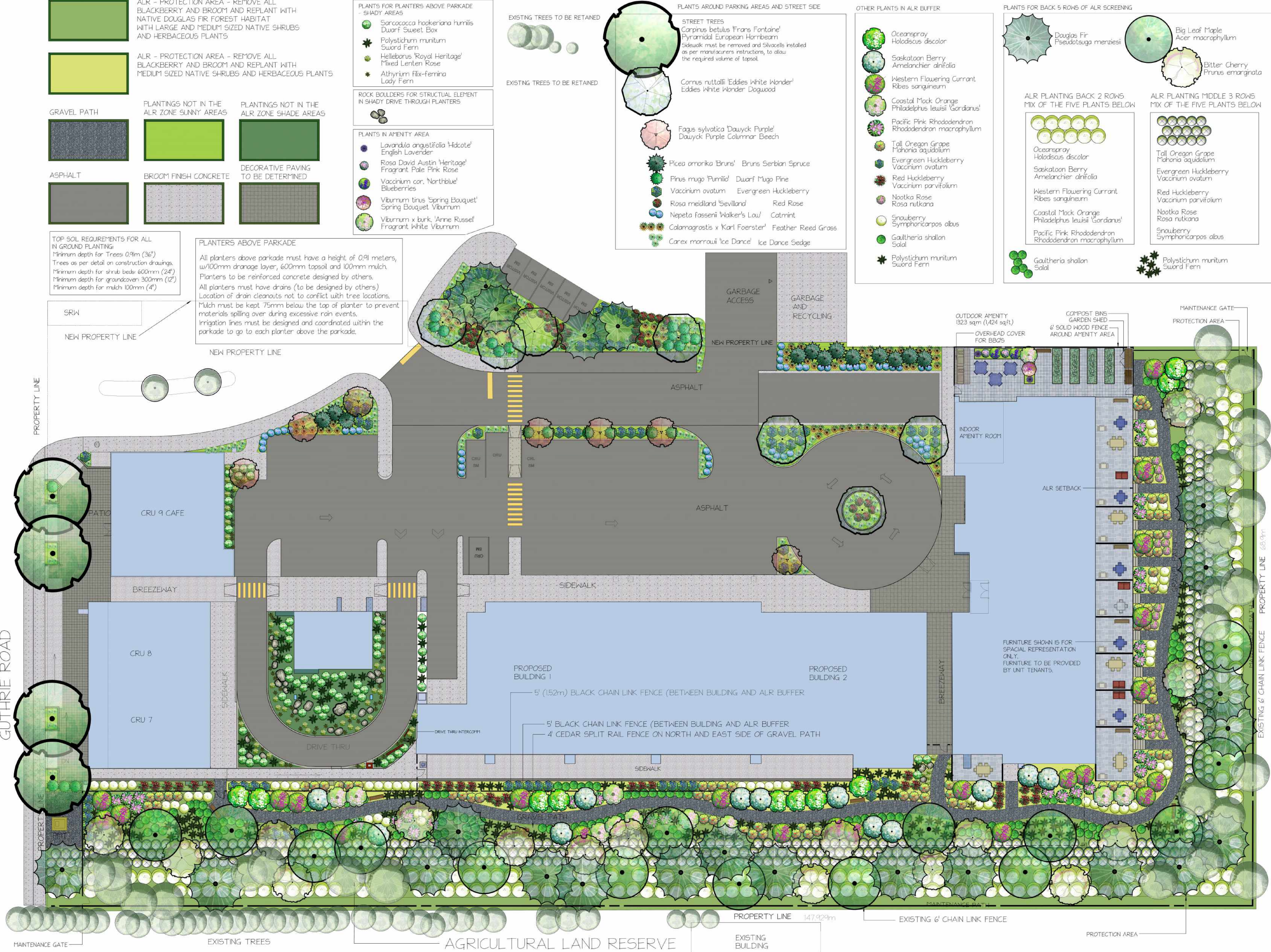
- Tall Oregon Grape Mahonia aquifolium
- Evergreen Huckleberry Vaccinium ovatum
- Red Huckleberry Vaccinium parvifolium
- Nootka Rose Rosa nutkana
- Snowberry Symphoricarpos albus

TOP SOIL REQUIREMENTS FOR ALL IN GROUND PLANTING:

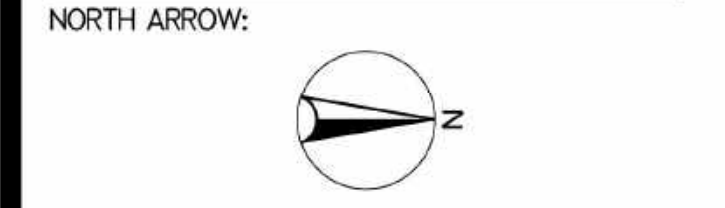
- Minimum depth for Trees: 0.91m (36")
- Trees as per detail on construction drawings.
- Minimum depth for shrub beds: 600mm (24")
- Minimum depth for groundcover: 300mm (12")
- Minimum depth for mulch: 100mm (4")

PLANTERS ABOVE PARKADE

All planters above parkade must have a height of 0.91 meters, w/100mm drainage layer, 600mm topsoil and 100mm mulch. Planters to be reinforced concrete designed by others. All planters must have drains (to be designed by others). Location of drain cleanouts not to conflict with tree locations. Mulch must be kept 75mm below the top of planter to prevent materials spilling over during excessive rain events. Irrigation lines must be designed and coordinated within the parkade to go to each planter above the parkade.



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OWNER/CLIENT:
Avtor Properties Management Corp.
3409-13496 Central Ave
Surrey, BC V3T 0K2
Contact: Chetan Tambar
Tel: 778-955-3576

LANDSCAPE ARCHITECT: LANDSCAPE DESIGNER:
Alison Mewett BCSLA 766 Lazo Road, Comox, BC V9M 3V6
236-255-1255 amewett@gmail.com
Corinne Matheson CLD Mystic Woods Landscape Design
250-286-1327 design@mysticwoods.co

1	Conceptual for review	23/4/10
2	Plan for Public consultation	23/5/4
3	Revised plan for coordination	23/8/2
4	Revised plan for coordination	23/8/24
5	OCP Amendment Rezoning and Development Permit	23/8/29
6	Revised plan for DP	24/2/15
7	Revised plan for DP	24/4/4
8	Revised plan for DP	24/7/19

NO: ISSUE Y/M/D
SEAL:

CONSULTANT:

VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
604.885.3629 | office@iwa-arch.ca | iwa-arch.ca

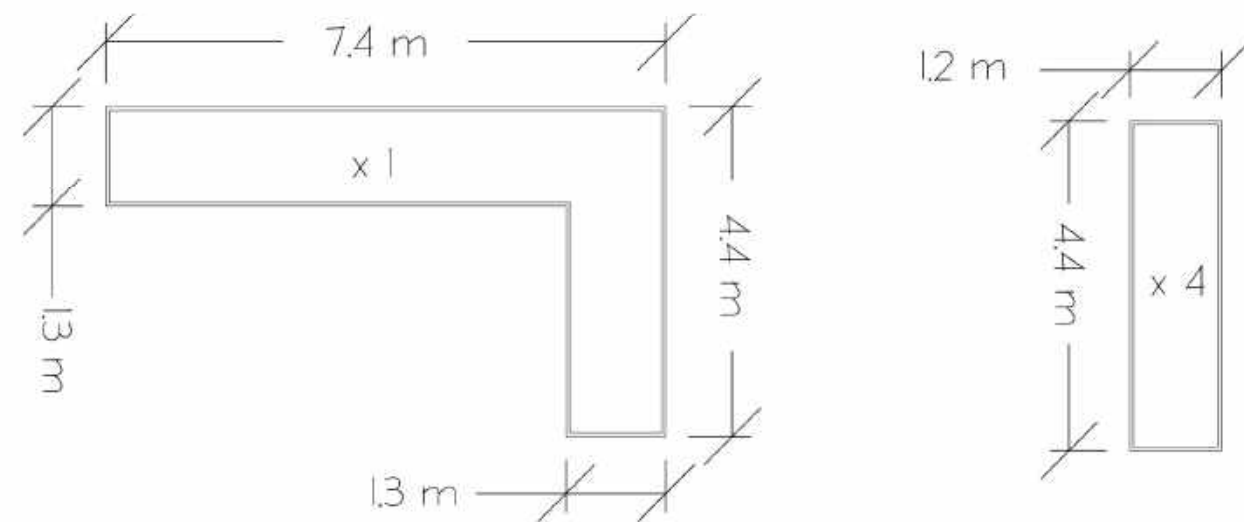
PROJECT NAME:
HIGHLAND VILLAGE

PROJECT ADDRESS:
1966 Guthrie Road,
Comox, BC V9M 3X7

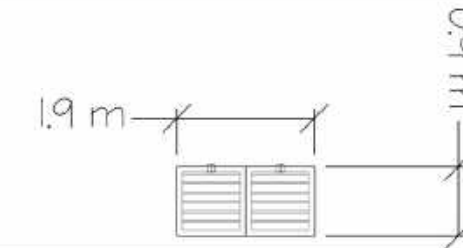
DRAWING TITLE:
MASTER LANDSCAPE PLAN

PROJECT NO: 21005 DRAWN BY: CM
SCALE: 1:200 REVIEW BY: AM
DWG NO: L-01

PLANTERS : WOOD WITH METAL LINER
 4 Vegetable Planters: 12m x 4.4 m x 0.6m high
 1 L-shaped Planter for Ornamentals
 To be built by General contractor



TWO COMPOST BINS
 3' x 3' x 3'



Wishbone Site Furnishings - Mountain Classic Park Bench
 5 benches placed in ALR buffer along gravel trail as shown on L1.
 Model Number : MCB-6
 Total Height 31" Seat Height 17", Seat Depth 18", Total Length 6', Height 160lbs
 Recycled Plastic Slats Colour: Walnut Aluminum Frame Colour: Black Textured
 Supply and Installation by General Contractor Install per manufacturer's instructions
 WISHBONE SITE FURNISHINGS: www.wishbonelltd.com sales@wishbonelltd.com 866-626-0476



5'9" (1.8m) BLACK METAL PICKET FENCE FOR CONTROLLED ACCESS #2 AND #3
 Details provided at Building Permit stage. See L3



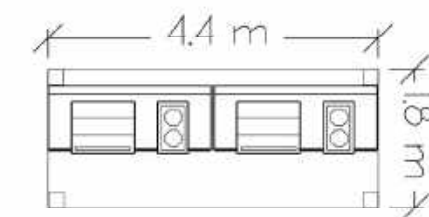
5'9" (1.8m) SOLID WOOD FENCE TO SURROUND THE AMENITY AREA
 Details provided at Building Permit stage. See L3



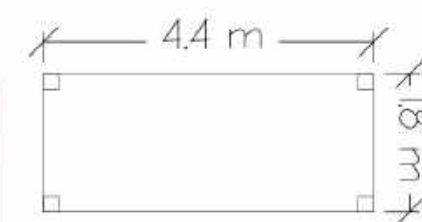
GARDEN SHED FOR TOOLS: 0.9m x 2.4m Details provided at Building Permit stage.



COVERED BBQ AREA CONCEPT
 DETAILS TO BE PROVIDED AT BUILDING PERMIT STAGE



BBQ SHELTER CONCEPT
 DETAILS TO BE PROVIDED AT BUILDING PERMIT STAGE



5' BLACK CHAIN LINK FENCE AS PER PLAN
 BETWEEN BUILDING AND ALR BUFFER. See L3



4' CEDAR SPLIT RAIL FENCE ON NORTH AND EAST SIDE OF GRAVEL PATH
 TO PROTECT ALR PLANTINGS. See L3

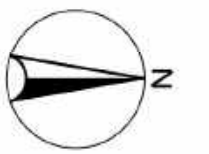


5'9" (1.8m) SOLID WOOD FENCE TO SURROUND THE GARBAGE / RECYCLING AREA
 Details provided at Building Permit stage. See L3



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NORTH ARROW:



OWNER/CLIENT:

Avtar Properties Management Corp.
 3409-13496 Central Ave
 Surrey, B.C. V3T 0K2

Contact: Chetan Tambar
 Tel: 778-955-3576

LANDSCAPE ARCHITECT: LANDSCAPE DESIGNER:

Alison Mewett BCSLA
 766 Lazo Road,
 Comox, BC V9M 3V6

Carrie Matheson CLD
 Mystic Woods
 Landscape Design

236-255-1055
 amewett@gmail.com

250-286-1327
 design@mysticwoods.ca

1	Conceptual for review	23/4/10
2	Plan for Public consultation	23/5/4
3	Revised plan for coordination	23/8/2
4	Revised plan for coordination	23/8/24
5	OCP Amendment Rezoning and Development Permit	23/8/29
6	Revised plan for DP	24/2/14
7	Revised plan for DP	24/4/4
8	Revised plan for DP	24/7/8

NO: ISSUE Y/M/D
 SEAL:

CONSULTANT:

WA ARCHITECTS

VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
 VIC 104 - 3212 Jacklin Road Victoria, V9B 0J5
 804.685.3529 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS:

1966 Guthrie Road,
 Comox, BC V9M 3X7

DRAWING TITLE:

LANDSCAPE IMAGES

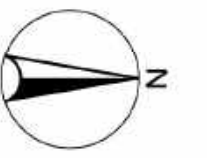
PROJECT NO: 21005 DRAWN BY: CM

SCALE: 1:200 REVIEW BY: AM

DWG NO:

L-06

NORTH ARROW:



OWNER/CLIENT:

Avlar Properties Management Corp.
3409-13496 Central Ave
Surrey, BC V3T 0K2

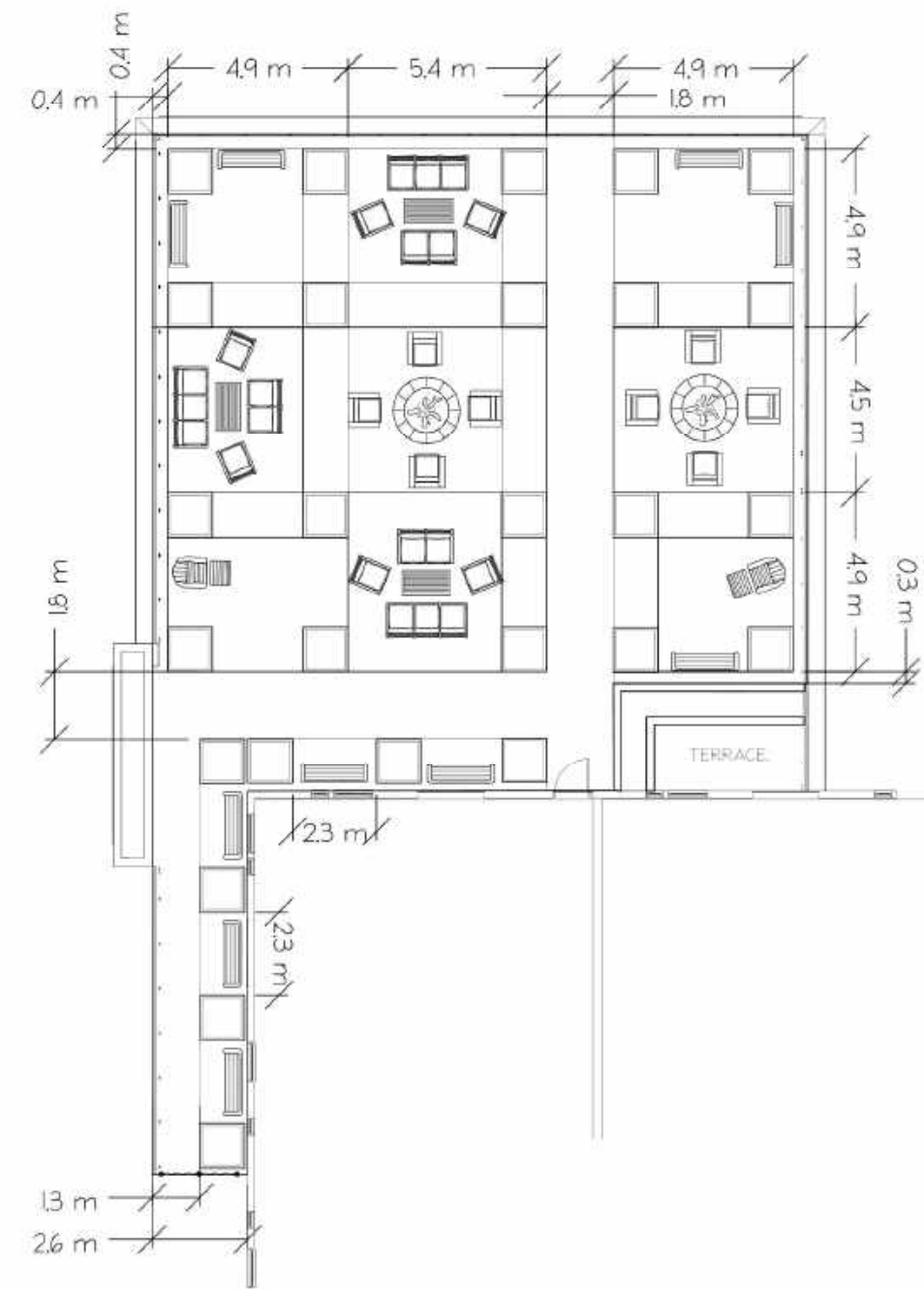
Contact: Chetan Tambar
Tel: 778-955-3576

LANDSCAPE ARCHITECT: LANDSCAPE DESIGNER:

Alison Mewett BCSLA 766 Lazo Road, Comox, BC V9M 3V6
Corinne Matheson CLD Mystic Woods Landscape Design

236-255-1055 amewett@gmail.com
250-286-1327 desigr@mysticwoods.ca

SECOND FLOOR OUTDOOR ROOFTOP AMENITY PATIO



GAS FIRE PIT CONCEPT WITH CHAIRS x 2



PROPOSED BENCH : WISHBONE FURNISHINGS
RUTHERFORD BENCH # RAL-6
COLOUR : BLACK / WALNUT LENGTH 72" X WIDTH 25.5"
SIZE SEAT DEPTH 17", SEAT HEIGHT 17.5" TOTAL HEIGHT 34.5"



PROPOSED PLANTERS FOR SECOND FLOOR WISHBONE FURNISHINGS
RUTHERFORD PLANTER # RPL-47
COLOUR : BLACK / WALNUT SIZE 48" X 48" X 23"
SOIL VOLUME: 20.6 CUFT / 582 L

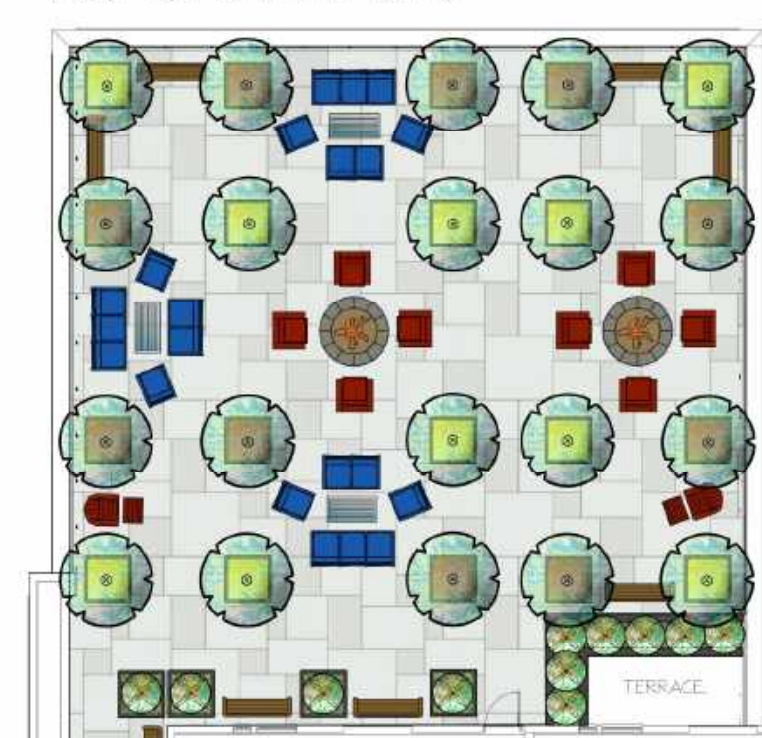


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2	Plan for Public consultation	23/5/14
3	Revised plan for coordination	23/8/2
4	Revised plan for coordination	23/8/24
5	OCP Amendment Rezoning and Development Permit	23/8/29
6	Revised plan for DP	24/2/15
7	Revised plan for DP	24/4/4
8	Revised plan for DP	24/7/8

NO: ISSUE Y/M/D

SEAL:

SECOND FLOOR OUTDOOR ROOFTOP AMENITY PATIO



PATIO SURFACE TO BE DETERMINED



Drip irrigation solid lines to be installed below patio surface and run into each planter. Design of irrigation by others at Building permit stage. Provide a hosebib on the building wall close to door to provide additional water if needed. Drains and clean out to be installed in corner of each planter. Design of drainage by others at Building Permit stage.

LARGE EVERGREEN SHRUB FOR PLANTERS

- Arbutus unedo 'Compacta' Evergreen Dwarf Strawberry Tree
- Pieris japonica 'Forest Flame' Forest Flame Pieris

OUTDOOR FURNITURE TO BE DETERMINED x 3

FLOWERING PERENNIALS

- Hemerocallis x 'Pardon Me' Dwarf Red Daylily
- Hemerocallis x 'Stella de Oro' Dwarf Yellow Daylily
- Ajuga reptans 'Catlin's Giant' Catlin's Giant Carpet Bugle

ADIRONDACK STYLE CHAIR TO BE DETERMINED x 2

Qty	Botanical Name	Common Name	Size/Condition
Shrubs			
20	Arbutus unedo 'Compacta'	COMPACT STRAWBERRY BUSH	# 5 pot
14	Pieris japonica 'Forest Flame'	FOREST FLAME PIERIS	# 5 pot
Perennials			
40	Hemerocallis x 'Pardon Me'	PARDON ME MINATURE/DWARF DAYLILY	# 1 pot
40	Hemerocallis x 'Stella de Oro'	STELLA DE ORO DWARF DAYLILY	# 1 pot
Groundcovers			
45	Ajuga reptans 'Catlin's Giant'	CATLINS GIANT CARPET BUGLE	# 1 pot

INDOOR AMENITY SPACE
RESIDENTIAL UNIT C1

CONSULTANT:



VAN 950 - 1500 W. Georgia Street Vancouver, V6G 2Z6
VIC 104 - 3212 Jacklin Road Victoria, V8B 0J5
604.895.3629 | office@wa-arch.ca | wa-arch.ca

PROJECT NAME:

HIGHLAND VILLAGE

PROJECT ADDRESS

1966 Guthrie Road, Comox, BC V9M 3X7

DRAWING TITLE:

ROOFTOP AMENITY

PROJECT NO: 21005 DRAWN BY: CM
SCALE: 1:200 REVIEW BY: AM

DWG NO: L-07

HIGHLAND VILLAGE

PROJECT # 23-026

PROPERTY ADDRESS & LEGAL DESCRIPTION

- 1966 GUTHRIE ROAD, COMOX, BC
- LOT 1, SECTION 77, COMOX DISTRICT PLAN VIP 72240

TABLE OF CONTENTS	
C100	OVERALL SITE SERVICING PLAN
C101	SITE SERVICING PLAN
C200	SITE GRADING PLAN
C300	PRE-DEVELOPMENT PLAN
C301	POST-DEVELOPMENT PLAN
C400	STANDARD DETAILS
C401	STANDARD DETAILS

CIVIL ENGINEERING DRAWINGS



GRADE CONSULTING INC.
E. MSHANKARUK@GRADE.CA
P. 780-920-0665

#201A, 10639 - 124 STREET
EDMONTON, AB
T5N 1S5

LEGEND:	PROPOSED	EXISTING
STORM LINE		
STORM MH-CBMH-CB		
SANITARY LINE		
SANITARY MH		
WATER LINE		
VALVE		
CROSS-TEE-90		
45-22.5-11.25		
NATURAL GAS LINE		
OVERHEAD POWER		
UNDERGROUND POWER		
GAS METER		
WATER METER		
FIRE HYDRANT		
SMS		
OVERLAND FLOW		
MAJOR STORM EVENT FLOW DIRECTION		
ELEVATION MARKER		
SUBDIVISION ELEVATION MARKER		

ABBREVIATIONS

AD	AREA DRAIN	GM	GAS METER
ABD	ABANDONED	GND	GROUND
BOS	BOTTOM OF SWALE	GUT	GUTTER
BOW	BACK OF WALK	LOG	LIP OF GUTTER
BRW	BOTTOM OF RETAINING WALL	INV	INVERT
CB	CATCH BASIN	OBV	OBVERT
CBMH	CATCH BASIN MANHOLE	PL	PROPERTY LINE
CSP	CORRUGATED STEEL PIPE	RD	ROOF DRAIN
DMH	DRAIN MANHOLE (STORM)	SMH	SANITARY MANHOLE
EL	EASEMENT LINE	SMS	SIAMSESE CONNECTION
EX	EXISTING	STZ	STORM CONNECTION
FFE	FINISHED FLOOR ELEVATION	TOA	TOP OF ASPHALT
FH	FIRE HYDRANT	TOB	TOP OF BERM
FLG	FLANGE ELEVATION	TOC	TOP OF CURB
FOC	FACE OF CURB	TRW	TOP OF RETAINING WALL
FOW	FRONT OF WALK	WM	WATER METER
		XING	CROSSING

STAMP



EGBC PERMIT#: 1003932

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N
KEY PLAN
SCALE: N.T.S.

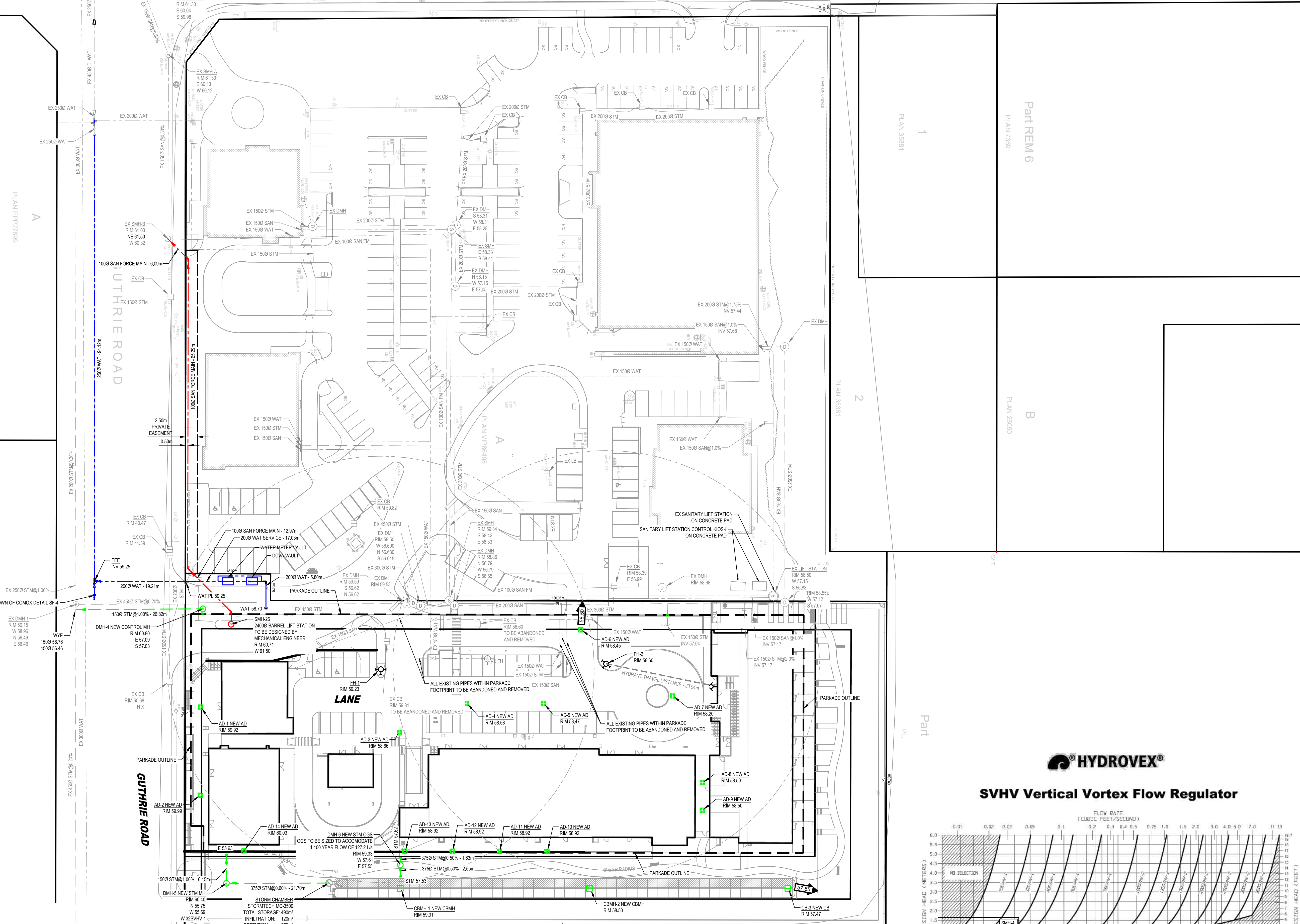
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3	ISSUED FOR SEPARATE LOT SERVICING	2024.05.30
2	RE-ISSUED FOR DP	2024.04.08
1	ISSUED FOR DP/ RZ/ DW/ OCP AMENDMENT	2023.08.28
NO.	REVISION	YYYY.MM.DD

DRAWN: HB DESIGNED: AR REVIEWED: AR

PROJECT NAME: HIGHLAND VILLAGE
PROJECT LOCATION: 1966 GUTHRIE ROAD, COMOX, BC
LOTS 1, SECTION 77, COMOX DISTRICT PLAN VIP 72240

DRAWING NAME: COVER
DRAWING NO.: C000
PROJECT NO.: 23-026

ANDERTON ROAD



LEGEND:

	PROPOSED	EXISTING
STORM LINE		
STORM MH-CBMH-CB		
SANITARY LINE		
SANITARY MH		
WATER LINE		
VALVE		
CROSS-TEE-90		
45.25-11.25		
NATURAL GAS LINE		
OVERHEAD POWER		
UNDERGROUND POWER		
GAS METER		
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FIRE HYDRANT		
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OVERLAND FLOW		
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ELEVATION MARKER		
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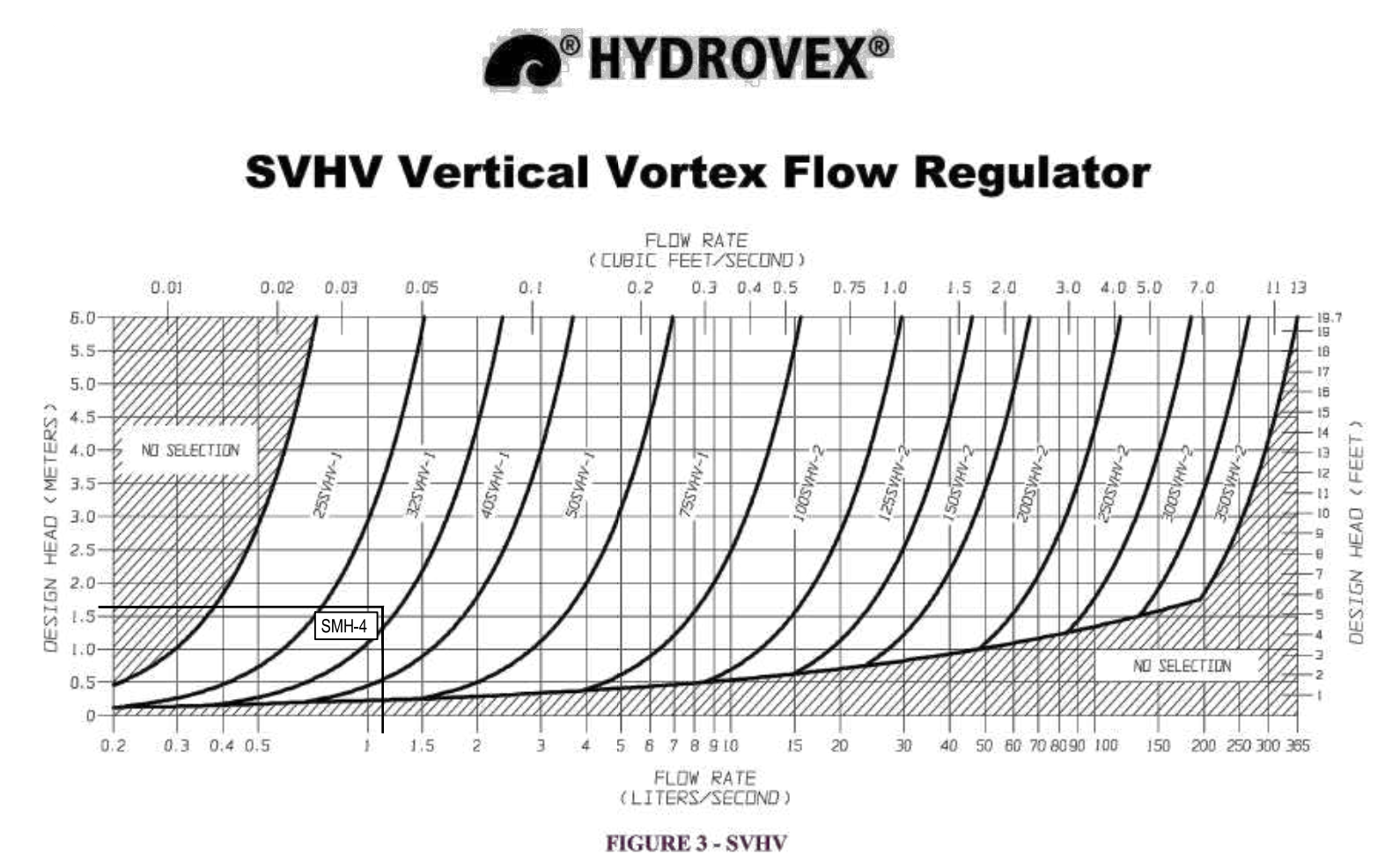
ABBREVIATIONS

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ABD	ABANDONED	GND	GROUND
BOS	BOTTOM OF SWALE	GUT	GUTTER
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FOC	FACE OF CURB	TRW	TOP OF RETAINING WALL
FOW	FRONT OF WALK	WM	WATER METER
		XNG	CROSSING

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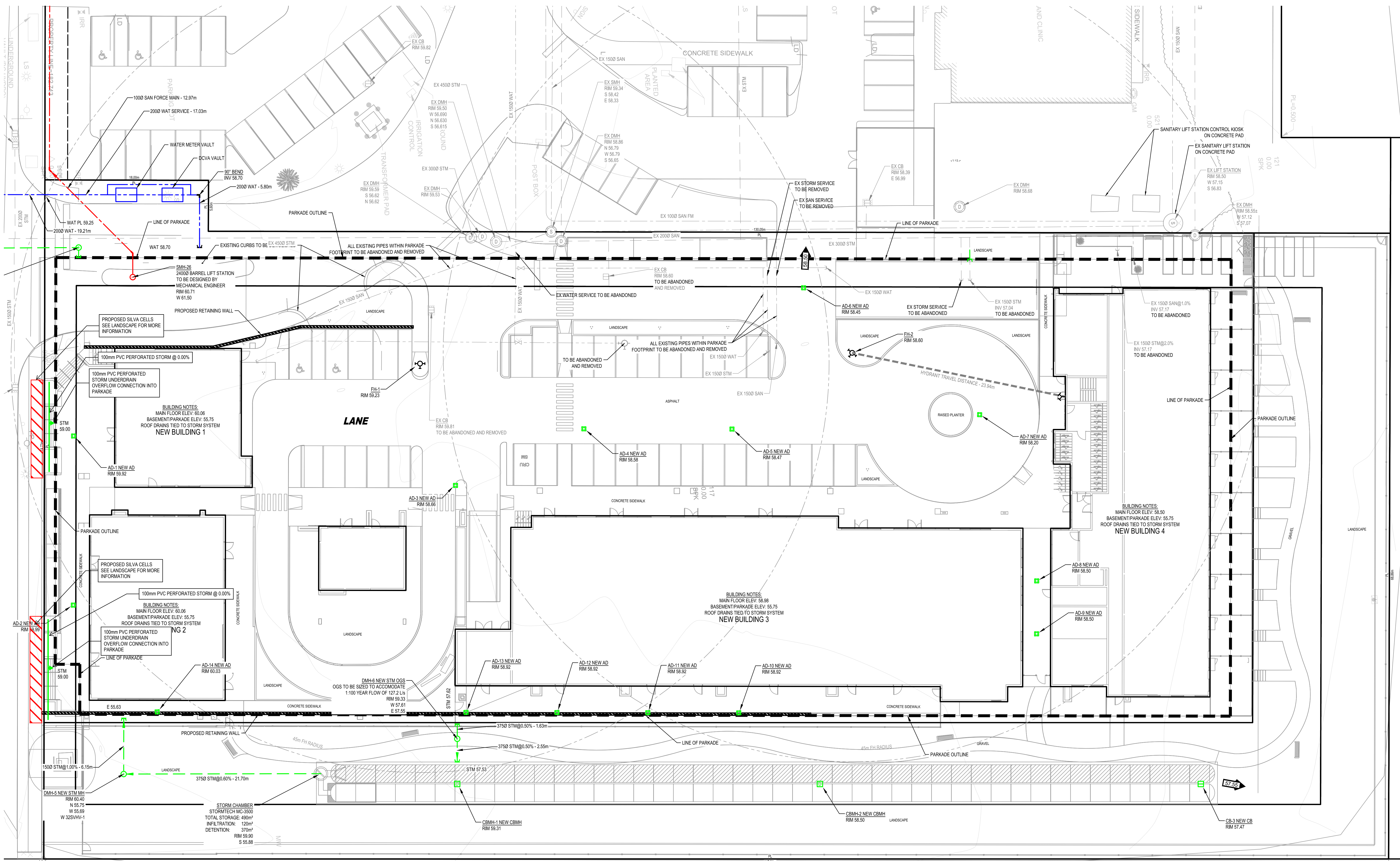
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OVERALL SITE SERVICING PLAN
SCALE: 1:400

PLAT DATE: 7/12/2024 2:14:47 PM

4	RE-ISSUED FOR DP/ RZ/ DV/ OCP AMENDMENT	2024.07.12			
3	ISSUED FOR SEPARATE LOT SERVICING	2024.05.30			
2	RE-ISSUED FOR DP	2024.04.08			
1	ISSUED FOR DP/ RZ/ DV/ OCP AMENDMENT	2023.08.28			
NO.	REVISION	YYYY.MM.DD			
DRAWN:	HB	DESIGNED:	AR	REVIEWED:	AR
PROJECT NAME:	HIGHLAND VILLAGE		PROJECT LOCATION:	1966 GUTHRIE ROAD COMOX, BC	
DRAWING NAME:	OVERALL SITE SERVICING PLAN		DRAWING NO.:	C100	
			PROJECT NO.:	23-028	



LEGEND:

	PROPOSED	EXISTING
STORM LINE		
STORM MHCMBH-CB		
SANITARY LINE		
SANITARY MH		
WATER LINE		
VALVE		
CROSS-TEE-90		
45.25-11.25		
NATURAL GAS LINE		
OVERHEAD POWER		
UNDERGROUND POWER		
GAS METER		
WATER METER		
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ELEVATION MARKER		
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EX	EXISTING	STZ	STORZ CONNECTION
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		XING	CROSSING

STAMP

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4	RE-ISSUED FOR DP/ RZ/ DV/ OCP AMENDMENT	2024.07.12
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NO.	REVISION	YYYY.MM.DD

DRAWN: HB DESIGNED: AR REVIEWED: AR

PROJECT NAME: HIGHLAND VILLAGE
PROJECT LOCATION: 1666 GUTHRIE ROAD COMOX, BC
LOTS 1, SECTION 77, COMOX DISTRICT PLAN WP 72240

DRAWING NAME: SITE SERVICING PLAN
DRAWING NO. C101
PROJECT NO.: 23-028



NOTE: AREA DRAINS TO BE INCORPORATED INTO THE PARKADE STRUCTURE AND COORDINATED WITH THE MECHANICAL DESIGN AT BP STAGE

LEGEND:	PROPOSED	EXISTING
STORM LINE		
STORM MH-CBMH-CB		
SANITARY LINE		
SANITARY MH		
WATER LINE		
VALVE		
CROSS-TEE-90		
45-22.5-11.25		
NATURAL GAS LINE		
OVERHEAD POWER		
UNDERGROUND POWER		
GAS METER		
WATER METER		
FIRE HYDRANT		
SMS		
OVERLAND FLOW		
MAJOR STORM EVENT		
FLOW DIRECTION		
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CBMH	CATCH BASIN MANHOLE	PL	PROPERTY LINE
CSP	CORRUGATED STEEL PIPE	RD	ROAD DRAIN
DMH	DRAIN MANHOLE (STORM)	SMH	SANITARY MANHOLE
EL	EASEMENT LINE	SMS	SIAMENSE CONNECTION
EX	EXISTING	STZ	STORM CONNECTION
FFE	FINISHED FLOOR ELEVATION	TOA	TOP OF ASPHALT
FH	FIRE HYDRANT	TOT	TOP OF TIE
FLG	FLANGE ELEVATION	TOC	TOP OF CURB
FOC	FACE OF CURB	TRW	TOP OF RETAINING WALL
FOW	FRONT OF WALK	WM	WATER METER
		XING	CROSSING

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EGBC PERMIT#: 1003932

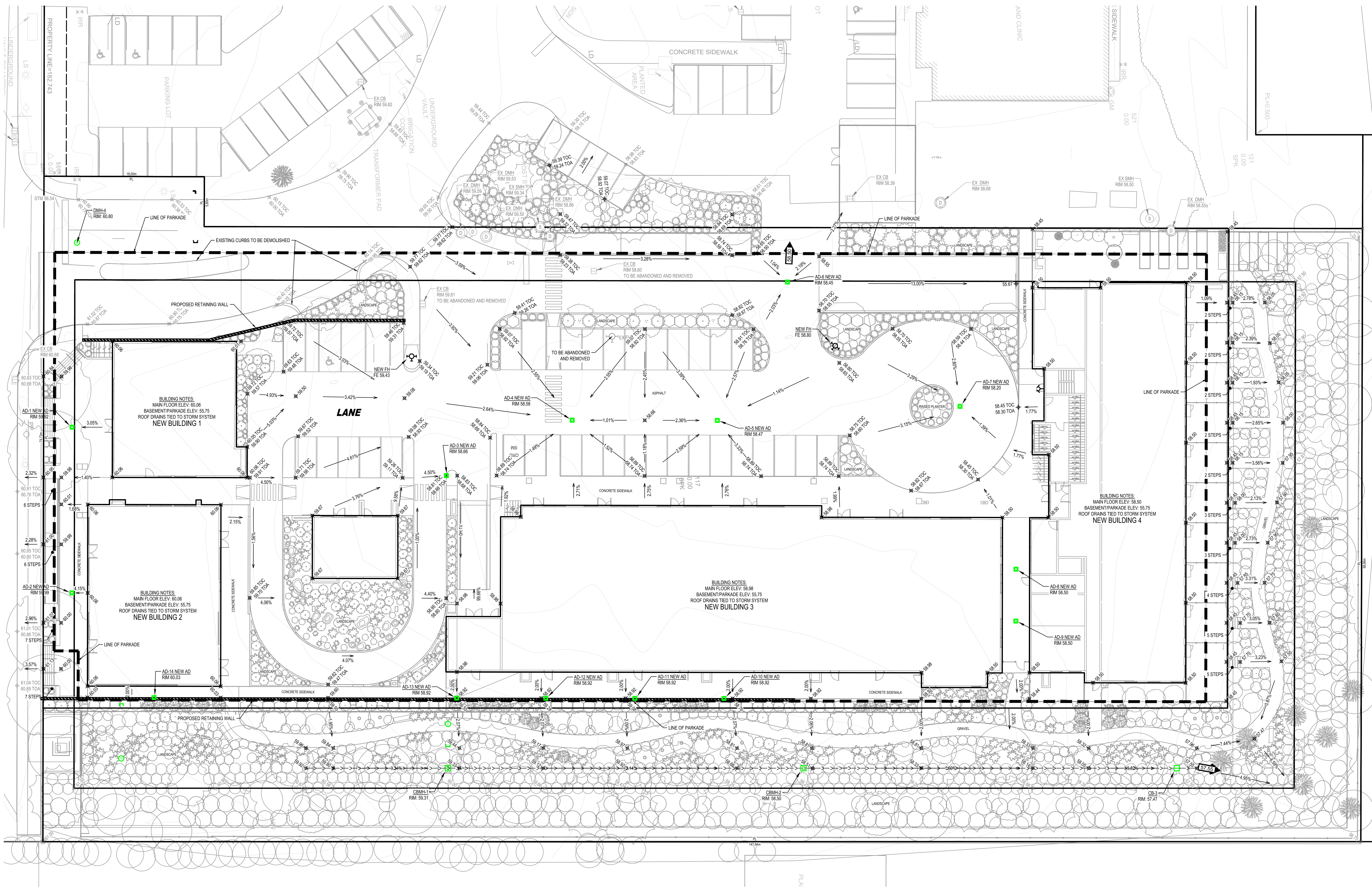
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NO.	REVISION	YYYY.MM.DD

DRAWN: HB DESIGNED: AR REVIEWED: AR

PROJECT NAME: HIGHLAND VILLAGE
PROJECT LOCATION: 1966 GUTHRIE ROAD, COMOX, BC
LOTS 1, SECTION 77, COMOX DISTRICT, PLAN VP 72240

DRAWING NAME: SITE GRADING PLAN
DRAWING NO.: C200
PROJECT NO.: 23-028



NOTE: AREA DRAINS TO BE INCORPORATED INTO THE PARKADE STRUCTURE AND COORDINATED WITH THE MECHANICAL DESIGN AT BP STAGE.

NOTE: BARRIER CURBS TO BE USED ON SITE UNLESS OTHERWISE NOTED. CURBS ARE IN COMPLIANCE WITH THE ZONING BYLAW.

SITE GRADING PLAN
SCALE: 1:200

LEGEND:

	PROPOSED	EXISTING
STORM LINE		
STORM MH-CBMH-CB		
SANITARY LINE		
SANITARY MH		
WATER LINE		
VALVE		
CROSS-TEE-90		
45-22.5-11.25		
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WATER METER		
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FOW	FRONT OF WALK	WM	WATER METER
		XMG	CROSSING

STAMP



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1	ISSUED FOR DP/ RZ/ DW/ OCP AMENDMENT	2023.08.28

DRAWN: HB DESIGNED: AR REVIEWED: AR

PROJECT NAME: HIGHLAND VILLAGE

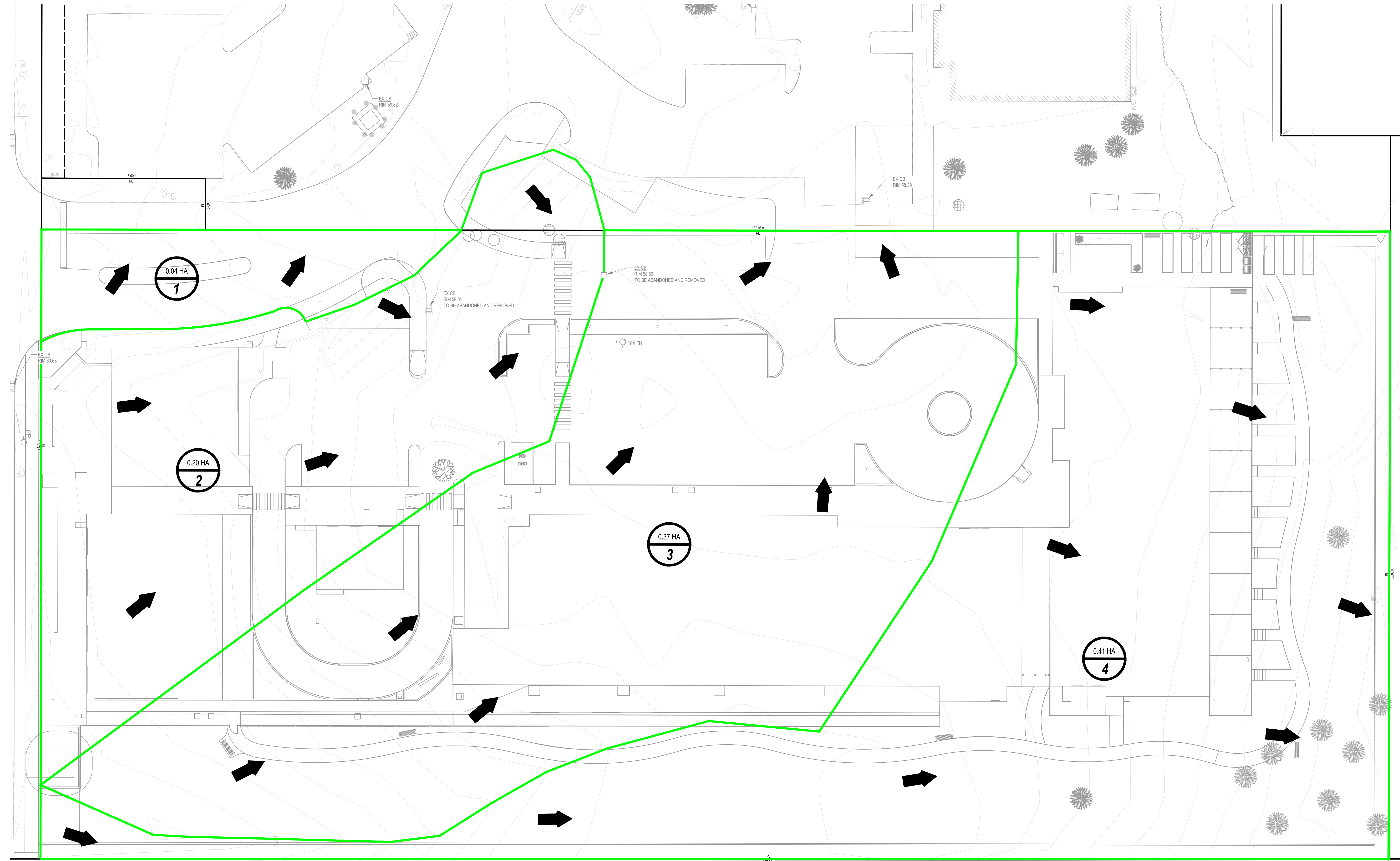
PROJECT LOCATION: 1966 GUTHRIE ROAD, COMOX, BC

LOTS 1, SECTION 77, COMOX DISTRICT, PLAN VP 72240

DRAWING NAME: PRE-DEVELOPMENT PLAN

DRAWING NO. C300

PROJECT NO.: 23-026



PRE-DEVELOPMENT PLAN

SCALE: 1:200

LEGEND:

	PROPOSED	EXISTING
STORM LINE		
STORM MH-CBMH-CB		
SANITARY LINE		
SANITARY MH		
WATER LINE		
VALVE		
CROSS-TEE-90		
45-22.5-11.25		
NATURAL GAS LINE		
OVERHEAD POWER		
UNDERGROUND POWER		
GAS METER		
WATER METER		
FIRE HYDRANT		
SMS		
OVERLAND FLOW		
MAJOR STORM EVENT		
FLOW DIRECTION		
ELEVATION MARKER		
SUBDIVISION		
ELEVATION MARKER		

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CBMH	CATCH BASIN MANHOLE	PL	PROPERTY LINE
CSP	CORRUGATED STEEL PIPE	RD	ROOF DRAIN
DMH	DRAIN MANHOLE (STORM)	SMH	SANITARY MANHOLE
EL	EASEMENT LINE	SMS	SIAMSE CONNECTION
EX	EXISTING	STZ	STOZ CONNECTION
FFE	FINISHED FLOOR ELEVATION	TOA	TOP OF ASPHALT
FH	FIRE HYDRANT	TOB	TOP OF BERM
FLG	FLANGE ELEVATION	TOC	TOP OF CURB
FOC	FACE OF CURB	TRW	TOP OF RETAINING WALL
FOW	FRONT OF WALK	WM	WATER METER
		XING	CROSSING

STAMP



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	NO. REVISION	YYYY.MM.DD

DRAWN: HB DESIGNED: AR REVIEWED: AR

PROJECT NAME: HIGHLAND VILLAGE

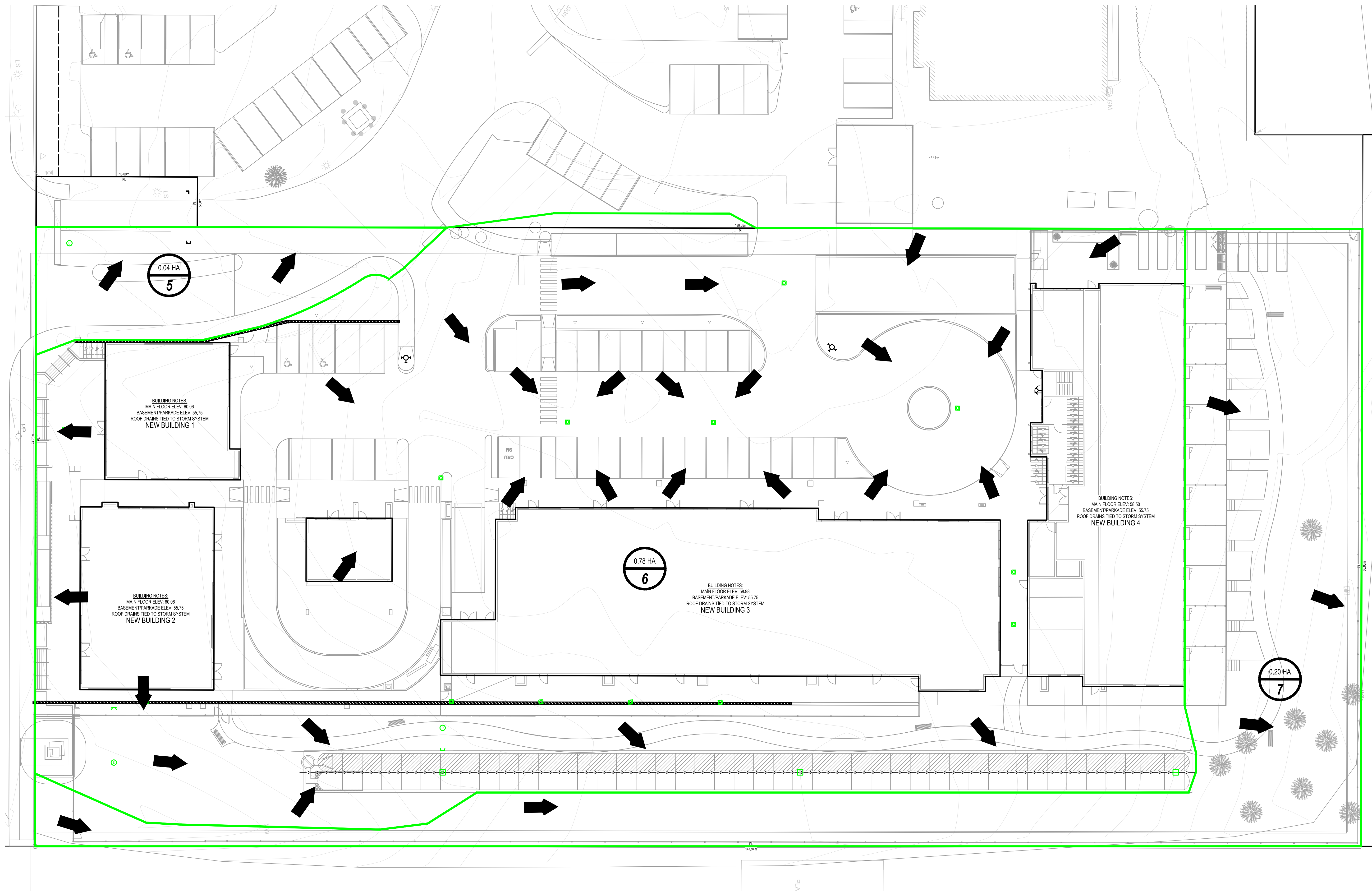
PROJECT LOCATION: 1966 GUTHRIE ROAD, COMOX, BC

LOTS 1, SECTION 77, COMOX DISTRICT, PLAN VP 72240

DRAWING NAME: POST-DEVELOPMENT PLAN

DRAWING NO: C301

PROJECT NO.: 23-028



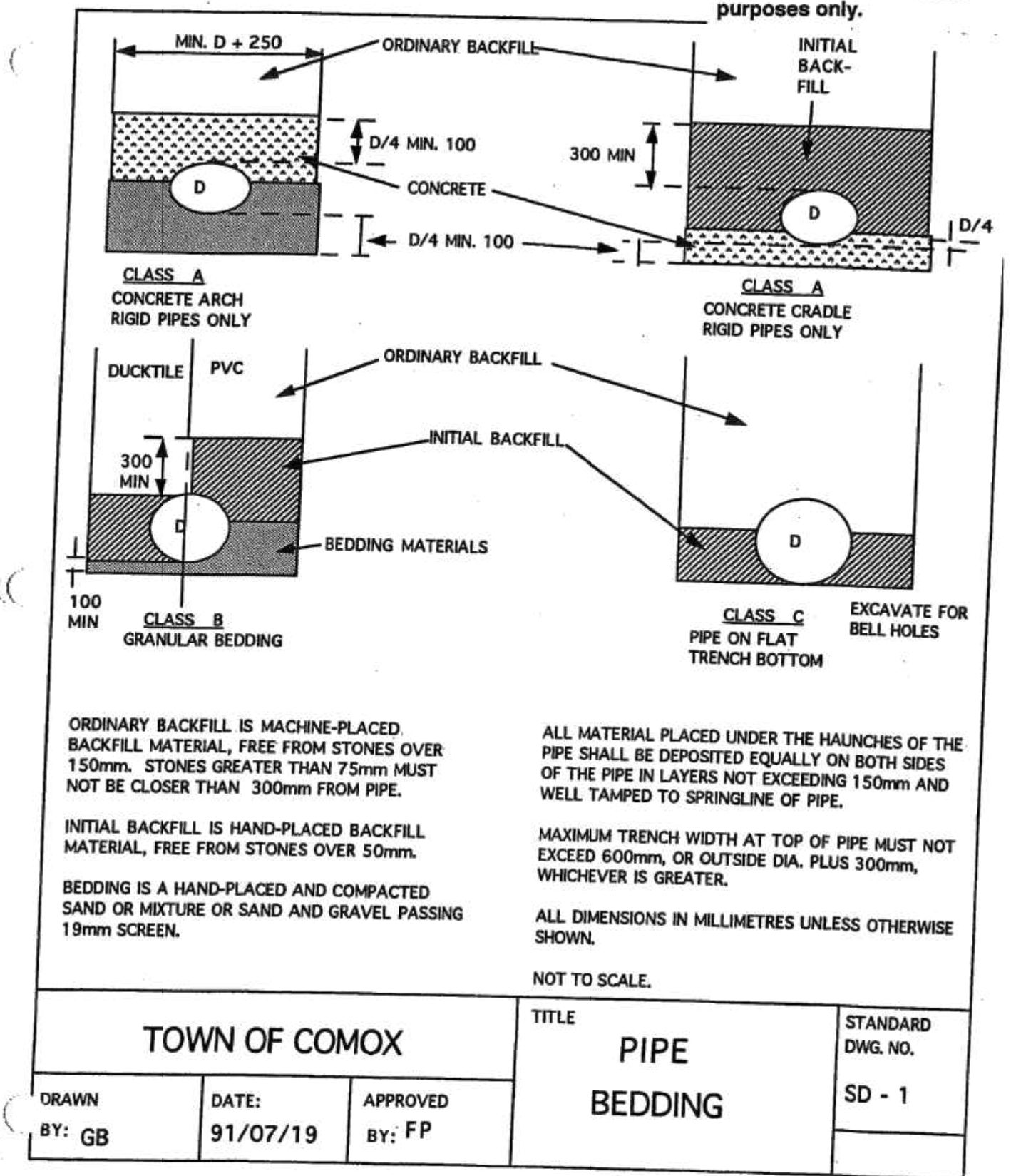
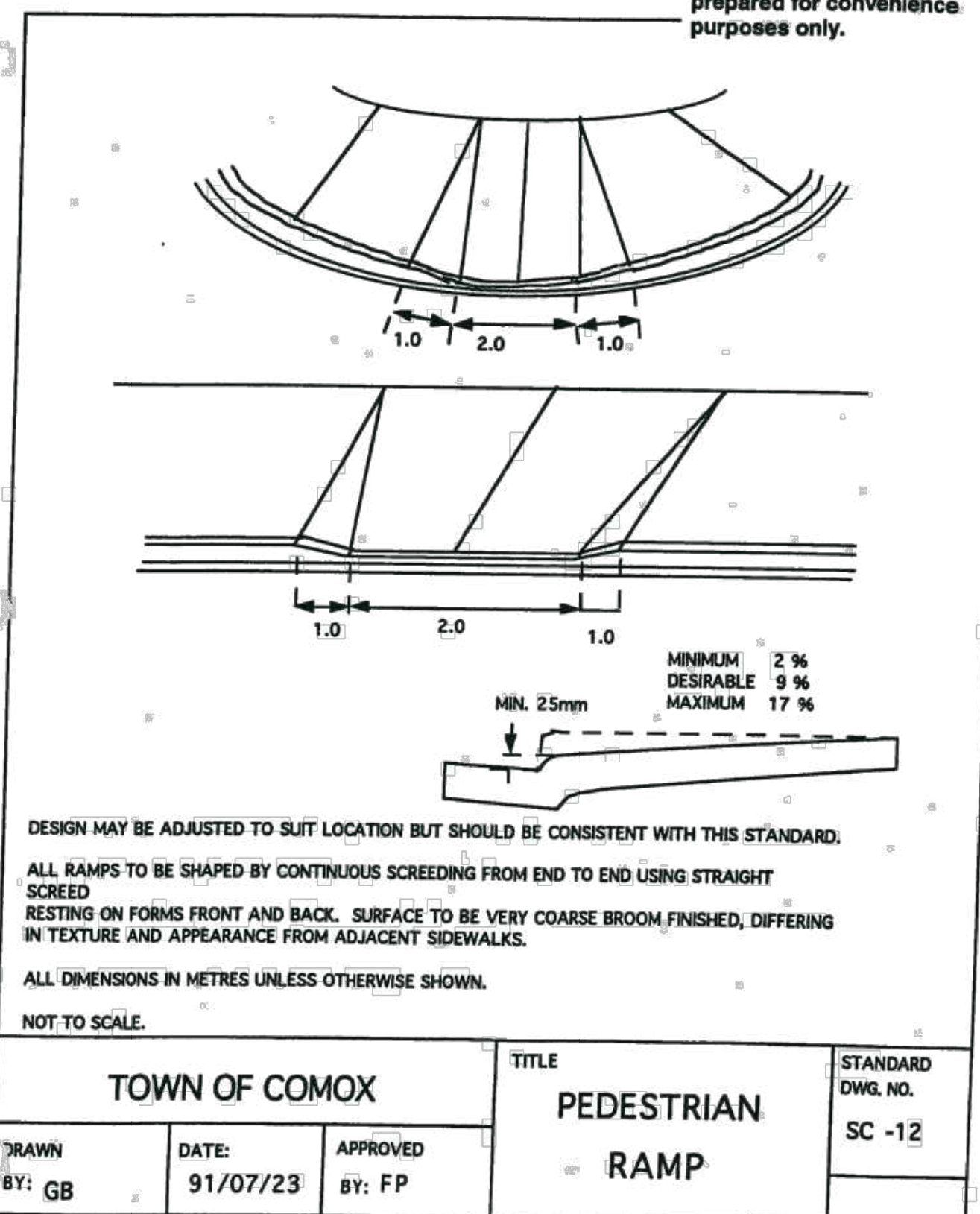
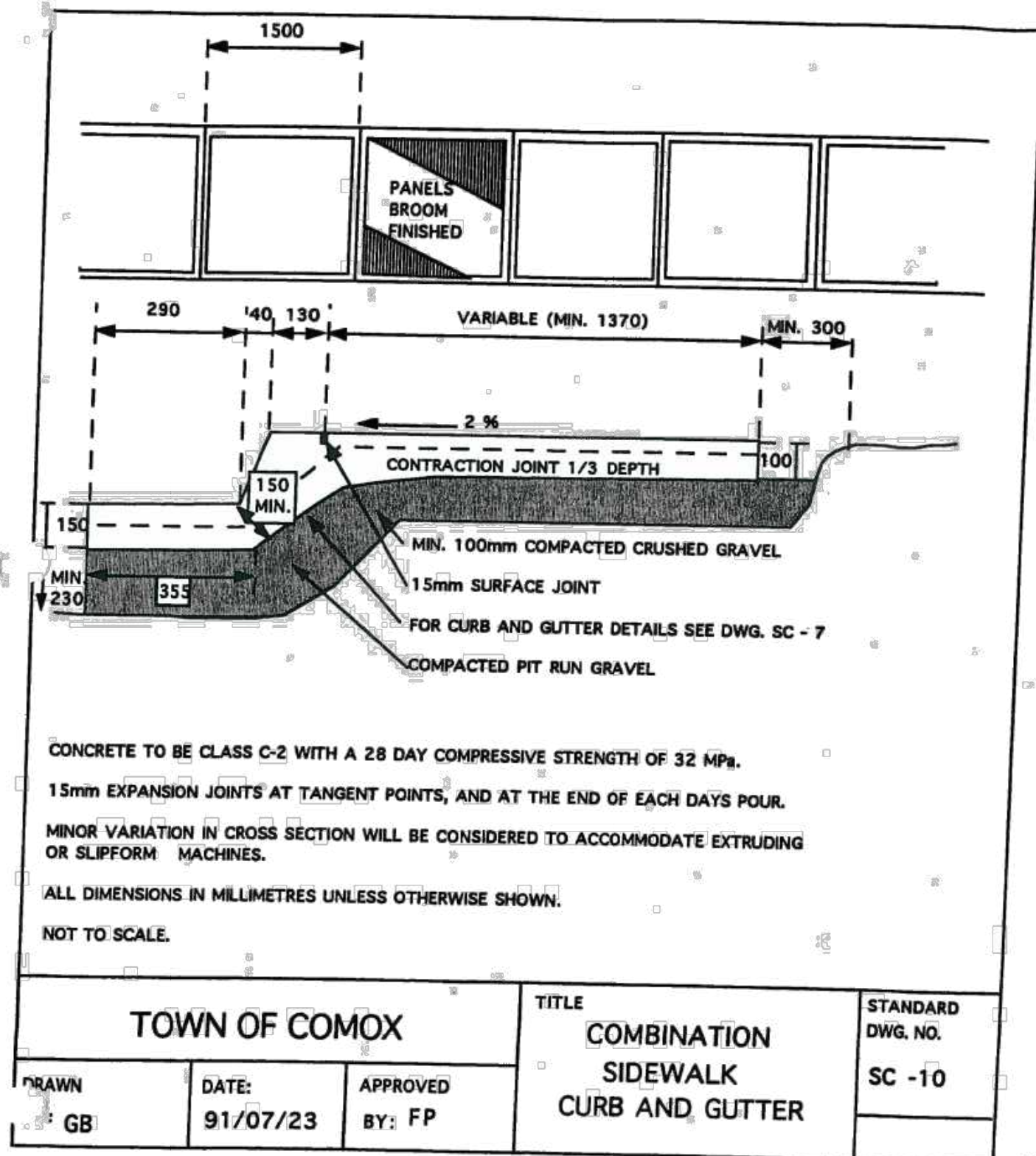
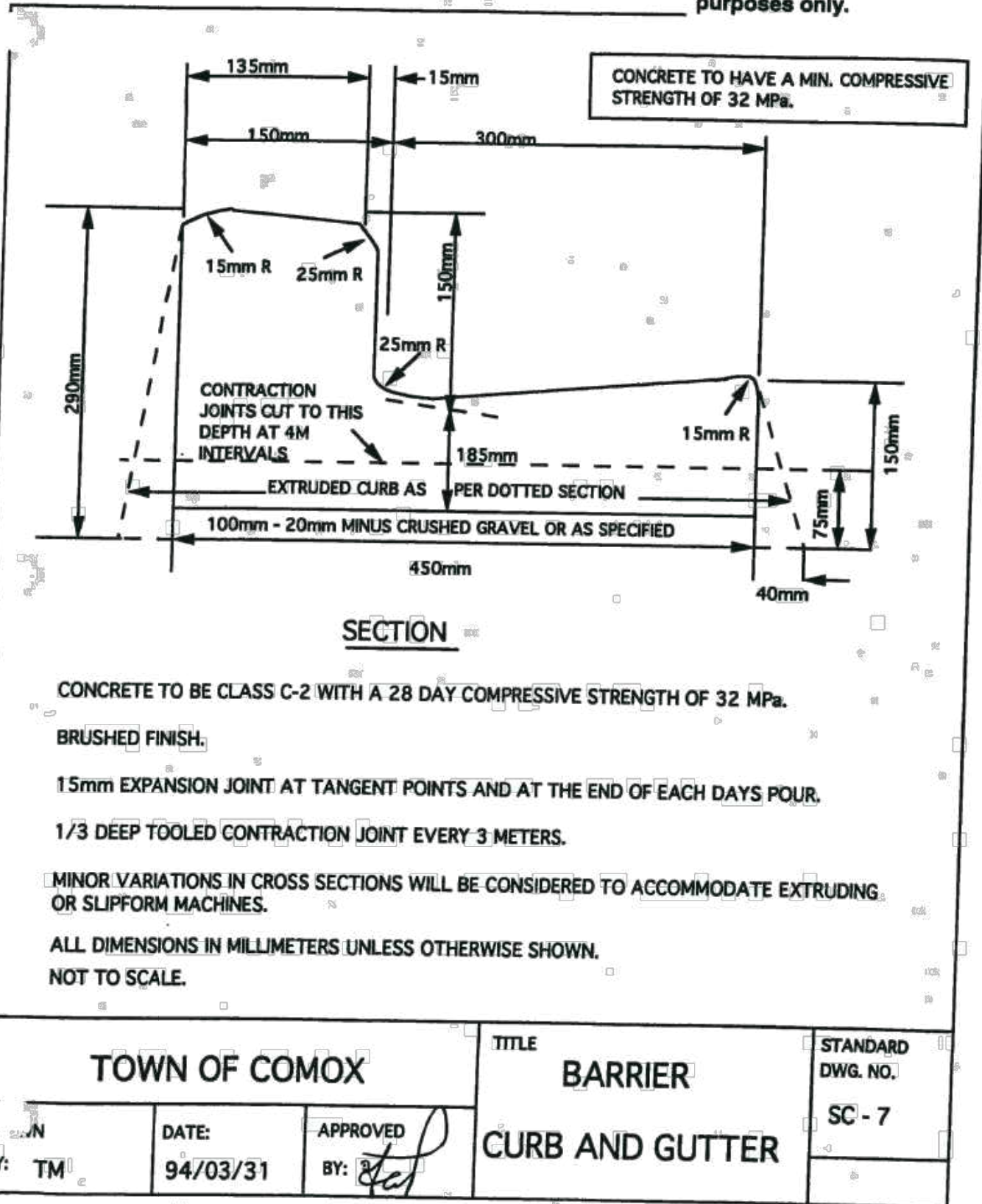
POST-DEVELOPMENT PLAN
SCALE: 1:200

This is a consolidated version prepared for convenience purposes only.

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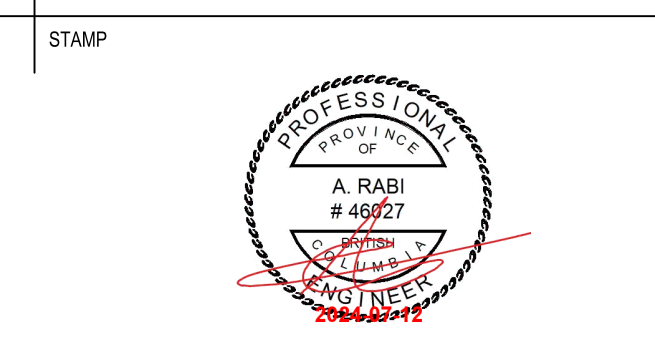
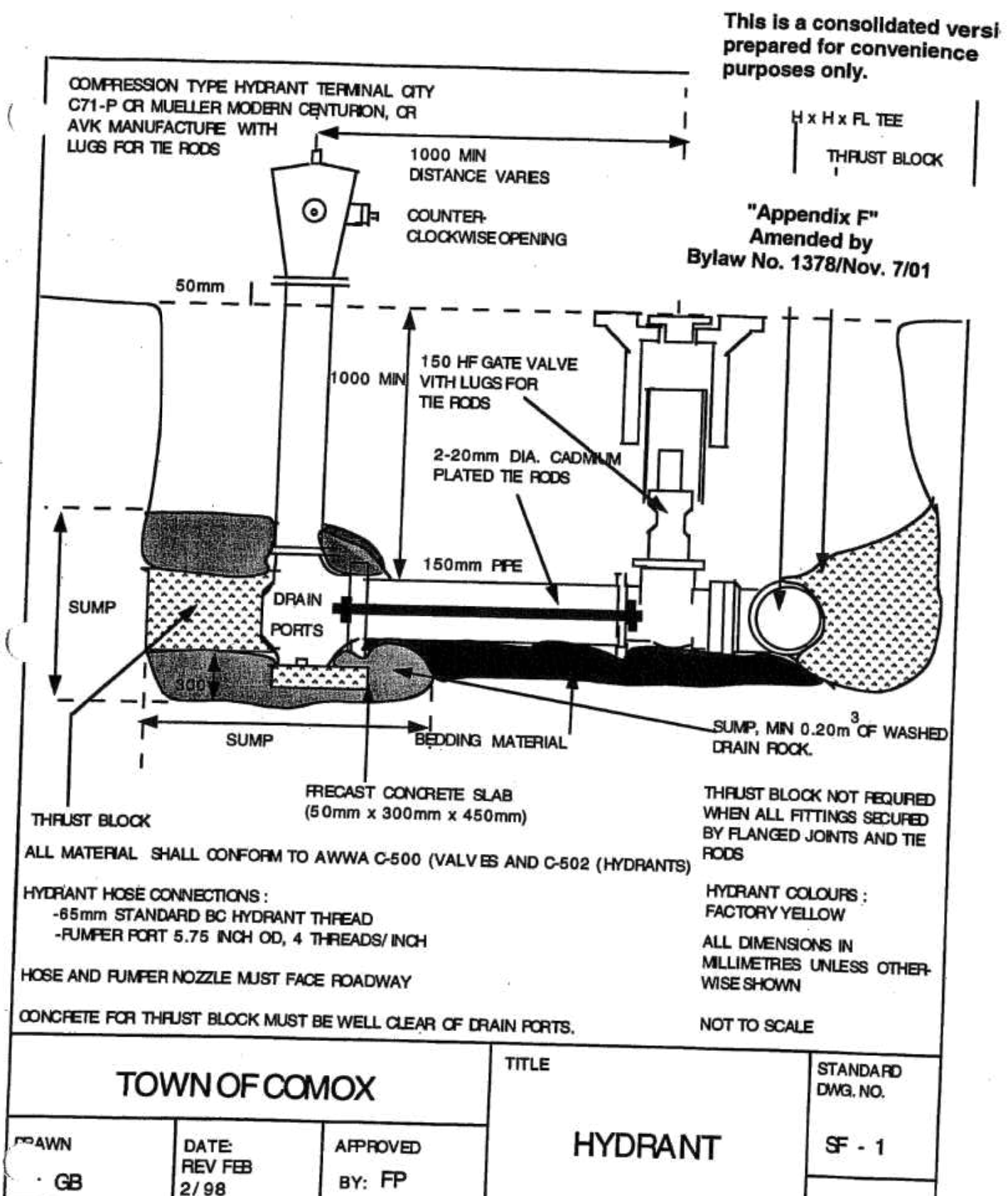
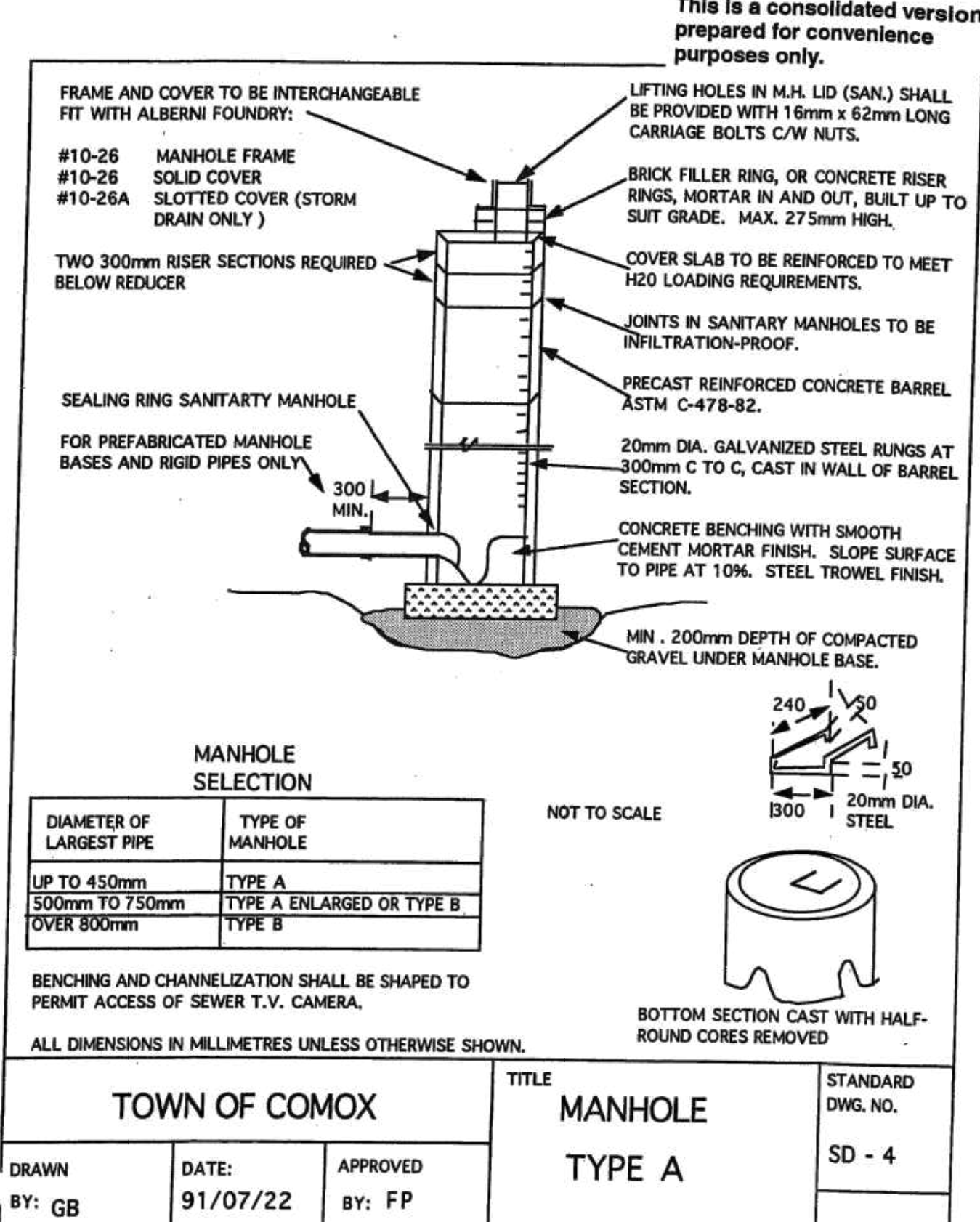
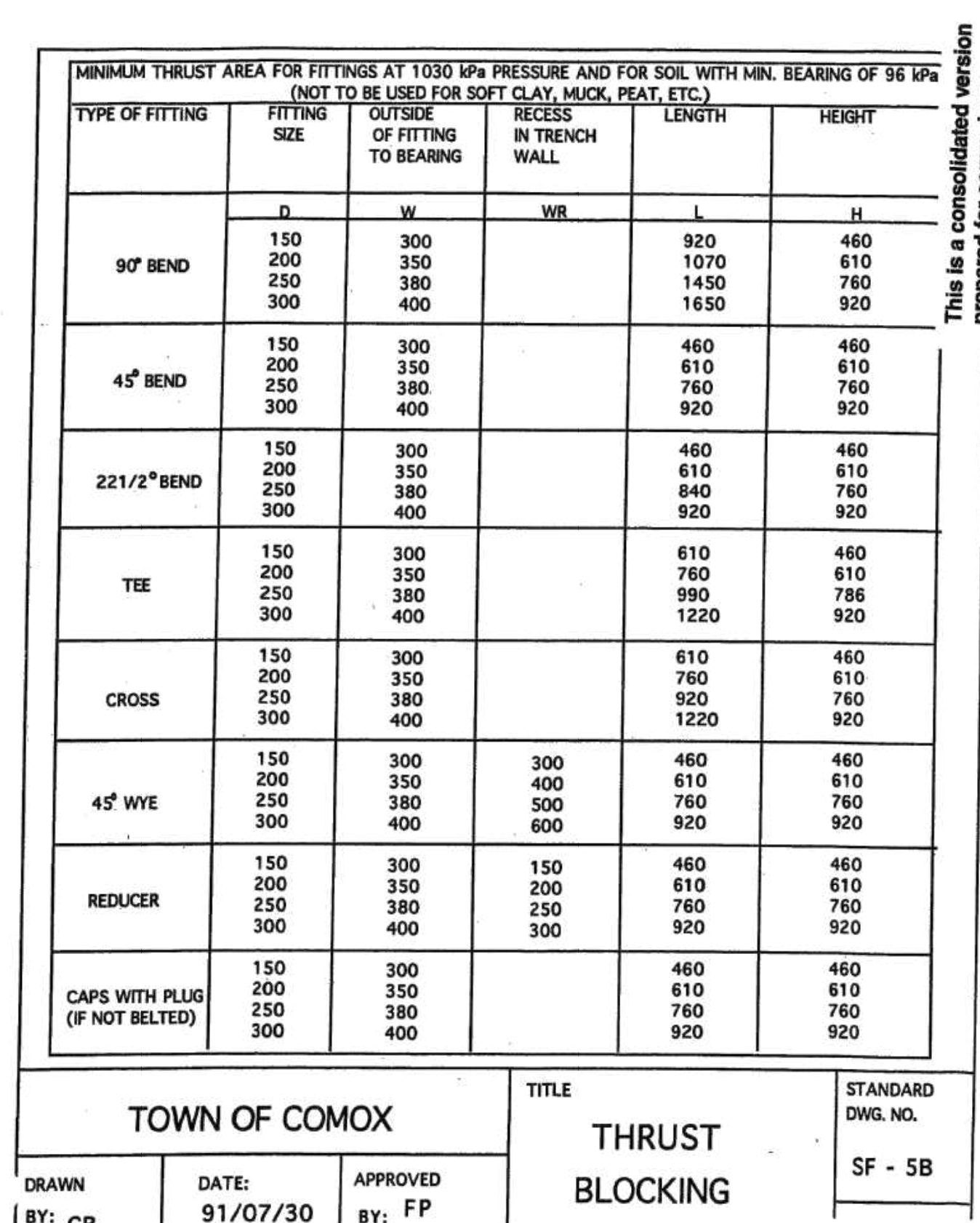
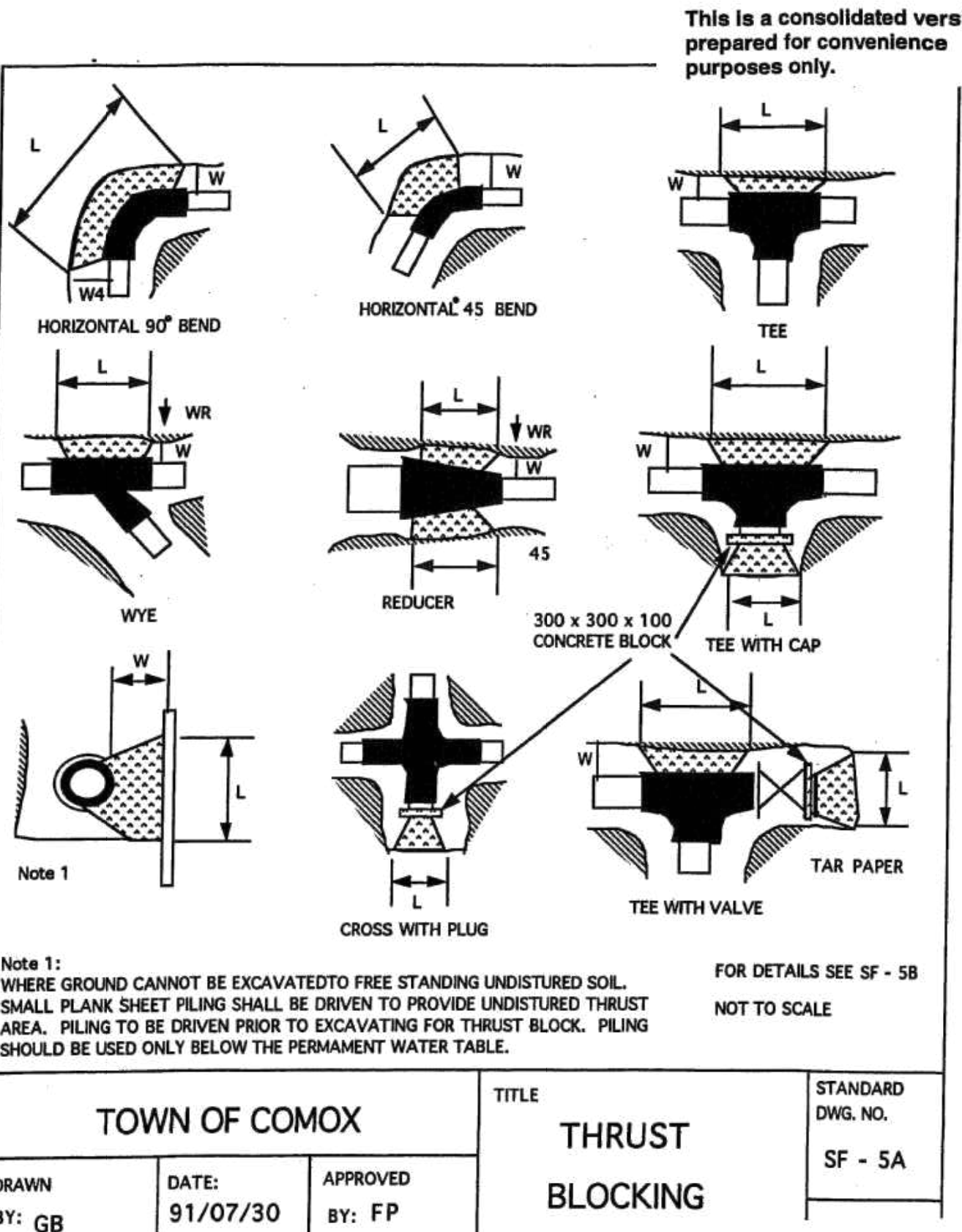


LEGEND:

PROPOSED	EXISTING
STORM LINE	STORM
MHCBMH-CB	MHCBMH-CB
SANITARY LINE	SANITARY LINE
SANITARY MH	SANITARY MH
WATER LINE	WATER LINE
VALVE	VALVE
CROSS-TEE-90	CROSS-TEE-90
45-22.5-11.25	45-22.5-11.25
NATURAL GAS LINE	NATURAL GAS LINE
OVERHEAD POWER	OVERHEAD POWER
UNDERGROUND POWER	UNDERGROUND POWER
GAS METER	GAS METER
WATER METER	WATER METER
FIRE HYDRANT	FIRE HYDRANT
SMS	SMS
OVERLAND FLOW	OVERLAND FLOW
MAJOR STORM EVENT	MAJOR STORM EVENT
FLOW DIRECTION	FLOW DIRECTION
ELEVATION MARKER	ELEVATION MARKER
SUBDIVISION	SUBDIVISION
ELEVATION MARKER	ELEVATION MARKER

ABBREVIATIONS

AD	AREA DRAIN	GM	GAS METER
ABD	ABANDONED	GND	GROUND
BOS	BOTTOM OF SWALE	GUT	GUTTER
BOW	BACK OF WALK	LOG	LIP OF OP
BRW	BOTTOM OF RETAINING WALL	INV	INVERT
CB	CATCH BASIN	OBV	OBVERT
CBMH	CATCH BASIN MANHOLE	PL	PROPERTY LINE
CSP	CORRUGATED STEEL PIPE	RD	ROAD DRAIN
DMH	DRAIN MANHOLE (STORM)	SMH	SANITARY MANHOLE
EL	EASEMENT LINE	SMS	SIAMSE CONNECTION
EX	EXISTING	STZ	STORAGE CONNECTION
FFE	FINISHED FLOOR ELEVATION	TOA	TOP OF ASPHALT
EX	EXISTING	TOB	TOP OF BERM
FLG	FLANGE ELEVATION	TOC	TOP OF CURB
FOC	FACE OF CURB	TRW	TOP OF RETAINING WALL
FW	FRONT OF WALK	WM	WATER METER
		XNG	CROSSING



EGBC PERMIT#: 1003932

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NO.	REVISION	DATE
4	RE-ISSUED FOR DP/RZ/DV OCP AMENDMENT	2024.07.12
3	ISSUED FOR SEPARATE LOT SERVICING	2024.05.30
2	RE-ISSUED FOR DP	2024.04.08
1	ISSUED FOR DP/RZ/DV OCP AMENDMENT	2023.08.28

DRAWN: HB DESIGNED: AR REVIEWED: AR

PROJECT NAME: HIGHLAND VILLAGE

PROJECT LOCATION: 1966 GUTHRIE ROAD COMOX, BC

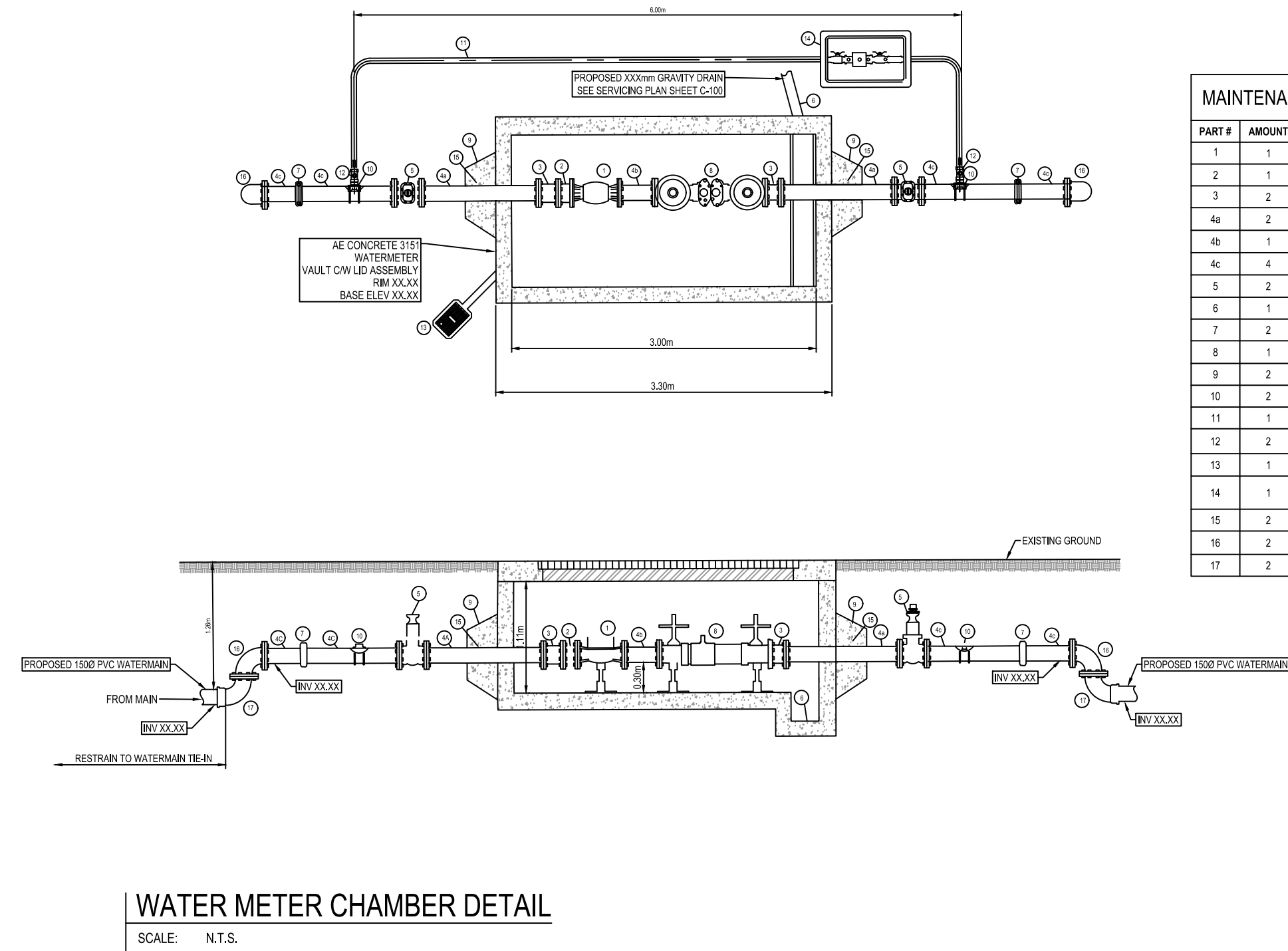
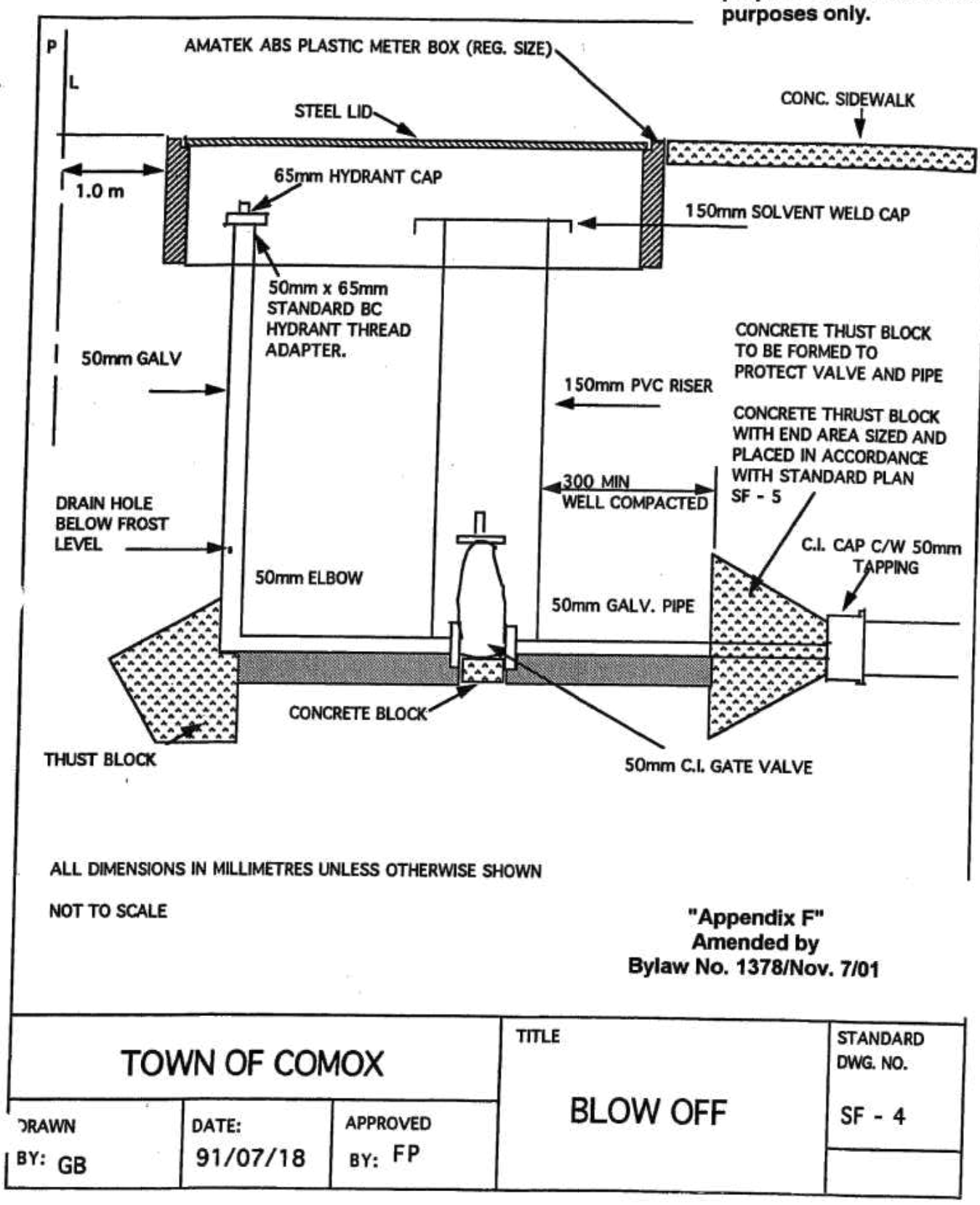
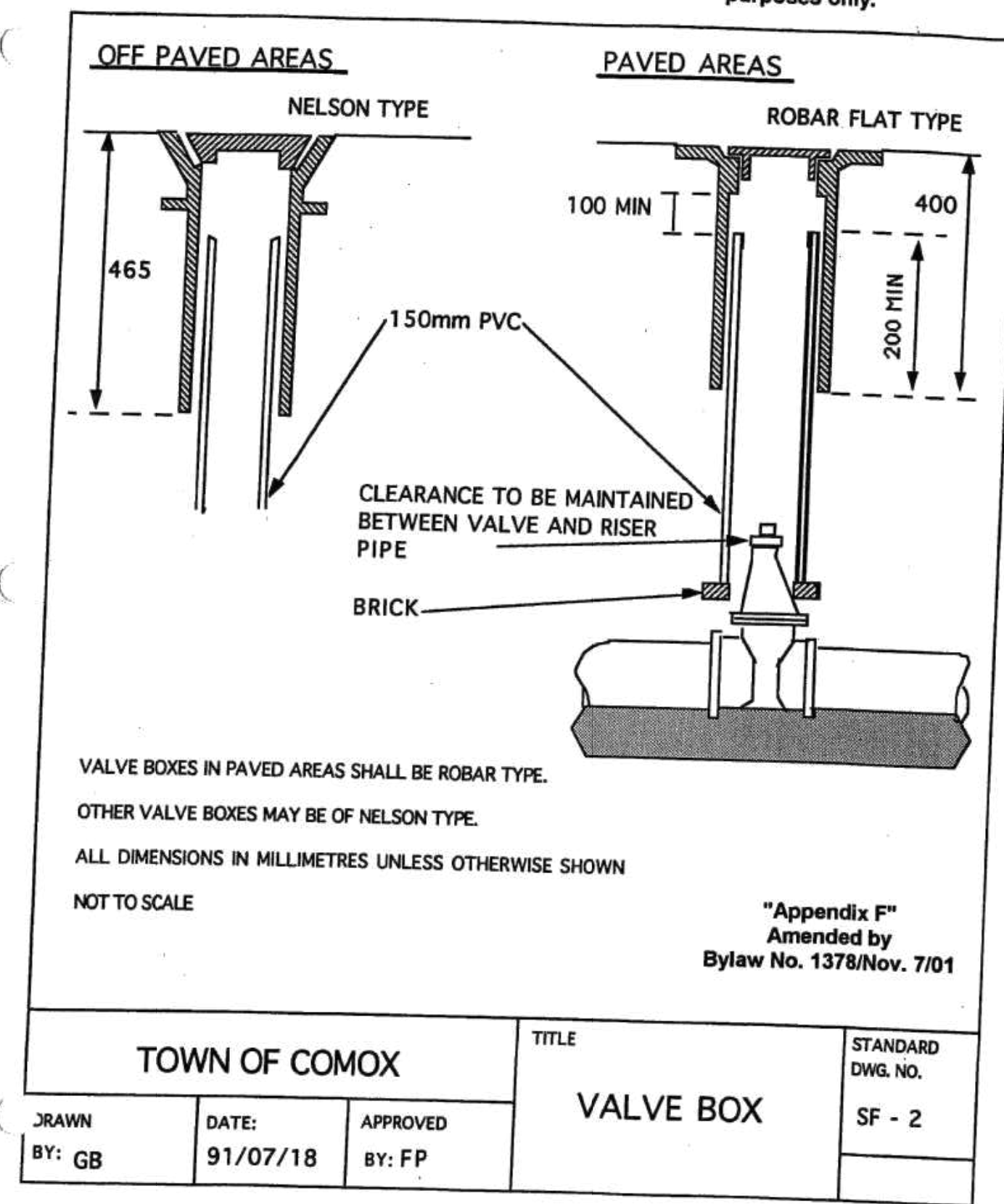
LOTS 1, SECTION 77, COMOX DISTRICT PLAN WP 72240

DRAWING NO.: C400

PROJECT NO.: 23-028

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prepared for convenience
purposes only.

This is a consolidated vers
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purposes only.



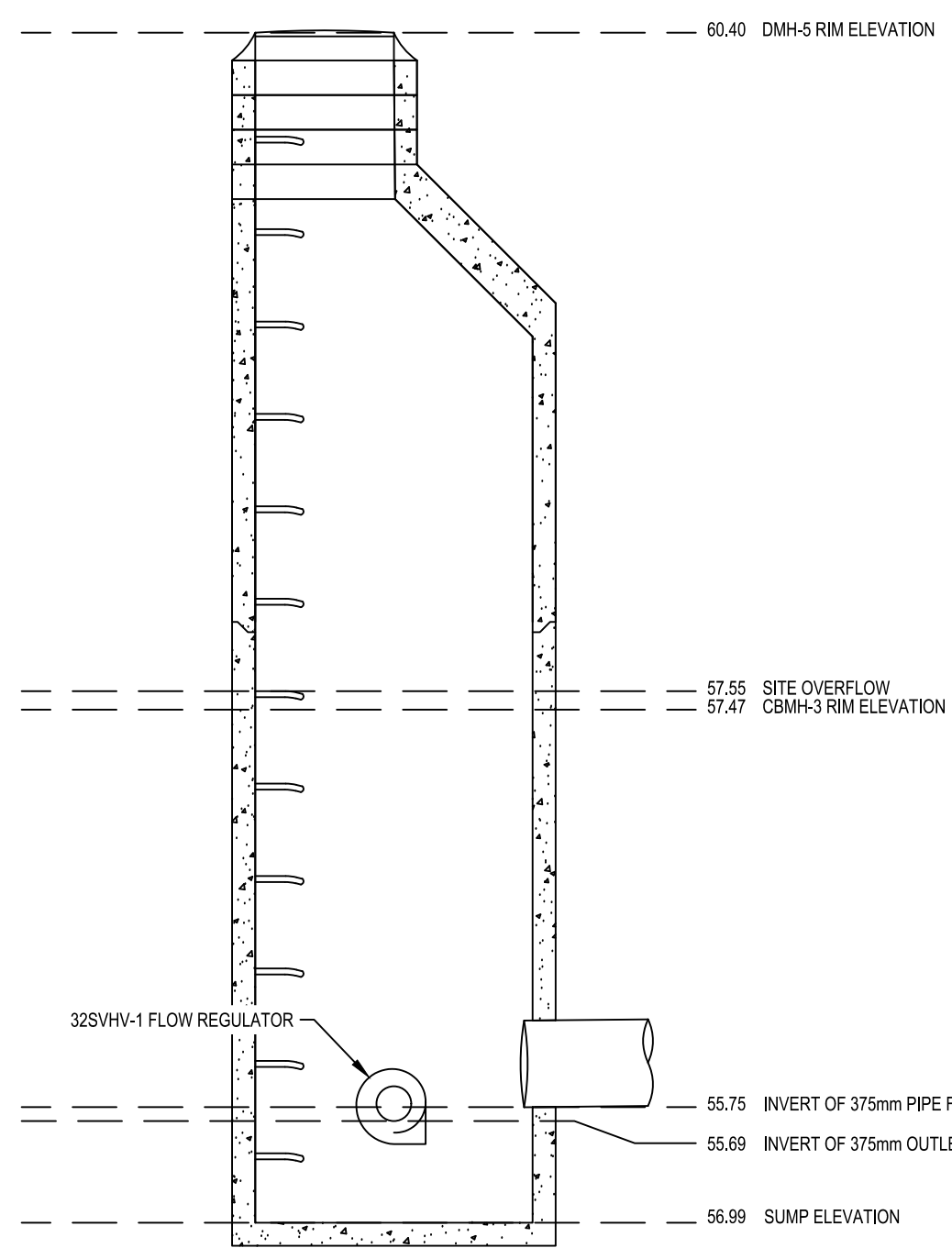
PART #	AMOUNT	DESCRIPTION
1	1	150 Ø NEPTUNE MACH 10 WATER METER (18 INCH LAY LENGTH)
2	1	150 Ø F x F NEPTUNE STRAINER
3	2	150 Ø DEWARTLING JOINT
4a	2	150 Ø F x F STEEL WATER MAIN TO AWWAC200 c/w AMERCOAT 395 EPOXY COATING OR APPROVED EQUIVALENT
4b	1	150 Ø F x F STEEL WATER MAIN TO AWWAC200 c/w AMERCOAT 395 EPOXY COATING OR APPROVED EQUIVALENT
4c	4	150 Ø F x F STEEL WATER MAIN TO AWWAC200 c/w AMERCOAT 395 EPOXY COATING OR APPROVED EQUIVALENT
5	2	150 Ø F x F GATE VALVE c/w NELSON BOX
6	1	DRAIN TO STORM MAIN
7	2	150 Ø WCTALUE COUPLER
8	1	150 Ø WATTS 757 DOUBLE CHECK VALVE ASSEMBLY W/ 90° GATE VALVES (OR EQUIVALENT)
9	2	200 Ø TRUCK THROUST BLOCK
10	2	150 Ø x 30 Ø DOUBLE STRAP SS SHOULDER
11	1	50mm Ø HDPE POLY
12	2	50mm Ø CAMBRIDGE BRASS MODEL 103 CURB STOP WITH NELSON BOX AND LOCKING LID
13	1	BROOKS BOX WITH CAST LID c/w RADIO HEAD REGISTER AND 90 Ø DUCT TO CHAMBER LOCATED IN A NON TRAVELED AREA
14	1	50 Ø WATTS 007 DOUBLE CHECK VALVE ASSEMBLY IN STANDARD CONCRETE METER BOX AS PER CCR STANDARD DRAWING C-1002
15	2	THRUST RING
16	2	150 Ø F x 90° VERTICAL BEND
17	2	150 Ø F x 90° VERTICAL BEND

LEGEND:

	PROPOSED	EXISTING
STORM LINE		
STORM MH/CSMH-CB		
SANITARY LINE		
SANITARY MH		
WATER LINE		
VALVE		
CROSS-TEE-90		
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ABBREVIATIONS

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FOC	FACE OF CURB	TRW	TOP OF RETAINING WALL
FOW	FRONT OF WALK	WM	WATER METER
		XNG	CROSSING



A DMH-5 DETAIL
C401 SCALE: N.T.S.

STAMP

EGBC PERMIT#: 1003932

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NO.	REVISION	DATE
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1	ISSUED FOR DP/ RZ/ DW/ OCP AMENDMENT	2023.08.28

NO. REVISION: YYYVJMMDD

DRAWN: HB DESIGNED: AR REVIEWED: AR

PROJECT NAME: HIGHLAND VILLAGE

PROJECT LOCATION: 1966 GUTHRIE ROAD, COMOX, BC

LOTS 1, SECTION 77, COMOX DISTRICT, PLAN VP 72240

DRAWING NAME: STANDARD DETAILS

DRAWING NO. C401

PROJECT NO.: 23-028

ATTACHMENT 4

APPLICATION PROCESSING STEPS

The following process would allow OCP and Zoning Amendment bylaws; Phased Development Agreement Authorization (PDA) bylaw; Development Variance Permit and Development Permit to be considered concurrently:

- ✓1. COMPLETE: Council consideration of proposed OCP amendment public consultation opportunities in accordance with *Local Government Act*, section 475;
- ✓2. COMPLETE: Publication of proposed OCP amendment notice on Town's bulletin board and website and the Introductory Planning Report on Town's website;
3. First Reading of proposed OCP Amendment Bylaw;
4. Council consideration of the Housing Needs Report dated May 2020, the Comox Strathcona Solid Waste Management Plan, and the current Financial Plan;
5. Second Reading of proposed OCP Amendment Bylaw;
6. First and Second Reading of proposed Zoning Amendment and Phased Development Agreement bylaws;
7. Notification of Public Hearing on proposed bylaws;
8. Public Hearing;
9. Third Reading of proposed bylaws;
10. Resolution of any outstanding items;
11. Adoption of proposed bylaws; and
12. Issuance of Development Permit.

ATTACHMENT 5

REFERRAL LIST AND RESPONSES (EXTERNAL; UPDATED)

REFERRAL RESPONSES (EXTERNAL; UPDATED)

NOTE: Rows shaded in light blue indicates new information

Attachment #	Referred Agency	Response
N/A	BC Assessment Authority	(Referral occurs following rezoning approval.)
5b	BC Hydro	<ul style="list-style-type: none"> ▪ Comments provided regarding the need to modify BC Hydro infrastructure in order to service the site.
5c	BC Transit	<ul style="list-style-type: none"> ▪ No concerns. ▪ Easy access to two transit stops that provide good access to the majority of Comox (via routes 1, 3 and 99). ▪ The Comox Valley Transit Future Action Plan 2022 proposes to increase service on routes 1 and 3, which are serviced by the nearby transit stops.
5d	Canada Post	<ul style="list-style-type: none"> ▪ "This development will require developer supplied lockboxes, parcel boxes, mailroom and we will need building access all to Canada Post Delivery Standards." ▪ "If approved, we request the developer contact us in advance to confirm all required standards and plan mail delivery within the development."
5e	Comox Valley Accessibility Committee	<ul style="list-style-type: none"> ▪ The CVAC provides suggested design changes.
5f	Comox Valley Food Policy Council	<ul style="list-style-type: none"> ▪ The CVFPC provides suggested design changes.
5g	Comox Valley Land Trust	<ul style="list-style-type: none"> ▪ The CVLT provides a suggestion (to the Town) for a condition of approval.
5h	Comox Valley Regional District	<ul style="list-style-type: none"> ▪ The CVRD provides comments, including a report from CAO James Warren regarding the proposed development to the Chair and Directors of the Electoral Areas Services Committee. ▪ See Attachment 6H for the full CVRD report responding from the referral.
5i	Emterra Environmental	<ul style="list-style-type: none"> ▪ The applicant self-referred to Emterra prior to submitting an application. ▪ The applicant has submitted a form (Attachment 6J) which is signed by an Emterra representative to confirm that the site can be serviced by Emterra.
N/A	Fortis BC	<ul style="list-style-type: none"> ▪ No response.
5j	K'ómoks First Nation	<ul style="list-style-type: none"> ▪ Written referral to KFN staff and Chief and Council. ▪ The proposed development does not require a KFN CHIP, however, a chance find procedure has been requested to be put into place during (during excavation and construction works). ▪ See Attachment 6J for a description of a Chance Find Procedure.
5k	Ministry of Transportation & Highways	<ul style="list-style-type: none"> ▪ No concerns.

5I	19 Wing Comox, DND	<ul style="list-style-type: none"> ▪ No concerns with the height of the proposed building (which exceeds the existing height restriction in the Comox Airport Zoning Regulations for buildings along the flight path). ▪ Comox Airport Zoning Regulations are in the process of being amended to relax existing height restrictions. Until the amendments are completed, the Minister's Office has agreed to register land sites that have been vetted by DND staff (as has been done for the proposed development).
5m*	NAV CANADA	<ul style="list-style-type: none"> ▪ No objection to the proposed development. ▪ The evaluation is valid for a period of 18 months (expiring on July 17, 2025).
5m*	Transport Canada, Civil Aviation	<ul style="list-style-type: none"> ▪ No objection to the proposed development. ▪ The evaluation is valid for a period of 18 months (expiring on July 17, 2025).
N/A	RCMP	<ul style="list-style-type: none"> ▪ No response.
5n**	School District SD71	<ul style="list-style-type: none"> ▪ Referred to SD 71 Board. ▪ SD 71 staff (Director of Operations) responded advising of the need for more school space and need for Town to identify and secure future school space for the School District.
N/A	School District SD93 (francophone)	<ul style="list-style-type: none"> ▪ Referred to SD 93 Board. ▪ No response.
N/A	Shaw Cable	<ul style="list-style-type: none"> ▪ No response.
5o	Telus	<ul style="list-style-type: none"> ▪ No concerns. ▪ Telus have existing conduit and if developer would like to change the route then relocation is required. ▪ A schematic for the existing conduit is provided on Attachment 6O

**Responses from NAV CANADA and Transport Canada provided in tandem.*

***Attached referral response is the email response to the referral only. The 61-page letter attached to the response email is from the SD 71 board and provides detailed standards (in an attachment document from the Ministry of Education) regarding prescribes areas in elementary, middle and secondary schools and other standards for sites, grounds and district service facilities. The letter from the SD 71 board and the attached ministry standards document are provided in attachment 3F of the November 15, 2023 Planning Report on RZ/OCP 23-4.*

North Vancouver Island Design
locatecty@bchydro.com

October 26, 2023

Robin Pallett
rpallett@comox.ca

Dear Robin:

Proposed mixed-use development at 1966 Guthrie Road

Application for: [1966 Guthrie Road](#)

We have reviewed your proposed Conventional Subdivision Application regarding the above noted location. BC Hydro has a Statutory Right-of-Way (SRW) registered against this property which allows BC Hydro to install, replace, maintain, and access works related to our electrical delivery system. Please read the [Rights-of-Way Guidelines](#) and call our Right-of-Way Helpline at 1-800-667-1517 to find out what restrictions apply to your development.

Modifications to the BC Hydro system will be necessary to extend the electrical system to this development. BC Hydro will need to complete a thorough system study and design to provide a cost estimate for this work. The developer must submit an application for service with our BC Hydro Express Connect Department by phone at 1-877-520-1355 or [online](#). Approximate design and construction lead times will be provided at the time of application.

There is overhead electrical infrastructure adjacent to this property which may affect this development. It is the developer's responsibility to ensure that any infrastructure built on this property will meet all current CSA requirements and limitations around high voltage equipment and transformation.

Normally, BC Hydro will not relocate existing BCH plant. If the developer wishes to have BC Hydro relocate its plant to meet CSA requirements, an application must be submitted to the local BC Hydro District Office to conduct a feasibility study of a proposed redesign. The developer will be fully responsible for all associated redesign costs.

In addition, please review the following links regarding [vegetation guidelines](#) (if applicable).

Sincerely,

Silvana Bravo

NVI Design Assistant Team



Development Evaluation

September 25, 2023

Development Location: 1966 Guthrie Rd.

Local Government: Town of Comox

Transit System: Comox Valley

Local Government File Number: OCP/RZ 23-4, SD/DP 23-5, DVP 23-2

Land-use Typology

- Urban
 Suburban
 Rural

Local Context

- Urban Infill
 Urban-suburban Fringe
 Suburban Infill
 Suburban-rural Fringe
 Rural
 Suburban Sprawl
 Urban Sprawl
 Urban-rural Fringe

Density

- Increasing
 Same/similar
 Decreasing

Supporting Policy & Design

- Offsite pedestrian connections to nearest bus stop(s)
 Onsite pedestrian connections to public pedestrian facilities
 Cyclist connections to nearest bus stop(s)
 Onsite cyclist facilities
 Universally accessible
 Reduced or remove minimum parking requirements
 Grid or grid-like block design
 Small block size
 Connections to lands beyond

Transit Readiness

Service:

Current service is...

Anderton at Guthrie- Route 1, 350m from site.

Guthrie at Stadacona- Route 1, 3, 99. 400m from site.

Planned service is...

Route 1- Increased service to 15 minutes frequency on weekends.

Route 3- Increase span of service on weekdays, Saturdays and Sundays

Infrastructure:

Current infrastructure is...

Bench, shelter, garbage, at Anderton at Guthrie and Guthrie at Stadacona

Sidewalks on the site side of Anderton and Guthrie

There is no know planned infrastructure at these bus stops

Thank you for the opportunity to review this proposed development.

BC Transit is satisfied with the current and planned infrastructure that the site provides for pedestrians accessing transit. There are two stops with the basic amenities within walking distance from the site with easy access by sidewalk to commute there. The routes accessible are the 1, 3, and 99 which provide great access to the majority of Comox.

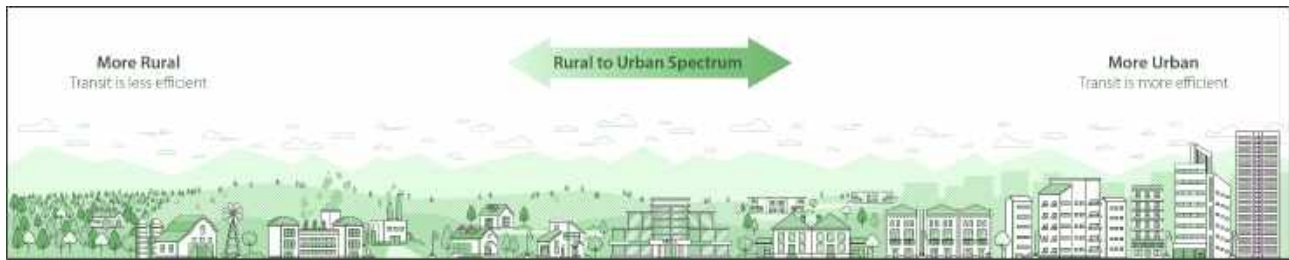
This development evaluation was completed by Nicholas Alfred (nalfred@bctransit.com). If you have any questions or would like to discuss further on this proposal, please contact:

Victor Copetti

Transit Planning

BC Transit

vcopetti@bctransit.com



Land-use Typology

The proposed development is considered a Suburban land-use typology is generally low to medium density and often lacks a sufficient range of uses to fully support people living with the community.

Local context

The proposed development location is considered a Suburban Infill: The majority of land uses around the development are suburban and the development is suburban

Transit Readiness

Existing Service

The proposed development has access to routes 1, 3, and 99.

Planned Service

The Comox Valley Transit Future Action Plan 2022 proposes increased service to 15-minute frequency on weekdays for route 1 and Increased span of service on weekdays, Saturdays and Sundays for route 3.

Existing infrastructure meets Standards.

Supporting Policy & Design

Offsite pedestrian connections to nearest bus stops(s)

All transit riders start and end as pedestrians. Without pedestrian connections to the nearest bus stop(s), customers will not be able to travel to and from transit services to the development. Public sidewalks, pathways, or other connections are need to connect the development to the nearest bus stop(s).

Onsite pedestrian connections to public pedestrian facilities.

All transit riders start and end as pedestrians. Without pedestrian connections to the nearest bus stop(s), customers will not be able to travel to and from transit services to the development. Sidewalks or pathways onsite need to connect to the greater public pedestrian network to be able to access transit services.

Cyclist connections to nearest bus stop(s)

Connecting active transportation networks to transit services allow for multi-modal transportation choices. Bike lanes or shared pedestrian/cyclist multiuse pathways should connect to the nearest transit stop(s).

Onsite cyclist facilities

The development should include facilities to support active transportation. At a minimum this would include parking or storage for bicycles. Other possible facilities could include a repair and maintenance facilities, or showers for employees who commute by bike.

Universally accessible

The development and all connections from the development to the nearest transit stop should be accessible and barrier free for all ages and abilities. Transit service is for everyone.

Reduced or remove minimum parking requirements

Requiring a minimum amount of parking incentivises the continued use of private automobiles which in turn decreases the efficiency of transit services. Reducing or removing the minimum parking requirements seeks reduces one of the subsidies that private automobile uses receive, which in turn can make transit more attractive.

Grid or grid-like block design

Grid or grid-like blocks or road network design helps support transit in a couple of ways. Grids allow for flexibility in the service; if a portion of the road needs to be closed a grid network allows for the bus route to take a small detour with minimal impact to service coverage area and trip times. Grid networks also allow for more connections throughout the area which decrease walking or cycling distances and increase the possible routing transit can take through the community.

Small block size

Smaller block sizes increase the number of connections which in turn decrease the total walking or cycling distance people need to travel to reach transit services. Blocks should be no longer than 200 metres in length along their longest side.

Connections to lands beyond

Development shouldn't prevent quality access to lands beyond the development area. Not providing connections can make providing transit service inefficient and expensive. This does not apply to lands beyond if they are outside of urban containment boundaries or within the Agricultural Land Reserve.

From: [PARRY, Doug](#)
To: [Robin Pallett](#)
Cc: [GRENIER, Nicolas](#)
Subject: RE: 1966 Guthrie Rd: Town of Comox applications OCP/RZ 23-4, SD/DP 23-5, DVP 23-2: external app referral
Date: September 11, 2023 10:41:49 AM

Warning* This E-Mail originated from outside The Town of Comox. *Please open with Caution

Good day Robin,

Thank you for the opportunity to review.

This development will require developer supplied lockboxes, parcel boxes, mailroom and we will need building access all to Canada Post Delivery Standards.

If approved, we request the developer contact us in advance to confirm all required standards and plan mail delivery within the development.

If proceeding our planning contact for this development will be Nic Grenier
nicolas.grenier@canadapost.postescanada.ca

Thank you,

Doug Parry
Canada Post Delivery Planning
1847 E. Wellington Rd
Nanaimo BC V9S 5L0

From: K Kratz <kratz.k@gmail.com>

Sent: Wednesday, September 20, 2023 7:22 PM

To: CVSPS - Christopher Bate <comoxvalleysocialplanning@gmail.com>

Subject: Re: Fwd: 1966 Guthrie Rd: Town of Comox applications OCP/RZ 23-4, SD/DP 23-5, DVP 23-2: external app referral

Warning This E-Mail originated from outside The Town of Comox. *Please open with Caution*

Thanks for the opportunity to look at this, my first impressions are:

1) there is only 1 accessible parking space for a total of 35 visitor parking (including the commercial units). This is not enough parking even for just the commercial units (8-9 including a cafe) This visitor parking is shared with the entire complex. If a working bus route/ public transit that captured the entire Town of Comox, serviced this area you might be able to get away with two for the commercial units alone. The demographics of the Town suggest this is an inadequate amount of parking for the visitor (including commercial) potential. I applaud the tremendous emphasis on biking (including bike repair and washing!) - will there be room for hand-cycles?

2) the underground/ tenant parking has a total of 8 accessible parking stalls out of 177 (for 205 residential units) Unless there is some viable public transit option, this is wholly outside the realm of reality in the Town of Comox. 8 units of accessible parking is not addressing the demographic of the Town of Comox. According to the census of 2021 a little over 42% of the population of the Town of Comox is 55 years of age or older. 20.5% of the BC population aged 15-64 is living with a disability. The statistic is higher for older populations. Unfortunately I could not find the number for the Town of Comox itself. Statistically, the demographic of Comox would encourage accessible consideration at least at 20% of parking stall requirement. Of 35 parking spaces would be 7 spaces. 20% of 177 would be over 35 spaces.

3) there is a delightful exterior pathway that navigates to the residential units "F1" . Each of these units is accessed via steps/ stairs. To create an accessible and more importantly - visitable- space, 'cart path with limestone crush' could be used in place of gravel to create an accessible pathway. The steps would be mitigated with sloping ramps.

4) There are two elevators to accommodate the daily access to 205 units. Are these double elevators, single elevators? I got a little confused as 'stair A' moves on the second story upward... all good now. Are there security features in place for the underground parking as there are at least four access points from the ground level into the area.

5) There are 10 adaptable units - nice start! Thanks for locating the units near the elevator.

6) Will the balconies of the adaptable units be zero barrier? Also, all the adaptable units are one bedroom.. will there be possibility of adaptation for larger units to house families? As well the outdoor amenity areas (how lovely!) ..will the access to the areas be zero barrier?

7) I really like the infill of vegetation and trees (or so it appears) to bring nature into the environment.

8) the drive through is generally situated to avoid the entrances to the CRU units and is broken up with vegetation. This is a departure for the Town of Comox since around 2009.. nonetheless, a painted crosswalk between the residential walkway (near the accessible parking) and the breezeway to the cafe would mitigate the possible problems of 'pedestrian traffic' across the drive through path. This is true also at the entry point to the drive through.. There may be some tricky moves for parked cars to leave especially the accessible stall and the stalls to the west across the way as they back into the drive through lane. I am assuming that the greyed out area is cement sidewalk, will there be curb cuts to accommodate mobility devices at all transition points?

9) I am assuming vehicles enter and exit via the ramp at the north west end ... Having the elevator again close to the accessible parking stalls is great. I could not see the entry to the elevator and assume it is within the vestibule.

Thank you so much for the opportunity to look at the drawings. I appreciate the work put into these designs. I look forward to feed back others have to enhance this opportunity to create a great housing option for the Town of Comox.

Karin

On 2023-09-14 20:57, CVSPS - Christopher Bate wrote:

Hello Accessibility Committee Members,

Please see attached information from the Town of Comox re: a development, including apartments, proposed for 1966 Guthrie Road. Please send any responses directly to the Town of Comox.

Kind regards,

Christopher Bate (they, them) [why I share my pronouns](#)

Coordinator, Social Planning Society and Accessibility Committee

~



December 21, 2023

Town of Comox

Planning Department

Mayor and Council

% Robin Pallett

RE: Consultation on development of property at 1966 Guthrie Road

Dear Planning Department, Mayor and Council,

I am writing to you on behalf of the Comox Valley Food Policy Council (CVFPC). The Comox Valley Food Policy Council provides a forum for advocacy and policy development that works towards the creation of a food system that is ecologically sustainable, economically viable and socially just.

We understand that The Town of Comox has received an application to amend the Comox Official Community Plan Bylaw, 2011 (OCP) for the vacant easterly portion of the property at 1966 Guthrie Road. As per the referral direction from Comox Council, the CVFPC is providing comment on the subject development application.

We recognize the need to build new homes to meet the needs of our growing, vibrant community. Reviewing the application through our lens of food security, we have 4 key recommendations:

Recommendation 1: Increased protection of the Agricultural Land Reserve Buffer

This property is farm-adjacent, backing onto ALR land on the North and East sides of the property. While the applicant has proposed a 15m landscaped buffer which “provides a significant buffer to shield agricultural lands from this proposed development”, we request this be revisited and additional protections be taken. We recommend the applicant consider moving the parking lot to the back of the building to reduce noise travel and further increase and protect the proposed buffer.

Recommendation 2: Increased permeability/drainage functions of paved area

We suggest the applicant look at permeable alternatives to standard paving, particularly in the parking lot areas. More paving leads to more runoff, especially in the case of the proposed development sitting directly adjacent to farms. The current proposal could result in flooding and introduce contamination onto farming land via runoff. Additionally, we want to recognize that the farmer on the neighbouring property has voiced concerns about the water impacts on the existing well.

Recommendation 3: Decreased size of paved area

Another recommendation to mitigate runoff and potential flooding and water contamination is to reduce the applicant's parking requirement. Reduction in parking spots should not reduce the desirability of the development, especially given the development's proximity to an active bus route and the area's "great walkability score and ... [adjacency] to dedicated bike lanes". The applicant states that they "support an active lifestyle by encouraging bike-use and showcasing two large secure bicycle centres": reviewing the applicant's parking statistics, there seems to be an opportunity to further increase the number of bicycle parking spaces, thereby reducing the amount of paved parking spaces required.

Recommendation 4: Increased food growing opportunities

Reducing parking areas also creates an opportunity for a larger community garden space. Members of our community moving into these new developments may be losing access to their food-growing spaces, creating a gap in accessibility. We recommend additional community garden plots be added to the landscaping plan to close this gap. A potential location we identified for these plots is at the back of the building within the ALR buffer on the North and/or East sides of the property.

There is also an opportunity for green space on rooftops, be that for food growing or environmental benefits such as reducing air pollution and reducing the urban heat island effect.

We also encourage incorporating traditional indigenous plants into landscaping plans and recognize that the applicant has made an effort to do so.

In conclusion, we urge the applicant to consider further increasing and protecting the ALR buffer, incorporate permeable alternatives to paving, reduce the paved parking areas and increase the food growing spaces for occupants, in accordance with their vision of a walkable, active community. These amendments to the plan will support the town of Comox and CVRD in building a food-secure future for our community.

Thank you for reaching out for our comments. Please do not hesitate to contact us with any questions, concerns, or requests for clarification.

Sincerely,

A handwritten signature in black ink, appearing to read "Maurita Prato". The signature is fluid and cursive, with the first name being more legible than the last.

Maurita Prato,
Coordinator, Comox Valley Food Policy Council



Brooklyn Creek Watershed Society
Web Address: Brooklyncreek.ca

November 29, 2023

Re: Comments on Development Proposal -- 1966 Guthrie Road

Dear Comox Town Council,

On behalf of Brooklyn Creek Watershed Society, thank you for the opportunity to comment on the above proposal. The proposed development lies adjacent to Brooklyn Creek, and at a higher point relative to the Creek. The development seems to be fairly large scale and it is reasonable to expect that considerable amounts of stormwater discharge (with the accompanying pollutants such as road salt, soaps from car washing and other cleaning, and toxic organic chemicals) could enter the watershed of Brooklyn Creek. Such pollution could compromise the efforts of the Town and its partners to restore Brooklyn Creek, through its investments totaling more than \$400,000 from 2005 to the present, thousands of person-hours of volunteer labour and very substantial in-kind contributions from generous donors.

To safeguard the Creek, we are strongly recommending that the Town makes the following condition of approval: *that the developer retains a properly-qualified consultant with fisheries and hydrological expertise to complete a comprehensive investigation of the plan for stormwater management.* The report should identify measures to mitigate any damaging effects of the development. Finally, the developer must commit to fully implement measures to protect Brooklyn Creek from any adverse effects from the construction and occupancy phases of the development.

As a minor comment, we appreciated the developer's efforts to include many native plants, as indicated in the landscape plan. We note, however, that one of the non-native species included (*Pieris japonica*) has not been thriving in this area, and the consulting landscape architect might be asked for a substitute that would do better in the Comox Valley.

Sincerely,

A handwritten signature in blue ink, appearing to read "John D. Neilson".

John D. Neilson, Ph.D.

President
Brooklyn Creek Watershed Society

770 Harmston Avenue, Courtenay BC V9N 0G8
Tel: 250-334-6000 Fax: 250-334-4358
Toll free: 1-800-331-6007
www.comoxvalleyrd.ca

File: 6500-30

March 6, 2024

Sent via email only: rpallett@comox.ca

Robin Pallett
Town of Comox
1809 Beaufort Avenue
Comox, BC V9M 1R9

Dear Robin Pallett:

Re: Town of Comox Referral – 1966 Guthrie Road

Thank you for the opportunity to provide comments on the proposed development 1966 Guthrie Road in the Town of Comox. The Electoral Area Services Committee considered the matter at its February 26, 2024 meeting and endorsed the comments outlined in the attached staff report.

In addition to the attached comments, the CVRD also wishes to emphasize the following points concerning this proposed development:

- That the 15-metre landscaped/vegetated buffer be maintained and that fencing be included in the buffer, along the property lines
- That the developer follows through on comments made at Advisory Planning Commission meetings that tenants will have to acknowledge in writing the differences that living next to an active farm can bring (e.g. noise, odour, etc. through regular operations, such as the use of manure or burning debris)
- That woodstoves are permitted as a primary heating source within the CVRD

Please contact Dylan Thiessen, Planner I – Long Range, at dthiessen@comoxvalleyrd.ca if you have any further questions.

Sincerely,

T. Trieu

Ton Trieu, RPP, MCIP
Manager, Planning Services
Planning and Development Services

Enclosure

The Comox Valley Regional District respectfully acknowledges the land on which it operates is on the unceded traditional territory of the K'ómoks First Nation, the traditional keepers of this land.

DATE: January 31, 2024

FILE: 6500-30/Comox and 6500-40/Comox

TO: Chair and Directors
Electoral Areas Services Committee

FROM: James Warren
Chief Administrative Officer

Supported by James Warren,
Chief Administrative Officer
J. Warren

RE: Town of Comox Referral – 1966 Guthrie Road

Purpose

To respond to the Town of Comox (Comox) referral on a proposed Official Community Plan (OCP) and Zoning Bylaw amendment for a multi-unit residential/commercial development that is adjacent to Lazo North (Electoral Area B) (Area B).

Recommendation from the Chief Administrative Officer:

THAT this staff report, dated January 31, 2024, be provided to the Town of Comox in response to a referral relating to a proposed mixed-use, multi-residential development at Lot A, Section 77, Comox District, Plan 86498, PID 027-869-067 (1966 Guthrie Road).

Executive Summary

- A referral from Comox was received for an OCP and Zoning Bylaw amendment application.
- The application proposes a mixed-use, multi-family development including 205 rental units and approximately 1,400 square metres of commercial floor area on the eastern portion of the property at 1966 Guthrie Road. The westerly portion of the property is developed for commercial use. The site of the proposed development is adjacent to properties in Area B to the east and north.
- Provided is an overview of the proposal and staff’s comments relating to transportation (including active and public transportation), housing supply and affordable housing, the residential/agricultural interface, landscaping, rainwater management, and servicing infrastructure.
- Staff recommend that this report be forwarded to staff at the Comox as a response to the referral.

Prepared by:

Concurrence:

Concurrence:

D. Thiessen

T. Trieu

A. Mullaly

Dylan Thiessen
Planner I – Long Range

Ton Trieu
Manager of
Planning Services

Alana Mullaly
General Manager of Planning
and Development Services

Government and Community Interests Distribution (Upon Agenda Publication)

Town of Comox	✓
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Background/Current Situation

The referral from Comox for a proposed development on the property at 1966 Guthrie Road (Figures 1 and 2) is adjacent to properties in Area B to the north and east. The subject property is approximately 2.7 hectares in size and is zoned C3.1 (Arterial Commercial) under Comox's Zoning Bylaw No. 1850. Most of the property (1.6 hectares) is already developed with commercial uses, though approximately 1.1 hectares of the easternmost part of the property remains undeveloped, which is the site of the proposed development. The proposed development requires several applications to Comox, including:

- Subdivision to subdivide the undeveloped 1.1 hectares from the remainder of the property;
- OCP amendment to change the land use designation and the development permit area map to capture mixed-use commercial-residential on the eastern (undeveloped) portion of the property;
- Rezoning to create a comprehensive development zone tailored for the proposed development;
- Development Variance Permit to allow the proposed development on proposed Lot 2 to be serviced via proposed Lot 1.

The proposed development includes 205 rental units and approximately 1,400 square metres of commercial floor area in a six-storey building (Figures 3 - 5). The proposal also includes 35 surface parking stalls, 178 underground parking stalls, rainwater management infrastructure, and servicing infrastructure.

Per Section 475(2) of the *Local Government Act* (RSBC, 2015, C. 1) (LGA), when a local government is considering an amendment to its OCP, it must consider opportunities for consultation with several affected groups, including the board of the regional district in which the area covered by the plan is located. This report is being brought to the Electoral Areas Services Committee given this LGA requirement, as well as the size and scope of the proposed development.

Planning Analysis

The Regional Growth Strategy (RGS) being the "Comox Valley Regional District Regional Growth Strategy Bylaw No. 120, 2010," designates the subject property as being within a Municipal Area. However, the RGS does not provide further direction on where growth should be concentrated *within* Municipal Areas. The proposed location is in the immediate vicinity of a major intersection (Anderton Road/Guthrie Road), a number of commercial businesses, and some higher density residential developments.

CVRD Comments: Staff's comments are informed by an understanding of applicable plans and bylaws as well as comments provided by various affected CVRD departments, the Agricultural Advisory Planning Commission (AAPC), and the Advisory Planning Commission (APC) for Area B. The comments focus on landscaping and rainwater management, active and public transportation, housing needs, the residential/agricultural interface, and servicing requirements.

Landscaping and Rainwater: The Town of Comox’s guidelines for the Mixed-Use Commercial-Residential development permit area require that all areas not covered by buildings, structures, driveways, or parking areas be landscaped. Permeable landscaping decreases the amount of impervious surface area, which helps manage rain and stormwater onsite. A stormwater management plan has been prepared which requires a bioswale capable of storing 459 cubic metres based on a 1-in-100 year storm event, though this is based on calculations for the entire development site except the 15 metre landscaped buffer between the proposal and the Agricultural Land Reserve (ALR) property and some proposed pathways – “the entire proposed development will be controlled other than the ALR [setback] buffer as well as some pathways south of the new Building 1 which will continue to drain per the existing drainage paths towards the Agricultural Land Reserve.” Staff want to ensure that this would not result in an increase in volume or change in timing of the flows from pre-development conditions, but strongly encourage Comox to ensure that all rainwater and stormwater is managed onsite in ways that account for climate change and increasingly frequent and severe storm events.

Transportation: The applicant’s design rationale (Appendix A) references working with Watt Consulting to “analyze parking and traffic requirements/patterns and provide alternate means (bicycle use, public transit)” for transportation; staff encourage Comox to ensure the traffic assessment includes impacts within the adjacent areas of Electoral Area B (i.e. consider a transportation network that transcends political boundaries).

Active Transportation: Staff note the amount of bike storage on site, as well as a bike repair and wash area, but encourage the development of a protected multi-use path along Guthrie Road to further increase the accessibility of active transportation options. This would align with a regional project outlined in the [Comox Valley Active Transportation Network Plan Implementation Strategy](#).

Public Transportation: Given the significant arterial connections provided by Anderton Road and Guthrie Road with bus stops near the intersection, improved bus shelters would be supported by policies 4A-5 and 7A-6 of the RGS if an opportunity exists for these upgrades.

Housing: The proposed development will contain 205 rental units, 10 per cent of which (20 units) will meet the Province of BC’s Adaptable Housing Standards for increased accessibility. Staff note the documented need for more purpose-built rental housing in the region, as well as a range of unit sizes to meet the needs of different individuals or families, noting that many are included in the proposal (from studio apartments to three-bedroom apartments). However, ensuring that a certain proportion are rented at below-market rates is necessary in order to support Comox Valley residents during the housing crisis. Staff note that an [introductory report](#) on the proposal to Comox Town Council indicates that no affordable housing contributions are proposed, and strongly encourage the Town to require the provision of affordable housing units or contributions per the Town’s Affordable Housing Amenity Contribution Policy. The [Regional Housing Needs Assessment](#) highlights a need for more subsidized and non-market rental housing, noting

that “the private market is not able to provide housing for a significant proportion of the Comox Valley.”

Residential/Agricultural Interface: Both the [CVRD OCP](#), and the [RGS](#) have policies that encourage natural or landscaped buffers between residential development and agricultural land. The CVRD’s Farmland Protection Development Permit Area guidelines also suggest a minimum 15.0 metre landscaped buffer when building a structure on a lot that is adjacent to land in the ALR. The Development Permit Area (DPA) guidelines do not apply to this development as the development is not within the CVRD’s jurisdiction, but the proposed development does feature a 15.0 metre vegetated buffer along both lot lines that are adjacent to ALR land. The AAPC was supportive of the proposal because of the 15.0 metre landscaped buffer and a commitment from the developer to have tenants acknowledge in writing the differences that living next to an active farm operation can bring (e.g. odour, noise).

Servicing Infrastructure: The Comox Valley Water System (CVWS) water master plan currently underway will confirm the capacity of existing regional water transmission systems and projects that may be needed to accommodate growth. CVRD staff will work closely with Comox staff to ensure that development projections are up to date and that specific water conservation measures will be implemented to minimize the cost of infrastructure to the community and limit the impact on local aquatic ecosystems. The proposed development will incur CVWS and Comox Valley Sewerage System (CVSS) development cost charges (DCCs), to be collected by Comox and transferred to the CVRD, and staff note that a study is underway to update the CVSS DCC bylaw and rates.

Options

1. Authorize staff to provide this report, as well as any public correspondence received about the proposal, to the Town of Comox as a response to the referral staff have received for the proposal at 1966 Guthrie Road.
2. Direct an alternative response.
3. Provide no comment to the Town of Comox.

Staff recommend Option 1.

Financial Factors

Resources spent on the referral review come from the Function 500 electoral areas planning service budget for staff time.

Strategic Considerations - Strategic Drivers							
Fiscal Responsibility		Climate Crisis and Environmental Stewardship and Protection		Community Partnerships	✓	Indigenous Relations	Accessibility, Diversity, Equity and Inclusion

Community Partnerships: The LGA requires a municipality to consider consultation with the applicable regional district when considering an amendment to its OCP. Referrals from member municipalities allow the CVRD to provide comments and recommendations based on CVRD policies and impacts on regional services (e.g. transit, solid waste) for consideration during Comox’s review process. This will promote a regionally aware approach to development and increase community partnerships.

Strategic Considerations - Regional Growth Strategy Goals							
Housing	✓	Ecosystems, Natural Areas and Parks	✓	Local economic development		Transportation	✓
Infrastructure	✓	Food Systems		Public Health and Safety	✓	Climate Change	✓

The proposal is related to several objectives, policies, and goals. A more fulsome discussion is provided above in the Planning Analysis section under “CVRD Comments,” but applicable objectives include:

- 1-B: Increase affordable housing options.
- 1-D: Minimize the public costs of housing (e.g. servicing).
- 2-B: Frame environmental protection and policies around the principles of precaution, connectivity, and restoration.
- 4-A: Increase public transit use.
- 4-B: Improve bicycle and pedestrian infrastructure to increase the use of active transportation options.
- 5-C: Manage stormwater to preserve ecosystem and watershed health.
- 7-A: Increase the number of pedestrians and cyclists in the Comox Valley.
- 8-B: Reduce Greenhouse Gas emissions created by the on-road transportation sector.

Intergovernmental Factors

Pending consideration by this Committee, staff will be providing this report to Comox staff as a response to the submitted referral for 1966 Guthrie Road.

Citizen/Public Relations

Any public correspondence received by the CVRD regarding the proposed development will be provided to Comox for their consideration. The proposal was also sent to the AAPC and the Area B APC. Both advisory planning commissions were supportive of the proposal due to the landscaped buffer between the building and the farming properties, a commitment from the developer to have tenants acknowledge in writing the differences that living next to an active farm operation can come with (e.g. odour, noise), and that more housing options are needed throughout the Comox Valley.

Attachments: Appendix A – Design Rationale for 1966 Guthrie Road

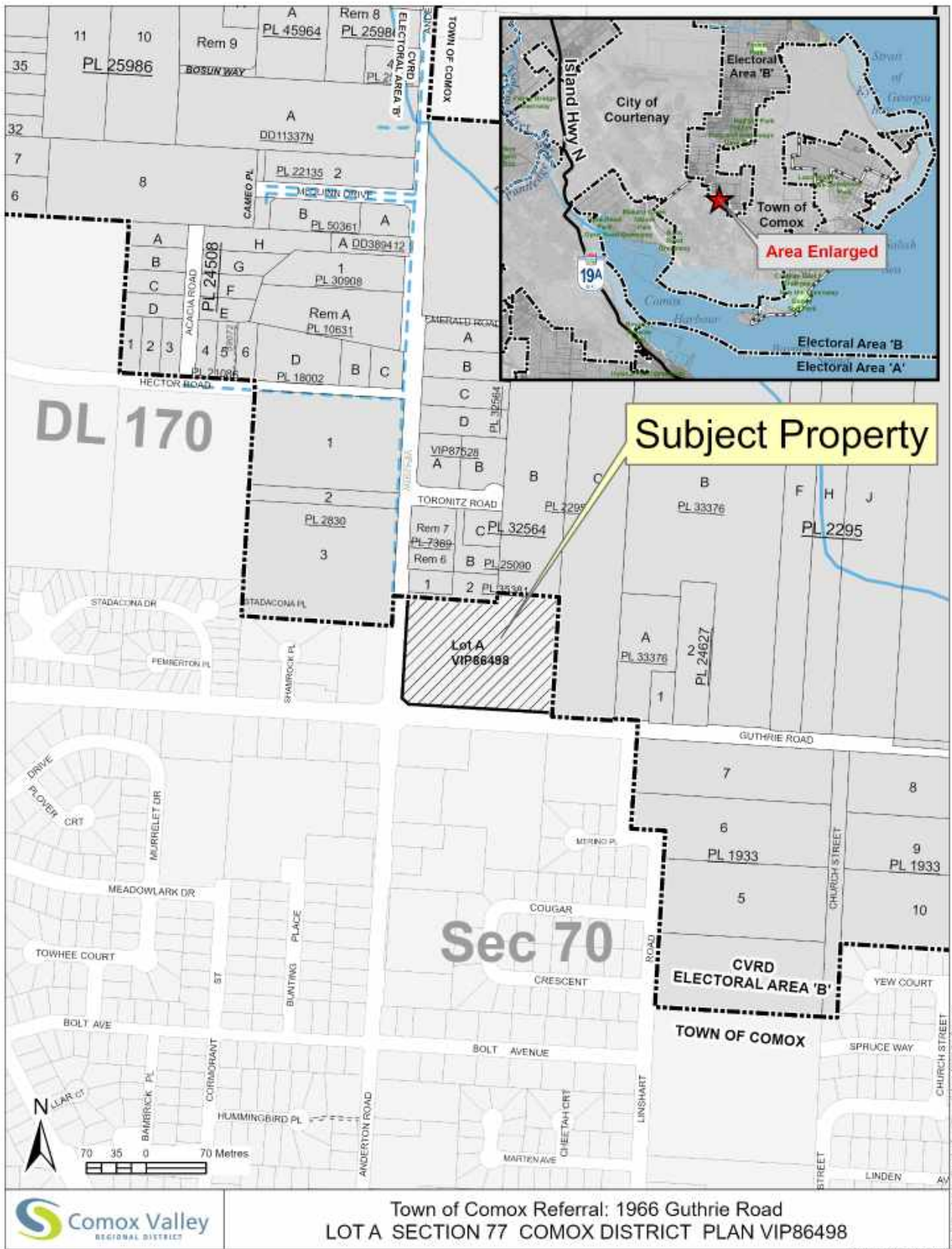


Figure 1: Subject Property Map

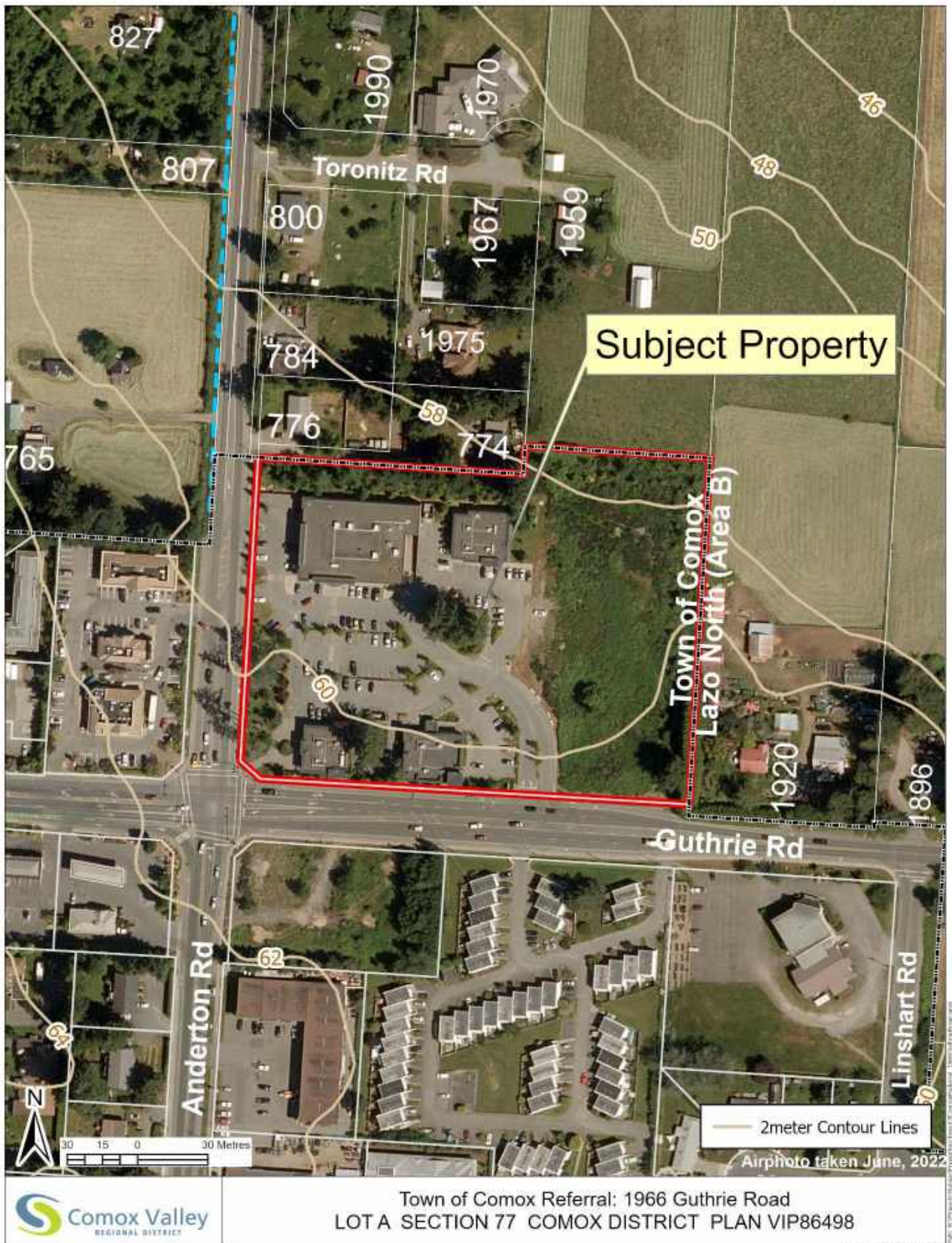


Figure 2: Aerial Image



Figure 3: Rendering, Viewed from Guthrie Road (from South East)



Figure 4: View from Proposed ALR Buffer (from North East)

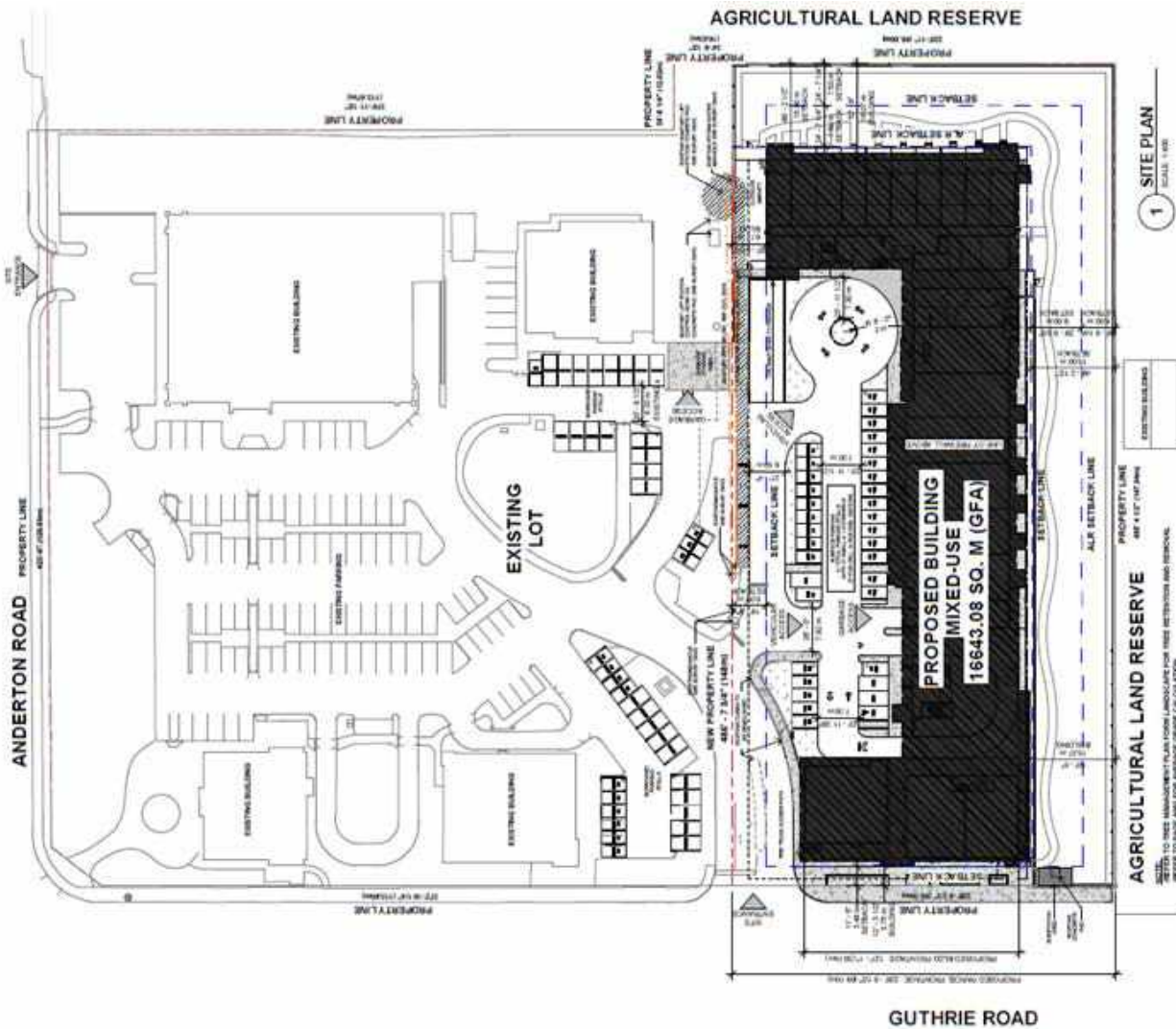


Figure 5: Site Plan



Aug 31st, 2023

DESIGN RATIONALE

1966 Guthrie Road Mixed Use, Comox, BC

Project: #21005

The Design team of this project respectfully acknowledges that the land on which we propose to build these homes is on the Unceded traditional territory of the K'òmoks First Nation, the traditional keepers of this land.

WA Architects Ltd.

| PRINCIPALS

Barry Weih
ARCHITECT AIBC, AAA,
SAA, LEED® AP

Neil Banich
DESIGN DIRECTOR

Joel Smith
ARCHITECT AIBC, AAA, SAA

David Echaiz-McGrath
ARCHITECT AIBC, AAA, SAA

| STUDIOS

VAN

950 — 1500 W Georgia Street
Vancouver, BC V6G 2Z6
604.685.3529

VIC

104 — 3212 Jacklin Road
Victoria, BC V9B 0J5
250.391.4933

wa-arch.ca

We are building communities.

Site Context

This proposed mixed-use residential development is located at the intersection of Anderton Road and Guthrie Road in the Township of Comox. Located between agricultural lands (ALR) along the North and East property lines and Highland Village shopping centre immediately to the West, it is a short distance from many of the Township's shops, recreational facilities, businesses, schools and parks. Multi-family and single family lots populate the areas South and South West of this property. It has a great walkability score and is immediately adjacent to dedicated bike lanes and public transit. This makes it an ideal location for residential development.

Land Use

The current use of the lot is commercial. There are 4 existing CRUs on Anderton roadside (Shoppers, Subway, Bank, Medical / Care buildings), and a vacant area on the other side. Our development takes place in the vacant area and offers a mixed use residential commercial program. A subdivision is proposed to make a clear separation between the existing and the proposed programs.

Adequate infrastructure

With the help of a team of qualified professional, we established that the existing utilities, routes, and walkways were suitable for our proposal.

Project Statistics and Programming

Highland Village Residence will consist of one six (6) storey mix-use building containing 205 purpose-built rental units over 15,000 sf of ground floor commercial space. This development proposes approximately 140,000 sf of residential area, comprised of 18% Studio, 54% 1-bedroom, 12% 2-bedroom, 7% 2-bedroom plus den, 4% 3-bedroom, and 5% Live/Work Lofts. Parking for both commercial and residential components will be provided through a combination of surface and below-grade structured parking. A 15m (49 ft) landscaped buffer separates the proposed building from the agricultural lands along the North and East facades.

The organization of commercial and residential uses was carefully considered. Commercial retail use was located along Guthrie Road for street-front activation while the upper residential floors are set back to visibly reduce massing and keep the frontage at a pedestrian scale. The ground floor residential components would be located along the quieter North side of the development away from traffic.

We chose to support an active lifestyle by encouraging bike-use and showcasing two large secure bicycle centres prominently adjacent to the main residential lobby. No more small bicycle storage in the furthest corner of an underground parkade.



Commercial and residential uses are further separated by a wide breezeway connecting the residential lobby with the lush, landscaped buffer along the East property line. Entrance to the underground parkade is located at the North West corner of the building adjacent to one of the building's indoor and outdoor amenity spaces.

To encourage a café or coffee shop, a prominent corner retail space along Guthrie was equipped with drive-through service and an outdoor patio space. The drive-through was strategically located to minimize visual impact to the neighbourhood and future residence of this development.

A generous 15m landscape strip along the North and East sides of the property provides a significant buffer to shield agricultural lands from this proposed development. This landscaped buffer is divided into two zones; a 6m (20ft) restricted area and a 9m (29 ft) zone designed with an undulating gravel path connecting Guthrie to the North facing live/work loft units. We have provided 2,450 sq. ft of outdoor amenity space that will accommodate BBQ areas and a community garden. Additional indoor and outdoor amenities are provided on the second floor above the commercial units along Guthrie Road.

Amendment and Variances requested.

Parking Amendment (Toc Zoning bylaw 1850 – 6.15)

The proposal meets the residential 177 permanent residential parking stalls in a secured underground parking, using the bylaw reduction of 28 stalls with providing all the minimum bike stalls required. To meet the residential parking visitor need, the proposal offers 11 surface parking stalls, and is asking for an amendment for the 40 remaining stalls to be able to share with existing retail complex in parking excess, through a parking covenant. The proposal meets the commercial/retail parking requirements with 27 surface parking stalls and one loading stall.

Building height Variance (Toc Zoning bylaw 1850 – 305.8)

To accommodate a six-storey mixed use development, our proposed max building height is 24m and would require a variance. The proposed building height is limited to 10m in the current zoning.

Servicing Variance (ToC Subdivision & Development servicing Bylaw 1261)

We worked closely with a qualified team of civil engineers to determine that the existing utilities were suitable for both existing and proposed programs. Our proposal is requesting a variance to allow the existing servicing to be shared between the two sites after subdivision.

Form and Character

To be consistent with the Land Use change, our proposal requires a change of Development permit zone from DPA #5 (commercial: neighbourhood) to DPA #4 (Mixed use: commercial / residential). Our design complies to the DPA #4 requirement.

We chose a West-coast Contemporary aesthetic with large varying sizes of balconies, generous roof overhangs and a modern palette of colours and material. From the early stages of design, we recognize the need to reduce the visual massing of this proposed structure. We started with a series of solid ground floor elements (commercial arches) that would march along the length of the building creating solid and void spaces. This served to emphasize the commercial programming, create a covered shopping experience, and separate the upper residential use from the predominantly commercial pedestal.



By stepping the second level back from the brick base, it allowed us visual separation of the upper floors and provided a continuous privacy screen for the residential units above. This technique was further utilized at either end of the building to present a pedestrian scale façade and reduce the overall size of the building along Guthrie Road and at the indoor amenity space along the North end of the building.

By introducing white residential blocks of varying width and height, we were able to breakdown the length of the building and create relief along its façade. Inset stacked balconies juxtaposed against cantilevered random ones served to further create the illusion of separate buildings and a unique skyline running across the length of this structure. The white element only breaks through the brick commercial base to the ground where we identify the residential character of lobby. The top floor is also set back to further reduce the impact of the six floors.

We identified any area we could to add planters and landscape nodes so as to provide visual interest and break up the continuity of the surface parking areas. A round-about driveway with central planter and tree identifies and separates the residential portion of this development from the commercial.

The design team held our first Public Information Meeting back in January 25th, 2021 and a second one in March 29th, 2023. We heard your comments and have responded as follows:

Views, Noise and Privacy Concerns – being surrounded by ALR and landscape buffer along two sides and a commercial development and Guthrie Road on the other two has allowed for significant separation between this proposed development and all residential developments around. Additionally, we have orientated the building massing so that the smallest cross section of the building faces directly onto Guthrie Road.

Parking and Traffic Concerns – We are working with WATT Consulting Group to analyze parking and traffic requirements/patterns and provide alternate means (bicycle use, public transit) to reduce the impact of the added density.

Protection of Environment/Farm/Water – We are working closely with Grade Consulting Inc. (Civil Engineers) and Mystic Woods Landscape Design to have a comprehensive storm management strategy to retain and control rainwater runoff and ensure the development service demands on the township's infrastructure meets all required levels and rates.

Community Benefits amenities

20 units (10% of the total proposed) have been designed to meet the city adaptable housing standards. An Affordable housing letter is submitted with the application to outline the measures provided. This project is also being design along Step 3 Sustainability requirements to ensure we meet responsible environmental mandates.

Regional growth context strategy

This community is growing. Studies and reports show the need to construct many new homes in the coming years to meet the demand of a vibrant economically strong centre, including the workers and families of CFB 19 Wing Comox who are challenged to find appropriate housing close to the Base. We welcome your feedback and look forward to working with you and the Township to create homes and places of business.

WA Architects Ltd.



ZONING AMENDMENT / DEVELOPMENT PERMIT For 1966 Guthrie Rd – Phase 3

APPENDIX 6

PRE-APPLICATION EMTERRA ENVIRONMENTAL DEVELOPMENT REVIEW FORM

Instructions to the Development Permit Applicant:

- Provide this form and a to-scale or fully dimensioned copy of the current site plan for the proposed development showing the location and accessibility of the waste storage area that Emterra will be asked to service. Widths of driveways, dimensions of turning areas, height of building over-hangings that may impede waste removal vehicle access must be provided.
- If the development proposal changes in any way that may affect the ability of Emterra to service a plan previously accepted by Emterra, it is the Applicant's responsibility to obtain a follow-up review from Emterra, citing the revision number and date of any such revised site plan, and to re-submit this form to the Planning Department.

Contact Information for the Town's waste management contractor:		
Emterra Environmental	Dave Ross Phone: (250) 336-8066	cumberland.admin@emterra.ca

Developer: Include this completed form with the Development Permit Application package.

The following is to be completed by Emterra Environmental:

In regard to providing waste removal services for the proposed:

- Residential
- Commercial
- Mixed use residential-commercial
- Industrial

development located at 1966 Guthrie Road in Comox,

we have reviewed Site Plan Drawing No. 21005 Project dated 27-07-2023 (day/month/year), revision no. _____ prepared by _____

_____ and find that:

- we are able to service the proposed development
- we are not able to service the proposed development

Additional comments:

As long as the ground is level to pull the FE bins across the 3yrd bins on castors will be serviced from both locations listed on drawing.

Dave Ross July 27, 2023

1809 Beaufort Avenue | Comox, BC | V9M 1R9 | Planning Dept. Ph: 250-339-1118 Fax: 250-339-7110 | www.comox.ca

From: [KFN Cultural Heritage Investigation Permits](#)
To: [Robin Pallett](#)
Subject: Re: OCP/RZ 23-4: referral from Town of Comox
Date: December 7, 2023 1:20:50 PM
Attachments: [image001.png](#)
[image003.png](#)

Warning This E-Mail originated from outside The Town of Comox. *Please open with Caution*

Hello Robin,

Here are they key points of a Chance Finds Procedure.

If potentially cultural sediments or features are identified the following procedures should be immediately implemented:

1. STOP WORK that could cause additional damage to the site deposits;
2. Inform on-site supervisor/proponent/town representative and contact KFN (KFN Contract Archaeologist Raini Bevilacqua; 250-339-4545 ext. 120/archpermits@komoks.ca)
3. Record location of the find;
4. Retain potentially archaeological sediments on-site (i.e., do not release dump truck if loaded with fill from the suspected location);
5. Site supervisor is to contact the a qualified archaeologist to be retained, if needed based on KFN assessment;
7. Use marker to indicate retained sediment that may contain archaeological materials; the location of the Chance Find should be encircled with flagging or temporary fencing to ensure its protection;
8. If possible, be prepared to initiate work at another location while archaeological testing and/or mitigation is conducted; work cannot be continued once archaeological material is uncovered without permits from the B.C. Archaeology Branch.
9. If human remains are encountered or suspected, the site supervisor should contact the RCMP depending on the location and nature of the discovery;

10. Treat human remains, regardless of age or condition, with utmost respect at all times; cover remains with a blanket or other readily available cover.

The common types of Archaeological sites can be found and are described in our Cultural Heritage Policy: this table is helpful as well.

Table 1. Cultural Heritage Site Types and Required Mitigation Measures.

Site Type	KFN Required Mitigation Measures
Shell Middens	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
House Site	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Fire-Cracked-Rock middens	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Cemeteries / Burials	No impacts; if cannot be avoided, impacts will require KFN consent and will maximize data recovery
Lithic Sites	Maximize data recovery
Rock Art	No impacts
Fortifications	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Fish Traps	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Clam Gardens	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Earth Ovens	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Culturally Modified Trees (CMTs)	Avoid; if cannot be avoided, maximize data recovery
Wet Sites	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Cultural Depression	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Canoe Runs	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology. 50 m cutting buffer around sites.
Trails	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Rock Shelters	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology
Petroforms	Avoid impacts; if cannot be avoided, maximize data recovery, compensatory archaeology

If you would like me to write up a full document aligned with KFN's Cultural Heritage Policy, please let me know and I work with my department and come up with a cost estimate and timeline.

Chance find procedures can also be commissioned from archaeological firms on a project by project basis.

If you would like a walkthrough of our CHIP Permitting Process or our policy please let me know and I can schedule a meeting.

Raini

-



Archaeology & Referrals Department
[K'ómoks First Nation](#)
3330 Comox Rd, Courtenay, BC
P: (250) 339-4545 ext. 120
Follow us on [Facebook](#) and [Instagram](#)

tuwa ak^ws χοχολ ?a x^w yιχμεtet (?a) k^woms hēhaw tōms gļē

"Care takers of the 'land of plenty' since time immemorial", ?aγājuθəm (eye-uhh-juu-eth-em Island Comox)

About K'ómoks First Nation

The K'ómoks First Nation is located in the heart of the Comox Valley on Vancouver Island. Membership is currently just over 300 members within four clans: Sahtloot, Saseetla, leeksun and Puntledge. Two cultures are identified in our community: Coast Salish (Island-Comox speaking peoples) and Kwakwaka'wakw (Kwak wala speaking peoples). K'ómoks originally occupied sites in Kelsey Bay, Quinsum, Campbell River, Quadra Island, Kye Bay and along the Puntledge Estuary.

Disclaimer

This email and any files transmitted with it are confidential and intended solely for the use of the individual or entity to whom they are addressed. If you have received this email in error, please notify me and follow with its deletion. Please note that any views or opinions presented in this email are solely those of the author and do not necessarily represent those of The K'ómoks First Nation. Finally, the recipient should check this email and any attachments for the presence of viruses. The sender accepts no liability for any damage caused by any virus transmitted by this email.

From: [Kelly, Brendan MOTI:EX](#)
To: [Robin Pallett](#)
Subject: RE: 1966 Guthrie Rd: Town of Comox applications OCP/RZ 23-4, SD/DP 23-5, DVP 23-2: external app referral
Date: September 27, 2023 10:00:41 AM

Warning This E-Mail originated from outside The Town of Comox. ***Please open with Caution***

MoTI File 2023-04587

The Ministry of Transportation and Infrastructure has no objections to the proposed OCP and Zoning Bylaw Amendment.

Brendan Kelly
Senior Development Services Officer
Ministry of Transportation and Infrastructure
Vancouver Island District
250-331-9903

From: Lise.Smith@forces.gc.ca
To: [Robin Pallett](#)
Subject: 036-R-1966 Guthrie Rd: Town of Comox applications OCP/RZ 23-4 SD/DP 23-5, DVP 23-2
Date: October 16, 2023 3:51:39 PM

Warning This E-Mail originated from outside The Town of Comox. ***Please open with Caution***

Hi Robin,

So sorry for late reply. **DND has no issues with the project as presented.**

BACKGROUND:

We had informed the developer in January 2023, that a 4-storey building on this site would contravene the air space at 19 Wing. There has been some movement on the ever-changing situation regarding construction heights and impacts on operations at CFB Comox. The Ministers Office and the Royal Canadian Air Force are making great strides in re-writing the Comox Airport Zoning Regulations. **The time horizon could be as long as 5 years**, however, we currently have approval from the Minister's Office to register "specific" land sites. These sites will be included in an "exemption area". It's the best we can do to satisfy the immediate need for development in the area.

What this means; these sites are in a very specific area of the current zoning restriction area. The Window for the "exemption area" will close, however all developers and cell tower companies that filter the development plans via the Real Property Office will be considered. I want to emphasize that they will be considered and not necessarily approved. Therefore, it's important to continue to provide my office with all developers proposals. The covenants on **ALL land title** documents within the 4km radius of the aerodrome will remain in affect therefore NO CHANGE to the Land Title Document. It's important to filter the applications through my office for them to be looked at on an individual basis.

If you have any questions, please contact me directly.

Regards.
Lise M Smith

Real Property Officer (RPO),
Department of National Defence / Government of Canada
lise.smith@forces.gc.ca / D365: lise.smith@ecf.forces.gc.ca / T:250-339-8211(8496)
C:236-255-1520



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navcanada.ca

January 17, 2024

Your file
21005 - 1966 Guthrie Road Mixed Use, Comox, BC
Our file
23-3699

Robin Pallett
Town of Comox

**RE: Development Proposal/Plans: Building(s) - Comox, BC
(N49° 41' 20.45" W124° 55' 57.27" / 78.7402' AGL / 273.4909' AMSL)**

Hello,

NAV CANADA has evaluated the captioned proposal and has no objection to the project as submitted. Our assessment does not constitute an approval and/or permit from other agencies.

The subject proposal data have been distributed to External Design Organizations (EDOs) for their assessment of possible effects on procedures they maintain. They will contact you directly if any concerns arise during their evaluation. If you have any questions or concerns pertaining to their assessment, please contact the EDO directly.

In the interest of aviation safety, it is incumbent on NAV CANADA to maintain up-to-date aeronautical publications. To assist us in that end, we ask that you notify us at least 10 business days prior to the start of construction. This notification requirement can be satisfactorily met by returning a completed, signed copy of the attached form by email at landuse@navcanada.ca or fax at 613-248-4094. In the event that you should decide not to proceed with this project or if the structure is dismantled, please advise us accordingly so that we may formally close the file.

If you have any questions, contact the Land Use Department by email at landuse@navcanada.ca.

NAV CANADA's land use evaluation is based on information known as of the date of this letter and is valid for a period of 18 months, subject to any legislative changes impacting land use submissions. Our assessment is limited to the impact of the proposed physical structure on the air navigation system and installations; it neither constitutes nor replaces any approvals or permits required by Transport Canada, other Federal Government departments, Provincial or Municipal land use authorities or any other agency from which approval is required. Innovation, Science and Economic Development Canada addresses any spectrum management issues that may arise from your proposal and consults with NAV CANADA engineering as deemed necessary.

This document contains information proprietary to NAV CANADA. Any disclosure or use of this information or any reproduction of this document for other than the specific purpose for which it is intended is expressly prohibited except as NAV CANADA may otherwise agree in writing.

Regards,

Land Use Office
NAV CANADA

cc PACR - Pacific Region, Transport Canada (ATS-23-24-00028995)
CYQQ - COMOX
CAH3 - COURTENAY AIRPARK
CBV8 - COMOX (COMOX VALLEY HOSPITAL)(HELI)
Direct Approach
Lise.Smith@forces.gc.ca
ron.maclellan@rcmp-grc.gc.ca

From: Ian Heselgrave <Ian.Heselgrave@sd71.bc.ca>
Sent: Friday, November 3, 2023 4:46 PM
To: Regina Bozerocka <rbozerocka@comox.ca>
Cc: Jeremy Morrow <jeremy.morrow@sd71.bc.ca>; Russell Horswill <Secretary.Treasurer@sd71.bc.ca>; Molly Proudfoot <Molly.Proudfoot@sd71.bc.ca>
Subject: RE: Town of Comox referral - proposed Official Community Plan amendment

Warning This E-Mail originated from outside The Town of Comox. *Please open with Caution*

Hello Regina,

In 2020 the former SD 71 Secretary Treasurer (Nicole Bittante) submitted a letter responding to the long term build out of Comox proposed in the last round of OCP review that detailed the requirement for a school site in Comox. The information contained in the attached letter remains relevant.

The two proposed developments will result in a significant number of students entering the Aspen Park Elementary catchment and the Highland Secondary catchment. The influx of students (more than 100 anticipated and split between elementary and high school) will pressure the already overfull elementary school. The most current student yield information for families living in the Town of Comox demonstrates that one student is added to the school system for every six row homes/duplexes (multi-family dwellings) constructed. This rate drops to one student for every 29 apartments units in a building and increases to one student for every five new single-family homes constructed. These proposed developments will undoubtedly be a mix of multi-family, single family, and apartment style dwellings. More assurance on student number growth will be derived when more details are received regarding the proposed development.

The bottom line is that these developments will result in a substantial increase in student numbers and more school space will be required. The Town should look to identify and secure future school space for the School District to meet this upcoming and future demand on the west side of Anderton road. The details of the school capacity and the land required are detailed in the attached letter.

Best regards

Ian

Ian Heselgrave
Director of Operations
Comox Valley School District
<http://www.sd71.bc.ca>

Phone: 250-334-5516
Email: ian.heselgrave@sd71.bc.ca

ATTACHMENT 6

APPLICANT'S RESPONSES TO COMMENTS RECEIVED FROM EXTERNAL AGENCIES

3rd PARTY AGENCY REFERRAL RESPONSES

Comox Valley Regional District

1. Awaiting response

Response: 1 Information acknowledged.

K'ómoks First Nation

1. KFN Cultural Heritage Inspection Permit (CHIP) is not required.
2. Request that a 'chance find procedure' be put into place prior to commencement of construction works.

- a. **Staff Comment:** Comox staff are awaiting confirmation regarding the details and scope of a 'chance find procedure'.

Response: 1, and 2 a) Information acknowledged.

SD71 (School Board) – (responding to two separate referrals in a single response)

1. **Staff Comment:** All Comments provided by SD71 are directed at the Town and discuss long-range planning of schools in the area. SD71 has not provided any comments that relate to the proposed development itself.

Response: 1 Information acknowledged.

SD71 (Francophone School Board)

1. Awaiting response

Response: 1 Information acknowledged.

19 Wing/DND

1. We had informed the developer in January 2023, that a 4-storey building on this site would contravene the air space at 19 Wing. There has been some movement on the ever-changing situation regarding construction heights and impacts on operations at CFB Comox.
2. The Ministers Office and the Royal Canadian Air Force are making great strides in re-writing the Comox Airport Zoning Regulations.
 - a. The time horizon could be as long as 5 years, however, we currently have approval from the Minister's Office to register "specific" land sites.
 - b. [1966 Guthrie Road] will be included in an "exemption area".

Response: 1, 2 a) and b) Information acknowledged.

Transport Canada & NavCan

1. Both are seeking details regarding cranes (for construction).

Response: 1 Construction crane would likely be 120 feet high at highest point.

Brooklyn Creek Watershed Society (c/o Comox Valley Land Trust)

1. To safeguard the Creek [from potential high-volume stormwater discharge with accompanying pollutants generated by the proposed development], we are strongly recommending that the Town makes the following condition of approval: that the developer retains a properly-qualified consultant with fisheries and hydrological expertise to complete a comprehensive investigation of the plan for stormwater management.
 - a. The report should identify measures to mitigate any damaging effects of the development.
2. The developer must commit to fully implement measures to protect Brooklyn Creek from any adverse effects from the construction and occupancy phases of the development
3. As a minor comment, we appreciated the developer's efforts to include many native plants, as indicated in the landscape plan.
 - a. We note, however, that one of the non-native species included (*Pieris japonica*) has not been thriving in this area, and the consulting landscape architect might be asked for a substitute that would do better in the Comox Valley.

Response: 1 to 3 Client to engage a consultant with fisheries and hydrological expertise.

BC Hydro

1. BC Hydro has a Statutory Right-of-Way (SRW) registered against this property which allows BC Hydro to install, replace, maintain, and access works related to our electrical delivery system. Please read the [Rights-of-Way Guidelines](#) and call our Right-of-Way Helpline at 1-800-667-1517 to find out what restrictions apply to your development.
2. Modifications to the BC Hydro system will be necessary to extend the electrical system to this development. BC Hydro will need to complete a thorough system study and design to provide a cost estimate for this work.
 - a. The developer must submit an application for service with our BC Hydro Express Connect Department by phone at 1-877-520-1355 or [online](#). Approximate design and construction lead times will be provided at the time of application.
3. There is overhead electrical infrastructure adjacent to this property which may affect this development. It is the developer's responsibility to ensure that any infrastructure built on this property will meet all current CSA requirements and limitations around high voltage equipment and transformation.
4. Normally, BC Hydro will not relocate existing BCH plant. If the developer wishes to have BC Hydro relocate its plant to meet CSA requirements, an application must be submitted to the local BC Hydro District Office to conduct a feasibility study of a proposed redesign. The developer will be fully responsible for all associated redesign costs.
5. In addition, please review the following links regarding [vegetation guidelines](#) (if applicable).

Response: 1) to 5) Information acknowledged. Client to start application process for primary services connection with BC Hydro and follow the right-of-way guidelines.

BC Transit

1. BC Transit is satisfied with the current and planned infrastructure that the site provides for pedestrians accessing transit.
2. There are two stops with the basic amenities within walking distance from the site with easy access by sidewalk to commute there. The routes accessible are the 1, 3, and 99 which provide great access to the majority of Comox.

Response: 1) and 2). Information acknowledged.

Canada Post

1. This development will require developer supplied lockboxes, parcel boxes, mailroom and we will need building access all to Canada Post Delivery Standards.
2. If approved, we request the developer contact us in advance to confirm all required standards and plan mail delivery within the development.

Response: 1) and 2). The Design team will follow the Canada post requirements for the building permit stage.

Comox Valley Accessibility Committee

1. there is only 1 accessible parking space for a total of 35 visitor parking (including the commercial units). This is not enough parking even for just the commercial units (8-9 including a cafe) This visitor parking is shared with the entire complex. If a working bus route/ public transit that captured the entire Town of Comox, serviced this area you might be able to get away with two for the commercial units alone.
 - a. The demographics of the Town suggest this is an inadequate amount of parking for the visitor (including commercial) potential. I applaud the tremendous emphasis on biking (including bike repair and washing!) - will there be room for hand-cycles?

Response: 1) We are compliant with the accessible parking requirements from section 6.9 of the Zoning Bylaw. We added 3 more accessible stalls and we are proposing 12 in total.

2. the underground/ tenant parking has a total of 8 accessible parking stalls out of 177 (for 205 residential units) Unless there is some viable public transit option, this is wholly outside the realm of reality in the Town of Comox. 8 units of accessible parking is not addressing the demographic of the Town of Comox.
 - a. According to the census of 2021 a little over 42% of the population of the Town of Comox is 55 years of age or older. 20.5% of the BC population aged 15-64 is living with a disability. The statistic is higher for older populations. Unfortunately I could not find the number for the Town of Comox itself.
 - b. Statistically, the demographic of Comox would encourage accessible consideration at least at 20% of parking stall requirement. Of 35 parking spaces would be 7 spaces. 20% or 177 would be over 35 spaces.
3. there is a delightful exterior pathway that navigates to the residential units "F1" . Each of these units is accessed via steps/ stairs. To create an accessible and more importantly - visitable- space, 'cart path with limestone crush' could be used in place of gravel to create an accessible pathway. The steps would be mitigated with sloping ramps.

Response: 3) There is an accessible entrance to the path from the outdoor amenity and from the

breezeway 3 to the exterior pathway. Path in ALR will be built with limestone crush (or equivalent) for full access.

4. There are two elevators to accommodate the daily access to 205 units. Are these double elevators, single elevators? I got a little confused as 'stair A' moves on the second story upward... all good now. Are there security features in place for the underground parking as there are at least four access points from the ground level into the area.
5. There are 10 adaptable units - nice start! Thanks for locating the units near the elevator.
6. Will the balconies of the adaptable units be zero barrier? Also, all the adaptable units are one bedroom.. will there be possibility of adaptation for larger units to house families? As well the outdoor amenity areas (how lovely!) ..will the access to the areas be zero barrier?

Response: 6) The outdoor amenity areas will be zero barrier. For the adaptable units we will follow the city requirements.

7. I really like the infill of vegetation and trees (or so it appears) to bring nature into the environment.
8. the drive through is generally situated to avoid the entrances to the CRU units and is broken up with vegetation. This is a departure for the Town of Comox since around 2009.. nonetheless, a painted crosswalk between the residential walkway (near the accessible parking) and the breezeway to the cafe would mitigate the possible problems of 'pedestrian traffic' across the drive through path. This is true also at the entry point to the drive through..
 - a. There may be some tricky moves for parked cars to leave especially the accessible stall and the stalls to the west across the way as they back into the drive through lane. I am assuming that the greyed out area is cement sidewalk, will there be curb cuts to accommodate mobility devices at all transition points?

Response: 8) Painted crosswalks and let-downs have been added to the parking layout.

9. I am assuming vehicles enter and exit via the ramp at the north west end ..

Response: 9) There is an exit at Guthrie Road and another exit at Anderton Road.

10. Having the elevator again close to the accessible parking stalls is great. I could not see the entry to the elevator and assume it is within the vestibule.

Comox Valley Food Policy Council

1. **Increased protection of the Agricultural Land Reserve Buffer:** This property is farm-adjacent, backing onto ALR land on the North and East sides of the property. While the applicant has proposed a 15m landscaped buffer which “provides a significant buffer to shield agricultural lands from this proposed development”, we request this be revisited and additional protections be taken. We recommend the applicant consider moving the parking lot to the back of the building to reduce noise travel and further increase and protect the proposed buffer.

Response: 1 This buffer has been established a long time ago, pushing it out any further will limit us to the capacity to build this development.

2. **Increased permeability/drainage functions of paved area:** We suggest the applicant look at permeable alternatives to standard paving, particularly in the parking lot areas. More paving leads to more runoff, especially in the case of the proposed development sitting directly adjacent to farms. The current proposal could result in flooding and introduce contamination onto farming land via runoff. Additionally, we want to recognize that the farmer on the neighboring property has voiced concerns about the water impacts on the existing well.

Response: 2 We used all opportunities to increase permeability and drainage functions and

reduce the paving area, even we are proposing off-site parking to meet the parking requirements to alleviate the site permeability.

3. **Decreased size of paved area:** Another recommendation to mitigate runoff and potential flooding and water contamination is to reduce the applicant's parking requirement. Reduction in parking spots should not reduce the desirability of the development, especially given the development's proximity to an active bus route and the area's "great walkability score and ... [adjacency] to dedicated bike lanes". The applicant states that they "support an active lifestyle by encouraging bike- use and showcasing two large secure bicycle centres": reviewing the applicant's parking statistics, there seems to be an opportunity to further increase the number of bicycle parking spaces, thereby reducing the amount of paved parking spaces required.

Response: 3 Please refer to answer from item #2 above.

4. **Increased food growing opportunities:** Reducing parking areas also creates an opportunity for a larger community garden space. Members of our community moving into these new developments may be losing access to their food-growing spaces, creating a gap in accessibility. We recommend additional community garden plots be added to the landscaping plan to close this gap. A potential location we identified for these plots is at the back of the building within the ALR buffer on the North and/or East sides of the property. There is also an opportunity for green space on rooftops, be that for food growing or environmental benefits such as reducing air pollution and reducing the urban heat island effect. We also encourage incorporating traditional indigenous plants into landscaping plans and recognize that the applicant has made an effort to do so.

Response: 4 Additional vegetable beds will be provided in the ALR buffer to increase on-site food production, pending Town of Comox approval. The Landscape plan proposed additional beds in ALR (dashed) in the proximity of the Community Garden.

Telus

1. No major concerns.
2. Telus has existing conduit and if developer would like to change the route then relocation is required.
3. The existing conduit permit is attached for your reference [on the following page].

Response: 1,2 and 3 Information acknowledged.

ATTACHMENT 7
HOUSING NEEDS REPORT (MAY 2020)



TOWN OF COMOX
Housing Needs Report
Data Results

May 2020

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WHAT TO EXPECT

The following report is result of the collection, consolidation, and analysis of multiple datasets prescribed by British Columbia's Housing Needs Report Regulation, approved April 16, 2019 as part of the *Local Government Statutes (Housing Needs Reports) Amendment Act, 2018*, S.B.C, c.20. Each report section is meant, where possible, to provide a summary of local trends, as well as discussions on notable findings. Comparison's to the Comox Valley Regional District (also referred to as Comox Valley or CVRD) and the Province of British Columbia (BC) are made to provide context for how the community relates to larger geographies.

Although the report aims to maintain consistency in the data it shares and analyzes, there are some notable considerations to keep in mind:

- (1) In order to provide tenure specific information (i.e. owner and renter persons and/or residents), the report had to use the custom Statistics Canada dataset generated on behalf of the Province. When compared to the aggregate data on the Statistics Canada website, the reader may notice discrepancies; particularly, for total populations. Accordingly, the report puts added emphasis on percentages when discussing trends or making cross-geographical comparisons.
- (2) Notwithstanding consideration (1), those sections that refer solely to the total population or total households (e.g. historical and anticipated), without reference to owners or tenures, use data acquired directly from Statistics Canada and not the custom dataset.
- (3) Between the 2006, 2011, and 2016 censuses, the Town of Comox's boundaries have changed, causing issues when comparing across time. Although historical comparisons can be made using percentages/proportions, the discrepancies can have considerable impact on population projection dependability. Accordingly, the projection model required estimations. Calculating these estimates involved the addition or subtraction of Dissemination Area (DA) data from the community total, adjusted by the proportion of land within that DA that was actually added or subtracted. The result is a 2016 community boundary applied to both 2006 and 2011, where necessary.
- (4) Both traditional Statistics Canada data and the custom dataset may have small discrepancies between its data categories for populations or households. The differences are due to statistical rounding within each individual category, which may result in those categorical sums differing from others.
- (5) Rental rate statistics reflect the average rent that is paid among all units in the market. In locations where rents are increasing, it is typical that asking rents for currently available (vacant) units are higher than average market rents. Occupied units may trail these asking rents for a variety of reasons: market changes since the lease contracts were executed, legislative controls on rental increases for existing tenants, the introduction of newly completed (more expensive) dwellings into the pool of available units, landlords applying less aggressive rent increases to current tenants to reduce unit turnover, etc. Therefore, rental statistics in this report likely understate the rents that households currently looking for rental accommodation would have to pay. CMHC does track the difference in rents between vacant and occupied units, but only for larger markets. The closest location for which data is available is the Victoria Census Metropolitan Area. The difference in rents between vacant and occupied units can vary significantly by unit type and location, in Victoria's submarkets this difference can vary from a 2 to 45 percent. Over the entire market, rents in Victoria are 20% higher in vacant units, compared to occupied.

Report discussions attempt to bridge data from separate sections where appropriate and/or possible. As such, it is important to consider the document as a whole and not solely as its individual parts. To understand how the Town of Comox compares to its neighbouring municipalities and electoral areas, please refer to Regional Housing Needs Profile for the Comox Valley Regional District, found at the beginning of this report.

TABLE SUMMARY OF FINDINGS

British Columbia's Housing Needs Report Regulation requires that a summary form be completed and submitted to the Ministry of Municipal Affairs & Housing. The collection of charts below reflects those requested data points, which can be found and discussed in greater detail within the report. For a glossary of definitions related to terms used throughout the text, please see page 104 of the Regional Report.

Data Collection Summary Form

Population			%Δ since 2016			Income			Overall	Owners	Renters
2016 census	14,020		-			Comox	\$69,254	\$76,595	\$46,762		
2020 estimated	14,855		6.0%			Comox Valley	\$64,379	\$73,367	\$38,394		
2025 anticipated	15,955		13.8%			British Columbia	\$69,995	\$84,333	\$45,848		
Seniors (65+)			2016	2025		Economy			Overall	Owners	Renters
Comox	29.1%		35.1%			Participation rate	53.7%	51.1%	64.5%		
Comox Valley	25.2%		32.7%			Unemployment rate	7.1%	7.6%	6.0%		
British Columbia	17.4%		23.7%			Employment rate	49.8%	47.2%	60.9%		
Median Age			2016	2025		Core Housing Need (%)			2006	2011	2016
Comox	51.0		54.8			Overall	4.9%	10.0%	7.5%		
Comox Valley	49.9		51.6			Owners	2.2%	4.5%	3.1%		
British Columbia	42.5		44.3			Renters	13.7%	30.4%	23.2%		
Households			%Δ since 2016			Core Housing Need (#)			2006	2011	2016
2016 census	6,210		-			Overall	250	585	460		
2020 estimated	6,770		9.0%			Owners	85	205	145		
2025 anticipated	7,495		20.7%			Renters	160	385	315		
Household Units (est.)			2016	2020	2025	Extreme Housing Need (%)			2006	2011	2016
0 bedrooms	25	25	30			Overall	2.2%	4.4%	3.9%		
1 bedroom	400	440	485			Owners	0.8%	2.5%	1.8%		
2 bedroom	1,500	1,640	1,805			Renters	6.9%	11.1%	11.1%		
3+ bedrooms	4,285	4,665	5,175			Extreme Housing Need (#)			2006	2011	2016
Total	6,210	6,770	7,495			Overall	110	255	235		
Household Size	2.2	2.1	2.1			Owners	30	115	85		
						Renters	80	140	150		

DEMOGRAPHY

1. Historical Population

Comox's population grew to 14,020 people in 2016, up 14.0 percent over 10 years – 1.3 percent annually. Its growth surpasses that of the Comox Valley Regional District (CVRD) and the Province; notably, due to the generally greater increases associated with being an urban community. Comox is the second largest community within the CVRD after its neighbour, the City of Courtenay.

Table Com 1.1: Historical Population, 2006 to 2016 (Statistics Canada)

COMMUNITY	2006	2011	2016	%Δ06-16
Comox	12,300	13,625	14,020	14.0%
Comox Valley	56,645	61,575	64,355	13.6%
British Columbia	4,054,605	4,324,455	4,560,240	12.5%

As is common across Canada and BC, Comox's population is ageing. Specifically, its senior population – defined as those persons at or above 65 years of age – grew 43.0% between 2006 and 2016 to 3,245 persons. This 3.6 percent annual increase is the fastest growth among age cohorts, greatly surpassing working age persons (herein defined as those aged 20 to 64 – 10.1 percent) and youth (0 to 19 – 0.5 percent). Accordingly, the proportion of seniors relative to total population is rising and is anticipated to continue as such – between 2006 and 2016, seniors grew 5.9 percent to 29.1 percent.

Table Com 1.2: Proportion of Senior (65+) Population (Statistics Canada)

COMMUNITY	2006	2011	2016	%Δ06-16
Comox	23.2%	25.8%	29.1%	43.0%
Comox Valley	18.1%	21.1%	25.2%	58.2%
British Columbia	14.0%	14.9%	17.4%	40.5%

Compared to the CVRD and BC, Comox has historically had higher rates of senior populations; however, its decade long growth is slower than the Region overall (58.2 percent in 10 years), and is about at par with the Province (40.5 percent).

2. Age

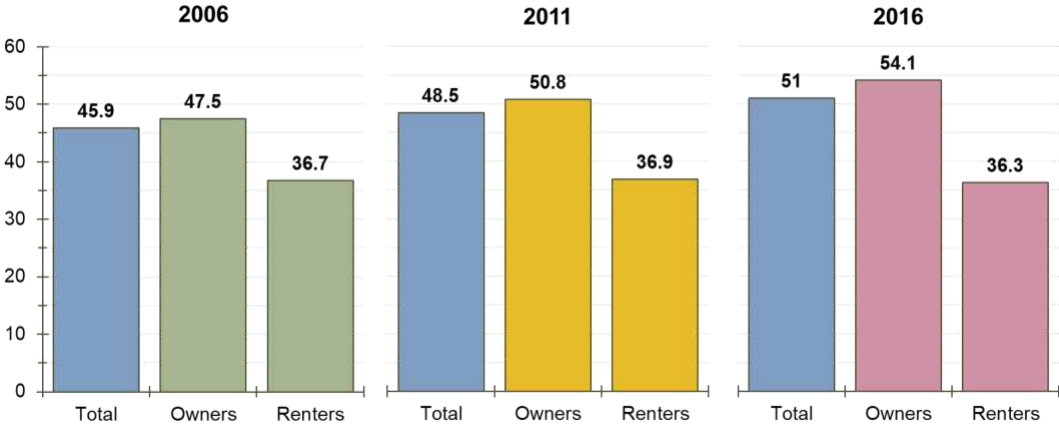
In 2016, residents between 65 and 84 grew 36.7 percent over 10 years, of which about all was attributed to owner growth. For renter residents, 52.1 percent (up 2.3 percent since 2006) were 25 to 64 years old, higher than owners at 48.0 percent. Relatedly, renters also demonstrated a greater share of people between 0 to 14 (21.3 percent), also up 2.3 points.

Table Com 2.1: Proportion by Age Group & Tenure (Statistics Canada)

	Total			'16 % of Total	Owners			'16 % of Total	Renters			'16 % of Total
	2006	2011	2016		2006	2011	2016		2006	2011	2016	
Total	11,915	13,285	13,705	100.0%	9,590	10,805	10,910	100.0%	2,320	2,480	2,795	100.0%
< 14 years	1,960	1,990	1,970	14.4%	1,520	1,545	1,370	12.6%	440	445	595	21.3%
15 to 19 years	780	795	785	5.7%	640	625	625	5.7%	130	170	155	5.5%
20 to 24 years	450	535	480	3.5%	270	395	340	3.1%	180	145	140	5.0%
25 to 64 years	6,060	6,740	6,685	48.8%	4,895	5,375	5,240	48.0%	1,155	1,350	1,455	52.1%
65 to 84 years	2,425	2,810	3,315	24.2%	2,065	2,530	2,955	27.1%	350	275	355	12.7%
85+ years	245	185	60	0.4%	425	325	105	1.0%	470	360	105	3.8%
Median Age	45.9	48.5	51.0		47.5	50.8	54.1		36.7	36.9	36.3	
Average Age	43.7	45.7	47.1		44.9	47.2	49.4		38.9	39.3	38.2	

As the population ages over time, unmatched by young migrants or births, the median age increases. Between 2006 and 2016, Comox’s median age grew 5.1 years – or 1.1 percent annually – to 51.0 years of age. Residents belonging to the “owner” tenure category have historically been older (based on the median) than their renting counterparts. Nevertheless, this is unsurprising due to the generally tendencies for home ownership to be more popular and/or accessible for older cohorts who trend towards higher incomes and investments that facilitate purchasing a home.

Figure Com 2.1: Historical Median Age by Tenure (Statistics Canada)



In 2016, the median age for owners was 54.1; whereas, renters were 36.3. Both tenure categories surpassed that of the CVRD overall and BC. However, Comox Valley’s overall median age grew about 1.0 percentage point faster than Comox (12.0 percent over 10 years); BC’s age growth was below half of Comox (4.9 percent).

Table Com 2.2: Median Age, 2016 – Comparison (Statistics Canada)

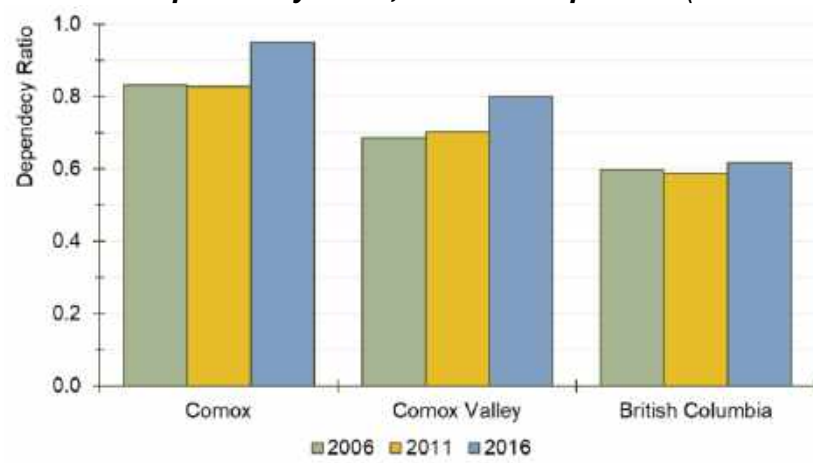
COMMUNITY	Overall	Owner	Renter
Comox	51.0	54.1	36.3
Comox Valley	49.9	53.5	34.5
British Columbia	42.5	46.5	33.8

3. Dependency Ratio

The trajectory of life generally dictates that you flow through varying levels of independence as you mature – children are highly dependent on their family to take care of them until they themselves can effectively contribute to society; while seniors, having contributed economically for the majority of their lives, begin to lose their independence as they age, mostly due to declining health. Often times these seniors depend on their children or community services to maintain a high quality of life.

Based on the assumption that youth and senior populations are “dependent”, while those of working age are “independent”, a dependency ratio can be calculated. Simply, the ratio illustrates the relationship between persons drawing from community resources to those contributing.

Figure Com 3.1: Dependency Ratio, 2016 – Comparison (Statistics Canada)



Since at least 2006, Comox’s dependency ratio has been below 1.0, demonstrating that there are more persons contributing resources than otherwise. For clarity, a ratio of 1.0 means that there are equal amounts of people assumed to be working for each dependent. A lower ratio would indicate more working age people versus dependents, while a higher ratio would be the opposite. **Figure Com 3.1** illustrates the change in ratios over time for each compared geography.

Table Com 3.1: Dependency Ratio, 2016 – Comparison (Statistics Canada)

COMMUNITY	2006	2011	2016	%Δ06-16
Comox	0.83	0.83	0.91	9.8%
Comox Valley	0.68	0.70	0.80	16.8%
British Columbia	0.60	0.59	0.62	3.4%

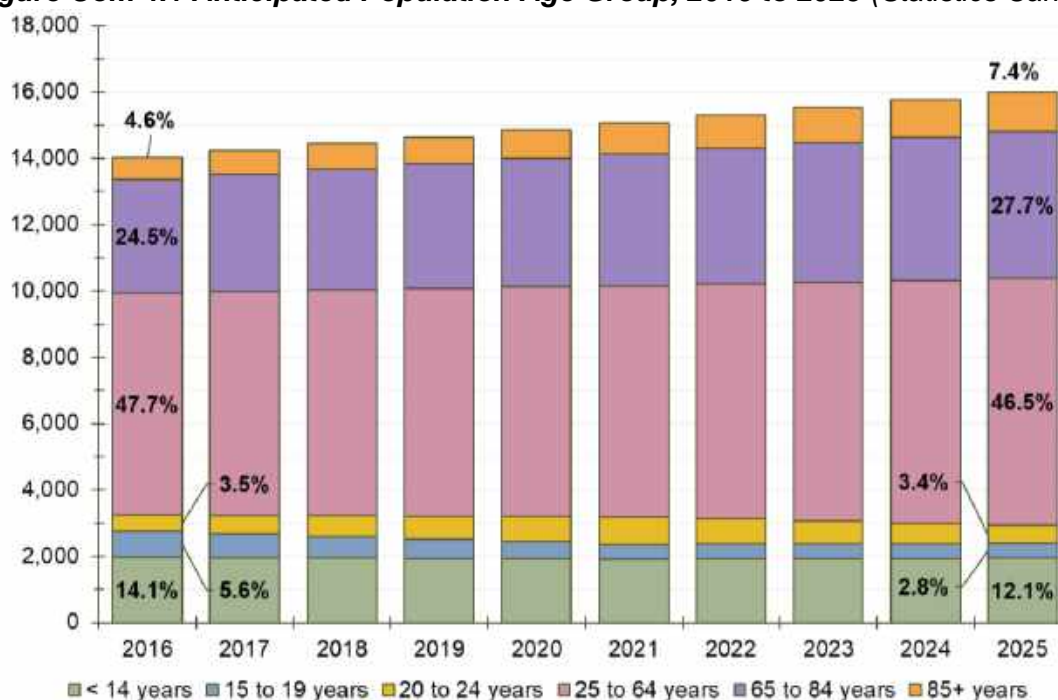
Similar to trends in median age, Comox has a higher ratio than CVRD and BC. In 2016, its ratio hit 0.95, 14.3 percent higher than 10 years prior. Although it has about four times greater growth than the Province, it grew slightly slower than the regional rate. This demonstrates a population whose relative ageing impacts are less than its neighbouring communities.

4. Anticipated Population

Population projections use the Cohort Survival Method (CSM) to anticipate growth every five years until a chosen cut-off period using historical birth, mortality, and migration rates. Similar to any projection exercise, results become less accurate over longer periods – this particular method treats the community as being in a constant state economically, socially, and environmentally when, in reality, these factors constantly change due to local, regional, and wider influences.

Because the CSM generates results every five years, straight line change between projection periods is used to estimate the population on an annual basis. The results are as displayed in **Figure Com 4.1** and **Table Com 4.1**.

Figure Com 4.1 Anticipated Population Age Group, 2016 to 2025 (Statistics Canada)



The 2020 estimated population is 14,855 residents (up 6 percent since 2016). In 5 years, this total will possibly rise to about 15,955, marking a 14.1 percent increase since 2016. During this time, all age groups will likely experience growth except for young persons – the 15 to 19 age cohort will drop 42.0 percent and the less than 14-year cohort will decline slightly by 1.5 percent. Declines are mostly attributed to overall shifts of the population to older cohorts as they age and out-migration of older students to other communities, unmatched and/or unsurpassed by births or in-migration.

In continuation of historical trends, senior populations are anticipated to rise for the foreseeable future. By 2025, total people 65 or older will reach 5,620. This represents 37.6 percent growth over nine years, or 3.6 percent annually.

Table Com 4.1: Anticipated Population, 2016 to 2025 (Statistics Canada)

	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025	%Δ '16-'25
Total	14,020	14,230	14,435	14,645	14,855	15,065	15,300	15,530	15,760	15,995	14.1%
< 14 years	1,970	1,960	1,945	1,935	1,925	1,910	1,920	1,925	1,935	1,940	-1.5%
15 to 19 years	785	715	650	580	510	445	445	450	450	455	-42.0%
20 to 24 years	490	560	625	695	765	830	760	685	610	540	10.2%
25 to 64 years	6,690	6,745	6,800	6,860	6,915	6,970	7,090	7,205	7,325	7,440	11.2%
65 to 84 years	3,435	3,545	3,655	3,765	3,875	3,990	4,100	4,210	4,320	4,430	29.0%
85+ years	650	705	760	810	865	920	985	1,055	1,120	1,190	83.1%
Dependency Ratio	0.95	0.95	0.94	0.94	0.93	0.93	0.95	0.97	0.99	1.00	5.4%
Median Age	51.0	50.9	50.8	50.7	50.6	50.6	51.6	52.7	53.7	54.8	7.5%
Average Age	47.5	47.9	48.3	48.8	49.2	49.6	49.9	50.2	50.4	50.7	6.8%

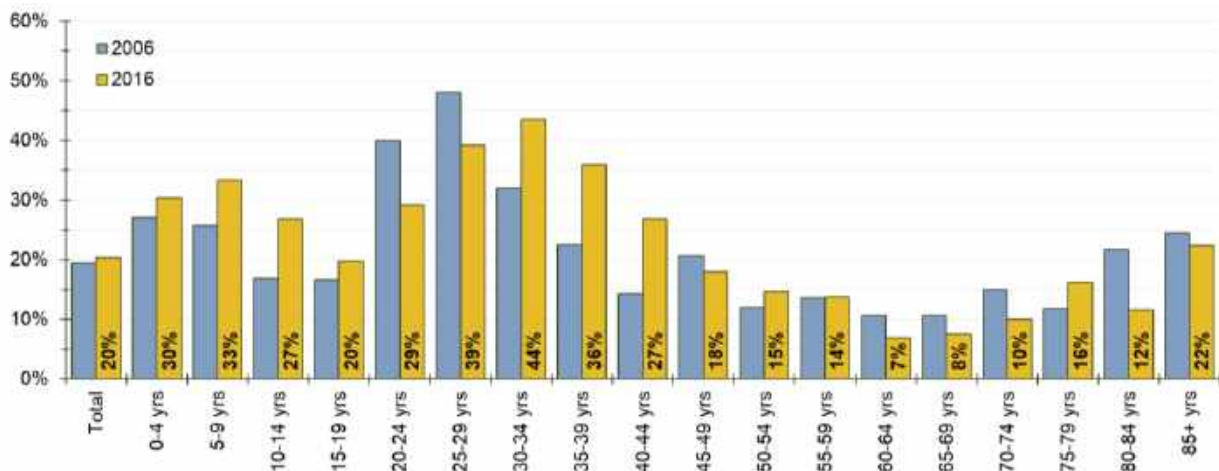
Median age will continue to increase as a function of the greater number of people in older cohorts, hitting 54.8 in 2025. Average age will remain lower, likely held down by relatively constant totals of persons less than 14-years-old. The dependency ratio will climb to 1.0 in 2025, illustrating the turning point when the dependent population will begin to surpass those that are independent.

This trend signifies an eventual shift in how the community will use, consume, and allocate assets among different age groups. Accordingly, Comox will have to review its provision of services to ensure there is capacity to take on an added burden.

5. Tenure

Overall, Comox has a renter to owner ratio of 20:80, meaning for every 20 renters there are 80 owners. Accordingly, approximately 2,795 residents rent their accommodation or belong to a household that rents – the report discusses maintainer tenure patterns later on.

Figure Com 5.1: Renters by Age, 2016 (Statistics Canada)

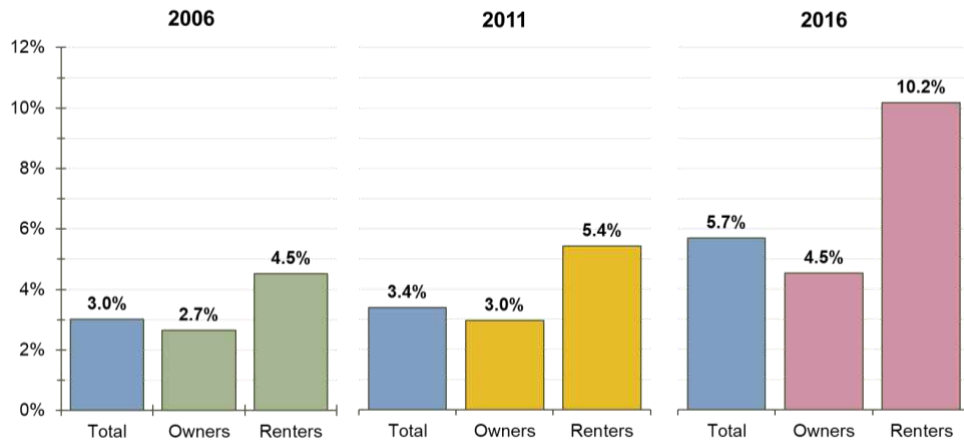


Renting gains momentum after the 15 to 19 age cohort as young adults choose to move away from home and become maintainers of their own households. It then peaks for persons between 30 to 34, reaching 44 percent – a 12 percentage point increase since 2006. Generally, renting rates increased across most cohorts until about 60 years old. Nevertheless, two outliers exist: both 20 to 24 and 25 to 29 age cohorts have noticeably higher rates in 2006. Unfortunately, there is insufficient data available to confirm what the underlying cause is. Speculations could include: (1) rental market entry costs increased enough within the previous 10 year period to disincentivize young adults from moving out of their family homes; (2) more individuals reported their permanent address as their family home even if they are living elsewhere, likely related to more young adults enrolling in higher education who may live away from home but not permanently; or (3) there is discrepancy within the dataset related to changes in how the data was collected or defined.

6. Indigenous Identity

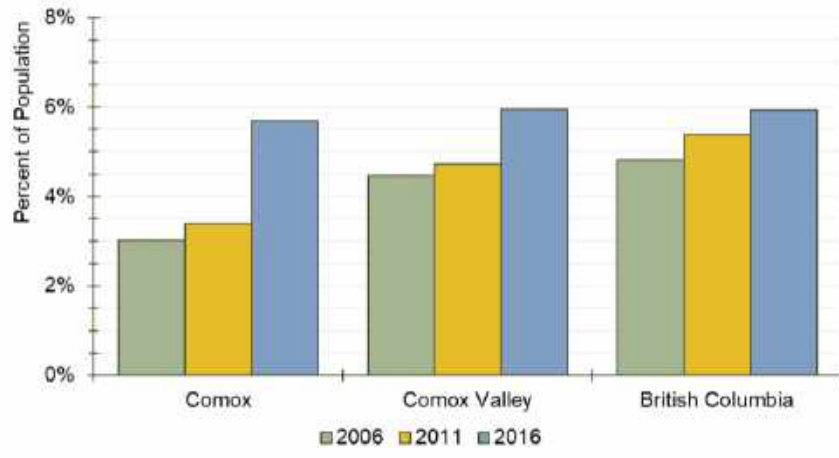
Since 2006, Comox's indigenous population more than doubled from 360 to 780. This surpasses the decrease experienced by on reserve K'ómoks First Nation populations (70) in the same period. Overall, 5.7 percent of the population identifies as having an indigenous identity.

Figure Com 6.1: Historical Indigenous Identity by Tenure (Statistics Canada)



Renter households demonstrate more than two times higher rates of indigenous identity than owner households (10.2 percent and 4.5 percent, respectively). Nevertheless, both household types grew by similar totals – 175 indigenous persons for owner households and 150 for renters.

Figure Com 6.2: Historical Indigenous Identity – Comparison (Statistics Canada)



Relative to CVRD and BC, Comox had significantly higher indigenous population growth between 2006 and 2016 – about 67 percent higher than the Region. However, Comox’s indigenous population is considerably smaller than larger geographies; thus, any changes in population will result in amplified percentage change calculations. Notwithstanding, Comox’s specific increase is likely associated (at least in part) by proximity to lands belonging to the K’ómoks First Nation.

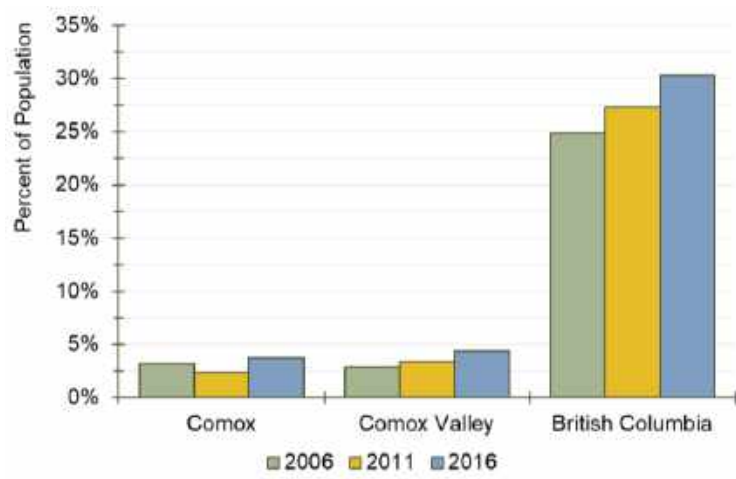
Table Com 6.1: Historical Indigenous Identity – Comparison (Statistics Canada)

COMMUNITY	2006	2011	2016	%Δ06-16
Comox	3.0%	3.4%	5.7%	116.7%
Comox Valley	4.4%	4.7%	5.9%	49.1%
British Columbia	4.8%	5.4%	5.9%	38.5%

7. Visible Minority

Comox mirrored the provincial change in persons identity as a visible minority between 2006 and 2016, achieving 35.5 percent growth. Relatedly, the Town's proportion of minority population increased from 3.2 percent to 3.8 percent during the same period, reaching 515 persons.

Figure Com 7.1: Historical Visible Minority Population – Comparison (Statistics Canada)



The Regional District's 2016 proportion was 4.4 percent, representing 70.0 percent growth in actual visible minority populations from 2006, higher than the Town and Province. The main contributor to this growth is the City of Courtenay which welcomed 735 new minority persons (73.5 percent growth) as of the last census.

Table Com 7.1: Historical Visible Minority Population – Comparison (Statistics Canada)

COMMUNITY	2006	2011	2016	%Δ06-16
Comox	3.2%	2.4%	3.8%	35.5%
Comox Valley	2.9%	3.4%	4.4%	70.0%
British Columbia	24.9%	27.3%	30.3%	36.9%

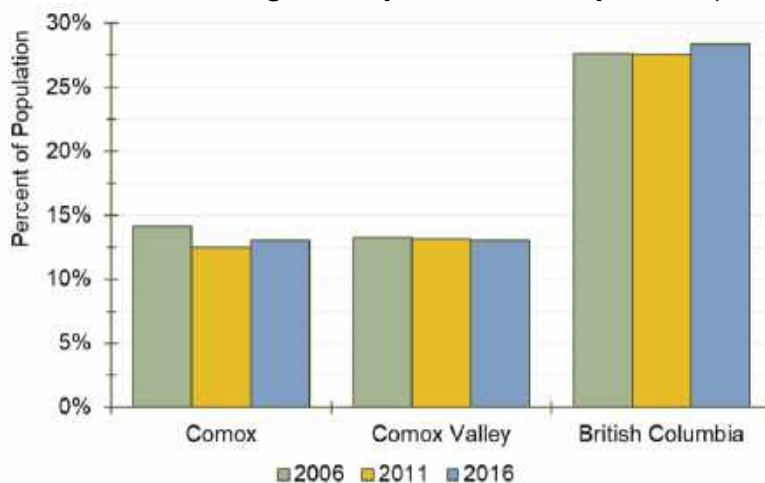
8. Immigrant Population

Comox's proportion of immigrant population declined from 14.1 percent to 13.0 percent between 2006 and 2016. Notwithstanding, the total number of immigrants increased 5.9 percent – 1,685 to 1,785 persons. This demonstrates that population growth is more dependent on increased levels of incoming nationals (whether by birth or in-migration).

Table Com 8.1: Historical Immigrant Population – Comparison (Statistics Canada)

COMMUNITY	2006	2011	2016	%Δ06-16
Comox	14.1%	12.5%	13.0%	5.9%
Comox Valley	12.8%	12.7%	12.6%	10.8%
British Columbia	27.6%	27.6%	28.3%	15.5%

Figure Com 8.1: Historical Immigrant Population – Comparison (Statistics Canada)



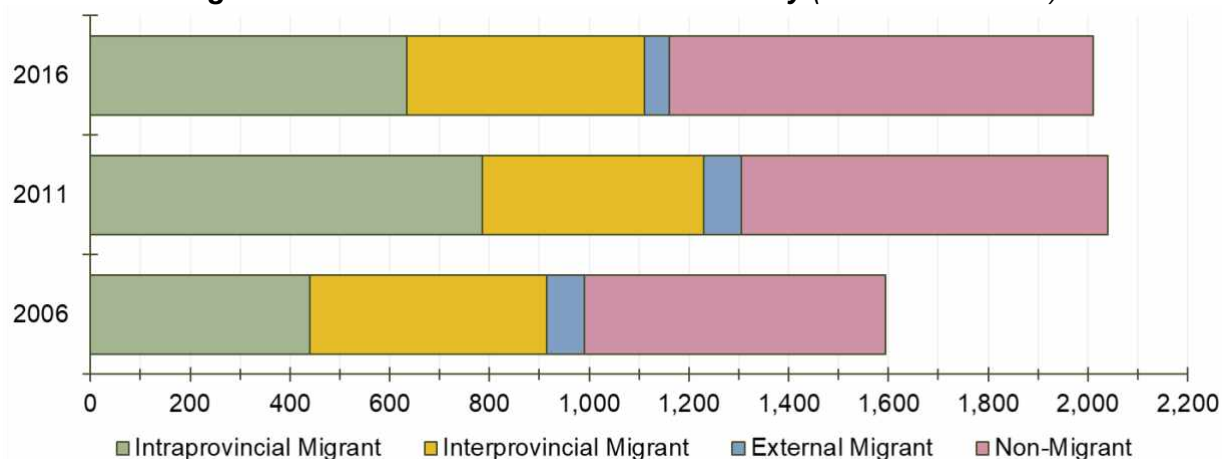
The Regional District’s proportion of immigrant population is lower in both compared censuses, which conveys a typical trend of an urban community versus a district that includes rural areas. CVRD’s actual immigrant persons growth almost doubled that of Comox, mostly due to trends within the City of Courtenay and Electoral Area A.

British Columbia about doubles Comox proportions and triples the growth in the actual number of immigrant people. However, this is largely attributed to the Vancouver Census Metropolitan Area which boasts a 40.8 percent rate of people identifying as immigrants (989,540 people in 2016 – more than entire population of Vancouver Island, whose immigrant proportions closely follow that of Comox).

9. Mobility

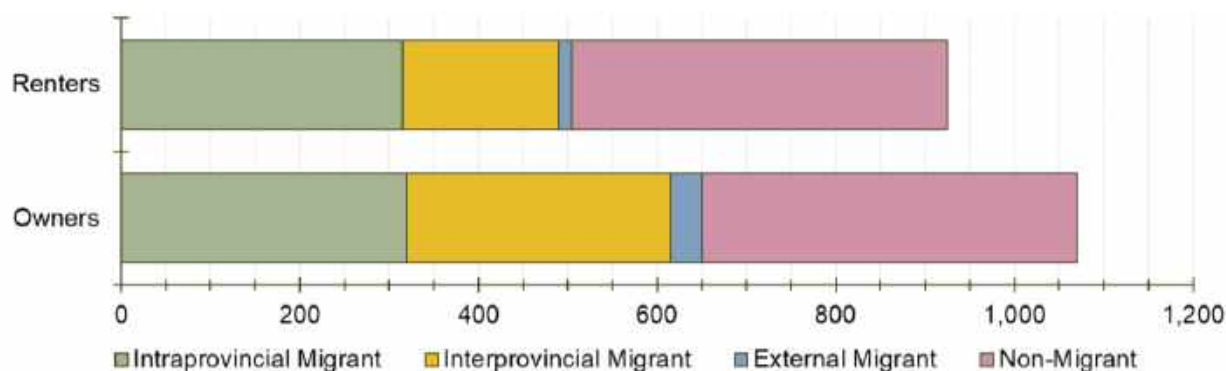
Changes in overall population are, at its simplest, defined by three primary variables: births, deaths, and migration. Although the two formers do change over time, their volatility is limited due to the social, economic, and political security offered by Canada, a country of high living standard that is simultaneously experiencing minimal conflict relative to other nations. However, migration can change quickly due to a combination of intra- and international forces.

Figure Com 9.1: Historical One-Year Mobility (Statistics Canada)



One-year mobility refers to the status of a person with regard to the place of residence on the reference day in relation to the place of residence on the same date one year earlier. According to the 2016 census, Comox experienced greater migrant totals than its 2006 counterpart – welcoming 1,155 new residents compared to 995. The major contributor to growth was persons moving to Comox from within the Province (inclusive of people moving from nearby communities). Total interprovincial migrants did not change, while external (international) migrants fell by 25.

Figure Com 9.2: One-Year Mobility by Tenure, 2016 (Statistics Canada)



The majority of migrants belonged to owner households; however, this is realistically more related to the trend that owner household sizes are, on average, larger than renters. In other words, when owners move to the region they generally do so with family while renters may be alone. That aside, the same total renters and owners moved to Comox from within British Columbia, while owners nearly doubled renters for migrants with interprovincial (national) or international origins.

Economic trends (discussed later on) demonstrate noticeable growth in high income households – a consistent change across the majority of CVRD. This trend, coupled with higher levels of in-migration could suggest that a strong proportion of those individuals and households moving to Comox are within higher income brackets. Their move may be stimulated by several factors, including: (1) local job creation (i.e. Comox Valley’s new North Island Hospital) or (2) maximizing returns on housing appreciation in another market to purchase a home of similar quality and size but for less money in Comox.

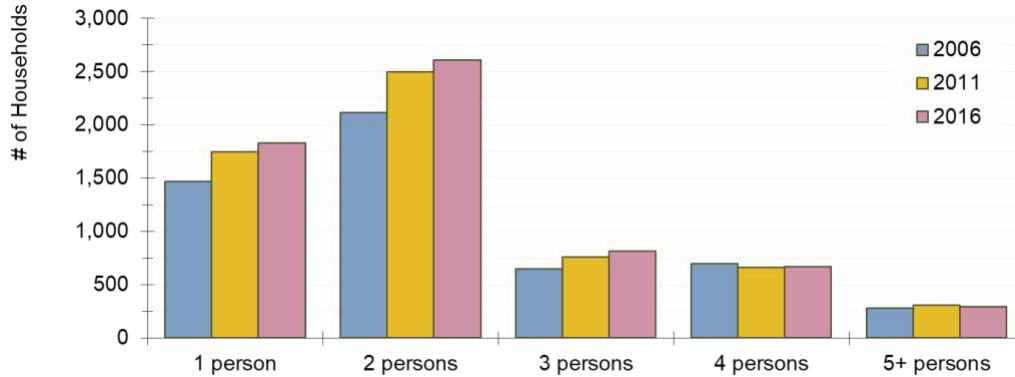
Table Com 9.1: Historical One-Year Mobility by Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Population	11,810	13,170	13,610	9,510	10,705	10,840	2,300	2,465	2,770
Non-Mover	10,210	11,130	11,610	8,515	9,525	9,770	1,695	1,605	1,845
Mover	1,595	2,045	2,005	995	1,180	1,075	605	865	930
Non-Migrant	605	735	850	290	450	420	315	290	420
Migrants	995	1,310	1,155	700	730	650	290	575	505
Internal Migrants	920	1,230	1,110	655	705	615	265	525	495
Intraprovincial Migrant	440	785	635	285	435	320	155	345	315
Interprovincial Migrant	475	445	475	365	270	295	110	180	175
External Migrant	75	75	50	45	25	35	30	50	15

10. Household Size

All household sizes experienced some growth between 2006 and 2016. The greatest increases occurred for 1- and 2-person households (365 and 495, respectively), most of which came from owner households. Two or fewer person households now hold a greater proportion of the total; consequently, average household size sits at 2.2 – 0.1 lower than 2006.

Figure Com 10.1: Historical Household Sizes (Statistics Canada)

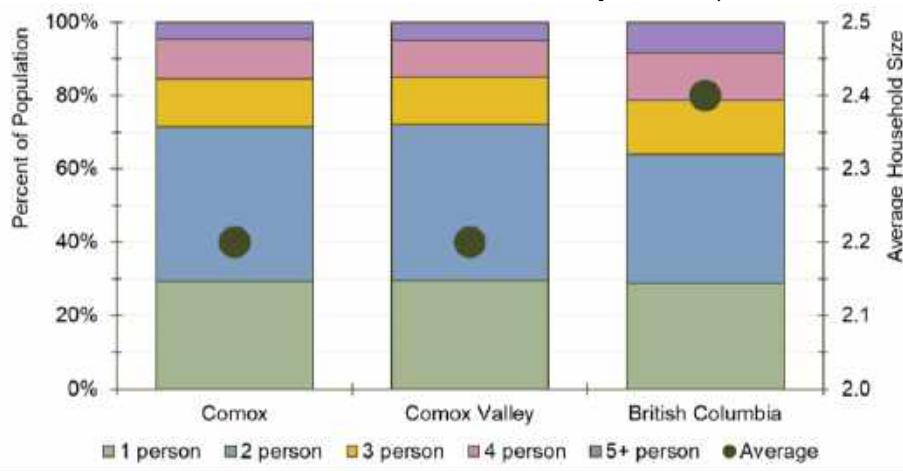


Interestingly, average household size increased for renter households. This may suggest more families relative to all renter households are renting rather than owning a home, as depicted by the greater relative change for households of 3 or more-persons. To illustrate, 23.8 percent of 2016 households were 3 or more people; whereas, it was 22.4 percent in 2006. Although a small difference, it is enough to increase average size by 0.1 to 2.0.

Table Com 10.1: Historical Household Sizes by Tenure (Statistics Canada)

	Total				Owners			Renters		
	2006	2011	2016	'16 % of Total	2006	2011	2016	2006	2011	2016
Total Private Households	5,205	5,970	6,205	100%	4,000	4,655	4,800	1,205	1,320	1,410
1 person	1,465	1,745	1,830	29.5%	880	1,085	1,195	580	660	635
2 persons	2,115	2,495	2,610	42.1%	1,765	2,165	2,170	345	330	440
3 persons	645	760	815	13.1%	505	550	665	140	210	145
4 persons	700	665	670	10.8%	615	580	545	80	90	130
5+ persons	280	310	290	4.7%	225	275	225	50	35	60
Average Household Size	2.3	2.2	2.2		2.4	2.3	2.3	1.9	1.9	2.0

Figure Com 10.2: Household Size, 2016 – Comparison (Statistics Canada)

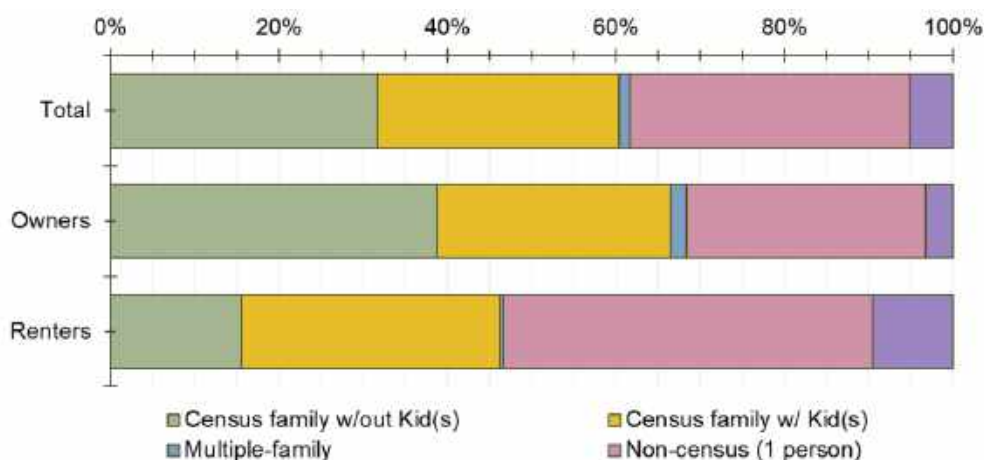


Comox’s 2016 distribution of household sizes replicates that of the CVRD, resulting in equivalent averages of 2.2. This is perceptibly lower than BC overall, which has an average household size of 2.4. The difference is due to the greater share of 3 or more person households – 35.9 percent versus Comox Valley’s 28.0 percent.

11. Household Type

Generally, owner and renter households require that their accommodations meet different needs regarding size, quality, and price. For instance, a single person may not need many bedrooms or may not have as high an income as a dual income household, so a rental may be most appropriate; whereas, a family with children would require more space that is traditionally offered by owner dominated dwelling types like single-family homes. The aforementioned are discussed in terms of their “census-family” type. A census-family is defined as a married couple and the children, if any, of either and/or both spouses; a couple living common law and the children, if any, of either and/or both partners; or a lone parent of any marital status with at least one child living in the same dwelling and that child or those children.

Figure Com 11.1: Distribution of Census Family Types by Tenure, 2016 (Statistics Canada)



Non-census families are the dominant renter household type at 50.7 percent (mostly due to 1 person households); whereas, census-families (i.e. couples with or without children) command 72.5 percent of owner homes. Overall, census families grew 620 (17.5 percent), while non-census families grew 420 (26.4 percent), meaning that non-census families have an increasing share of the household pie – up from 30.5 percent to 32.4 percent over 10 years.

Table Com 11.1: Historical Census Family Types by Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total - Private Households	5,205	5,975	6,205	4,000	4,655	4,795	1,205	1,320	1,410
One-census Family	3,545	4,025	4,165	2,975	3,425	3,475	570	600	695
Census family w/out Kid(s)	1,780	2,130	2,240	1,560	1,930	2,005	220	205	235
Census family w/ Kid(s)	1,625	1,890	1,925	1,290	1,495	1,470	335	395	455
Multiple-family	70	35	30	70	35	35	0	0	0
Non-census Family	1,590	1,920	2,010	960	1,195	1,295	630	720	715
Non-census (1 person)	1,465	1,745	1,830	885	1,085	1,195	585	655	630
Non-census (2+ person)	125	175	180	75	115	100	50	60	85

Relatedly, renter households experienced greatest unit and percentage family type growth in census families with children (120 and 35.8 percent, respectively). Conversely, non-census 1-person households had greatest owner growth (35.0 percent). The results are gradually changing family distributions in both household tenure types.

What is causing the change is unclear. One could suggest that there are more lone parent households (which count as census families with kids) who are looking for alternative housing; thus, leading parents to seek out more affordable rental options. Such a suggestion is a possibility, especially considering that the proportion of lone-parents among couples with children has grown slightly from 2006 to 2016 – 35.4 to 37.2 percent, respectively. Alternatively, couples with young children may not yet be able to afford a home in the rapidly appreciating Comox, CVRD, and BC markets, forcing them to find rental accommodation instead.

Figure Com 11.2: Couples with Kid(s) & Lone Parents as % of All Couples, 2016 (Statistics Canada)

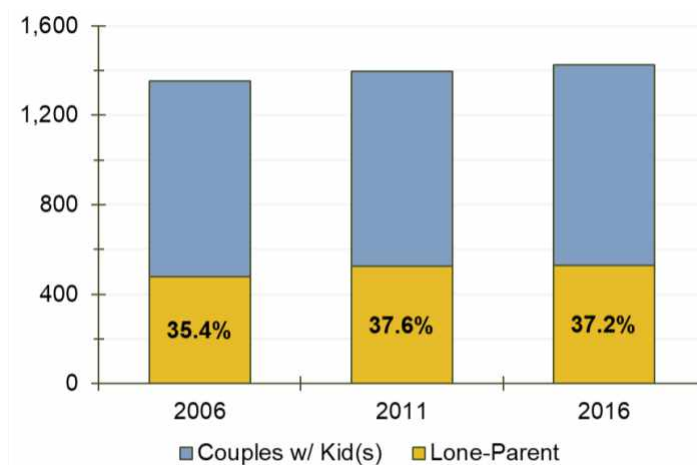


Table Com 11.2: Historical Couple Households (Statistics Canada)

	2006	2011	2016
Total Couples	3,210	3,585	3,720
Couples w/out Kid(s)	1,855	2,200	2,295
Couples w/ Kid(s)	1,355	1,395	1,425
Lone-Parent	480	525	530

12. Household Maintainers

A household maintainer refers to whether or not a person residing in the household is responsible for paying shelter costs (e.g. rent, mortgage, taxes, or utilities). Knowing the makeup of a community's maintainers provides greater understanding of the households mostly taking part in the market and hints at what economic or demographic circumstances may be impacting those households.

Comox's 2016 distribution of primary owner household maintainers follows a parabolic trend, illustrated in **Figure Com 12.1** by a uniform increase in ownership rates and maintainer totals until about 65 to 74 years old, which is followed by a drop in both variables. Generally, this indicates that as households age, their ability and willingness to take on home ownership

increases. This is until circumstances (e.g. health) force some to part with their homes and seek alternative housing (i.e. smaller rentals or retirement homes).

Figure Com 12.1: Tenure Distribution of Maintainers by Age, 2016 (Statistics Canada)

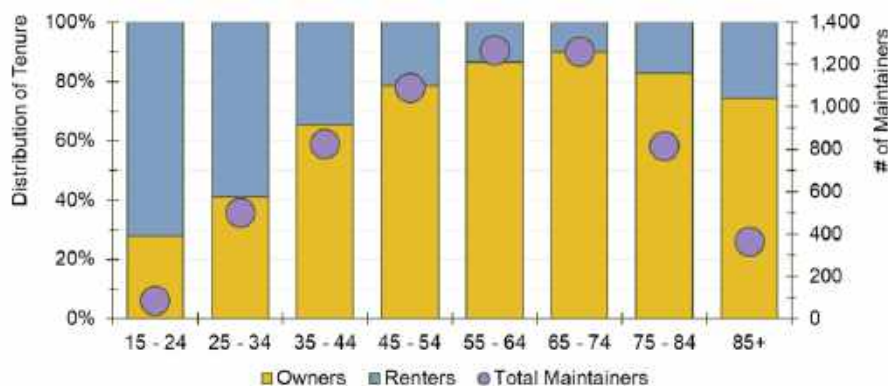
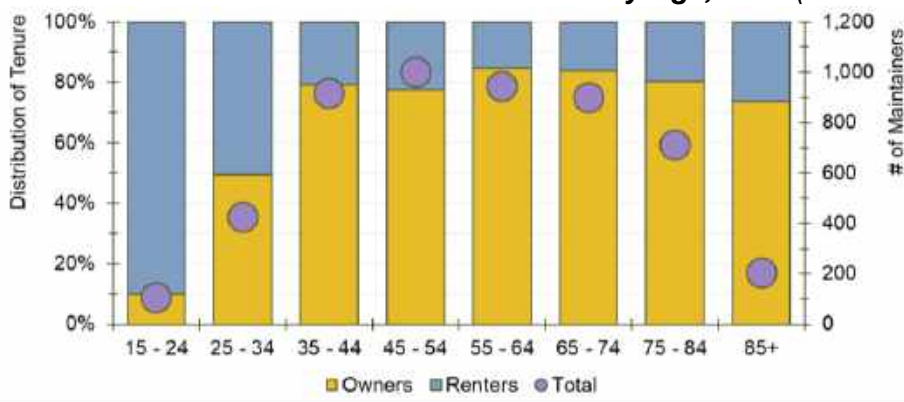


Figure Com 12.2: Tenure Distribution of Maintainers by Age, 2006 (Statistics Canada)



Comox’s transition between renting and owning has not always been as gradual. As recently as 2006, almost half of maintainers between 25 to 34 owned a dwelling compared to 41 percent in the latest census. Similarly, the proportion of owner maintainers between 35 to 44 dropped 13.8 percent to 65.5 percent. Nevertheless, 2016 still demonstrated a higher overall ownership rate (77.4 percent), driven by growth in maintainer totals between 45 to 74.

Table Com 12.1: Historical Number of Maintainers by Age & Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Household	5,205	5,975	6,205	4,000	4,655	4,800	1,205	1,320	1,410
15 - 24 yrs	105	120	85	10	30	25	90	90	65
25 - 34 yrs	425	525	500	210	280	205	215	240	295
35 - 44 yrs	915	785	825	725	550	540	190	235	285
45 - 54 yrs	1,000	1,215	1,090	775	1,005	860	225	210	235
55 - 64 yrs	945	1,135	1,265	805	955	1,095	145	185	170
65 - 74 yrs	900	1,065	1,260	755	935	1,135	145	130	130
75 - 84 yrs	710	790	815	570	660	675	140	130	140
85+ yrs	205	340	365	155	245	275	55	95	95

The 2016 census shows a 19.9 percentage point increase in 15 to 24-year-old ownership rates since 2006. However, this is mostly due to a slight increase in owners coupled with a larger

decrease in renters. The culprit is the decreased total maintainers in this cohort, many of whom have likely decided to wait longer before permanently leaving their family home.

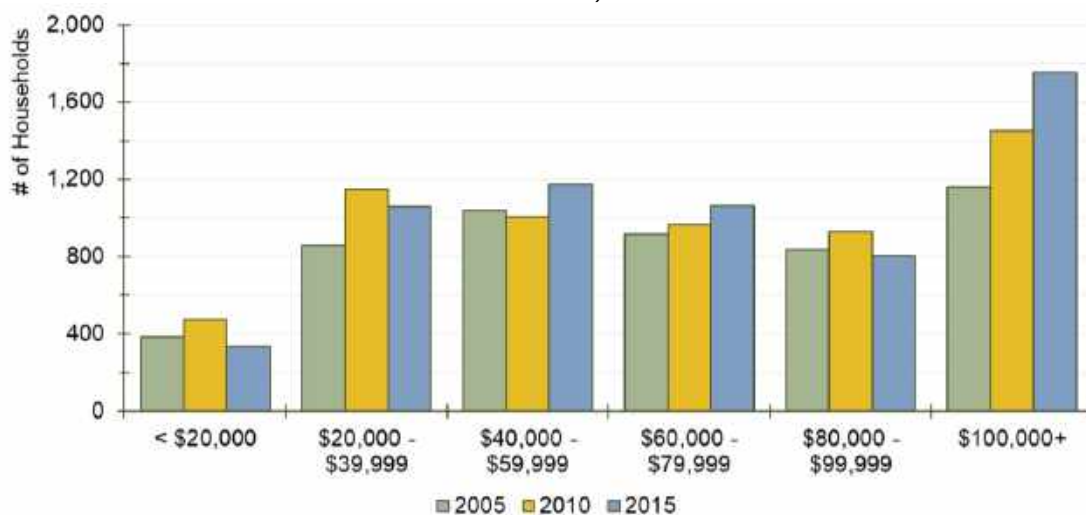
ECONOMY

13. Income

Since 2006, Comox has seen an increase in its overall households by about 1,000, which has generally resulted in increases within all income distributions, as shown in **Figure Com 13.1** below. Of the six distributions (measured in increments of \$20,000), only two experienced a decrease in the number of households: (1) those making less than \$20,000 (dropping from 385 to 335 – 13.0 percent) and (2) those making between \$80,000 and \$99,999 (dropping from 840 to 805 – 4.2 percent). Of those that increased, the greatest growth occurred for households making more than \$100,000, rising from 1,160 to 1,755 – 51.3 percent.

Please note that all reported incomes within this report have been adjusted to 2015 dollars (adjusted for inflation) for better comparison. Readers may also notice that 2005 and 2015 comparison years differ from the normal 2006 and 2016. The reason is that census incomes come from the previously reported tax year.

Figure Com 13.1: Historical Before-Tax Income Distribution, 2015 dollars (Statistics Canada)



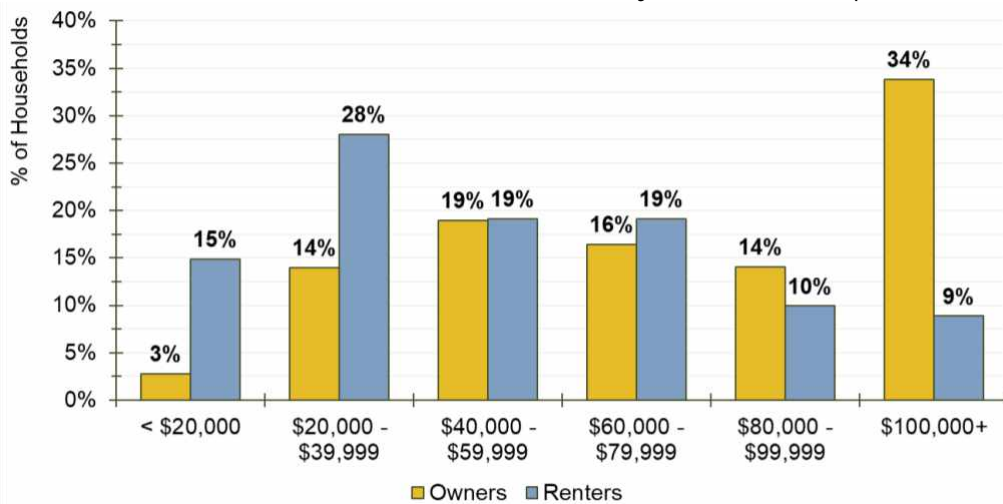
Among all reported Statistics Canada before-tax income brackets, the mode household income was between \$50,000 to \$59,999. Since the \$100,000 or more bracket encompasses all possible greater incomes, it cannot be properly compared to those below it. Nevertheless, 28.3 percent of households made more than \$100,000 in 2015, of which 3.4 percent of total households was for incomes above \$200,000. Unsurprisingly, the average income sits above the median, demonstrating that significant outliers exist within the highest income brackets.

Table Com 13.1: Historical Before-Tax Income Distribution by Tenure, 2015 dollars
(Statistics Canada)

			Total 2015	% of Total			Owners 2015	% of Total			Renters 2015	% of Total
	2005	2010			2005	2010			2005	2010		
Total Household	5205	5970	6205	100.0%	4000	4655	4800	100.0%	1205	1315	1410	100.0%
< \$5,000	55	55	35	0.6%	35	35	25	0.5%	25	15	10	0.7%
\$5,000 - \$9,999	70	55	35	0.6%	45	20	10	0.2%	20	30	30	2.1%
\$10,000 - \$14,999	90	95	85	1.4%	35	30	10	0.2%	55	65	75	5.3%
\$15,000 - \$19,999	170	270	180	2.9%	85	160	90	1.9%	85	115	95	6.7%
\$20,000 - \$24,999	205	275	205	3.3%	80	120	105	2.2%	125	155	105	7.4%
\$25,000 - \$29,999	215	190	270	4.4%	80	100	195	4.1%	130	95	75	5.3%
\$30,000 - \$34,999	170	340	285	4.6%	110	245	155	3.2%	60	95	130	9.2%
\$35,000 - \$39,999	270	345	300	4.8%	215	250	215	4.5%	55	95	85	6.0%
\$40,000 - \$44,999	230	200	230	3.7%	190	160	175	3.6%	40	40	55	3.9%
\$45,000 - \$49,999	285	280	315	5.1%	220	225	240	5.0%	65	60	75	5.3%
\$50,000 - \$59,999	525	525	630	10.2%	355	425	495	10.3%	170	100	140	9.9%
\$60,000 - \$69,999	435	530	560	9.0%	345	425	415	8.6%	90	105	145	10.3%
\$70,000 - \$79,999	480	435	505	8.1%	380	335	375	7.8%	100	100	125	8.9%
\$80,000 - \$89,999	405	505	460	7.4%	390	445	360	7.5%	15	60	100	7.1%
\$90,000 - \$99,999	435	425	345	5.6%	375	330	315	6.6%	55	95	40	2.8%
\$100,000+	1160	1450	1755	28.3%	1045	1350	1625	33.9%	115	90	125	8.9%
\$100,000 - \$124,999	460	590	740	11.9%	395	510	660	13.8%	60	80	80	5.7%
\$125,000 - \$149,999	360	340	470	7.6%	345	325	435	9.1%	10	0	35	2.5%
\$150,000 - \$199,999	220	300	335	5.4%	210	300	330	6.9%	20	0	10	0.7%
\$200,000+	120	220	210	3.4%	95	215	200	4.2%	20	0	0	0.0%
Median Income	\$66,583	\$66,284	\$69,254		\$73,372	\$74,179	\$76,595		\$45,299	\$39,639	\$46,762	
Average Income	\$77,946	\$79,482	\$82,032		\$81,988	\$88,008	\$90,306		\$64,507	\$49,375	\$53,873	

The distribution of incomes across tenure types is distinct, showcasing that 43 percent of renter households make less than \$39,999, as of 2015, while 17 percent of owners fell within the same category. On the other end, 34 percent of owner households make more than \$100,000, compared to 9 percent for renters. Although visually jarring, the results are not necessarily surprising as tenure type is highly determined by available income relative to housing prices. Even with that consideration, the number of renter households making above \$60,000 increased 42.7 percent between 2005 and 2015, while owner households increased by 21.9 percent. This perhaps suggests that the relative increase in renter households that are overall better off financially may be tied to the housing market.

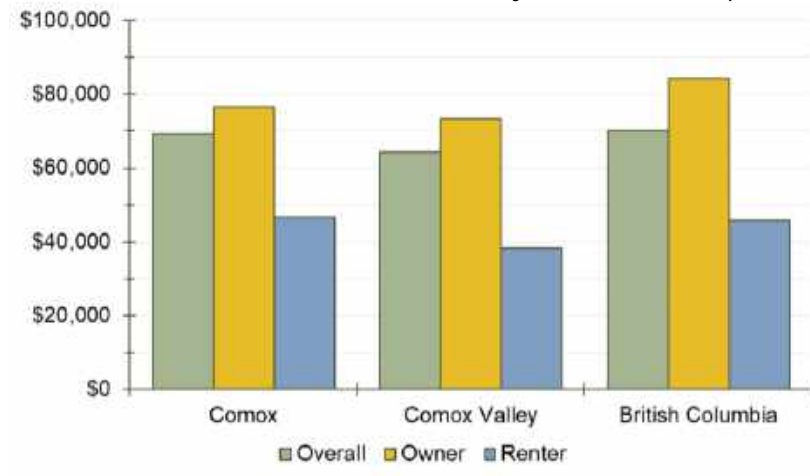
Figure Com 13.2: Before-Tax Income Distribution by Tenure, 2015 (Statistics Canada)



Across Comox, CVRD, and BC, renter households generate less income than their owner counterparts, largely due to the difference in household makeup between both tenure types. For

instance, owners tend to be older, have been in the workforce longer, and are more likely to have dual incomes; whereas, renters are generally younger and are just starting careers, and may live alone or with roommates in similar situations.

Figure Com 13.3: Before-Tax Median Income by Tenure, 2015 (Statistics Canada)



At \$69,254, Comox’s 2015 before-tax median household income surpasses that of the Region (\$64,379), and is slightly below that of the Province (\$69,995). However, Comox’s percent growth in 2015 constant dollars fell behind at 4.0 percent – or 0.4 percent annually. CVRD and BC experienced 1.0 and 1.2 percent annual growth over the same period, adjusted for inflation.

Table Com 13.2: Before-Tax Median Income by Tenure, 2015 – Comparison (Statistics Canada)

COMMUNITY	Overall	%Δ05-15	Owner	%Δ05-15	Renter	%Δ05-15
Comox	\$69,254	4.0%	\$76,595	4.4%	\$46,762	3.2%
Comox Valley	\$64,379	11.2%	\$73,367	11.1%	\$38,394	17.6%
British Columbia	\$69,995	12.2%	\$84,333	12.1%	\$45,848	15.9%

14. Income by Household Type

Statistics Canada defines an Economic Family as a group of two or more persons of the same or opposite sex who live in the same dwelling and are related to each other by blood, marriage, common-law union, adoption or a foster relationship. Economic families can be “couples without children or relatives in the home,” “couples with children,” or “lone parents.” All other cases are considered to be a non-economic family, such as a person living alone or with roommates.

More than half of couples with children make more than \$111,275 (median before-tax household income), the highest of Statistics Canada’s defined family types. Next are couples without children or relatives at home at \$78,763. The discrepancy between the two is mostly due to couples with children having a greater likelihood of being in the workforce based on age; whereas, without children could include retired individuals whose income are pensions or investments that produce minimum required returns/incomes to fulfill a particular quality of life. Median income for lone parents is about half of couples with children.

Figure Com 14.1: Median Income by Economic Family Type, 2015 (Statistics Canada)

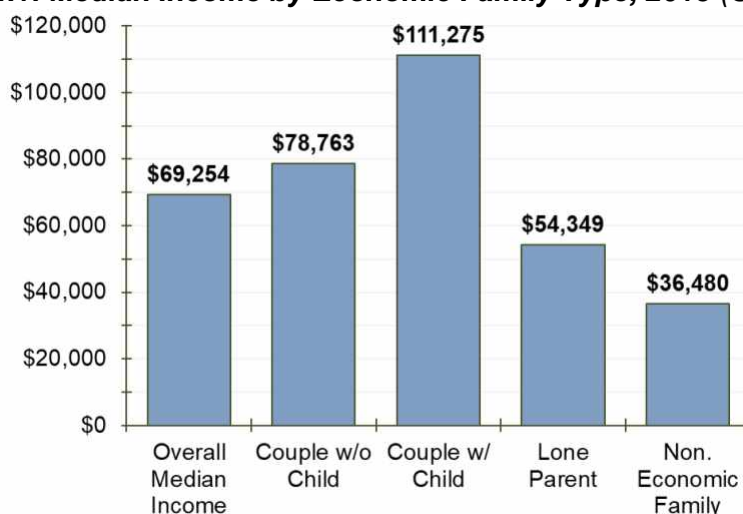


Table Com 14.1: Economic Family Type Before-Tax Median Incomes, 2015 – Comparison (Statistics Canada)

COMMUNITY	Couple w/o		Couple w/	Lone	Non Econ.
	Overall	Kid(s)	Kid(s)	Parent	Family
Comox	\$69,254	\$78,763	\$111,275	\$54,349	\$36,480
Comox Valley	\$64,379	\$74,775	\$103,797	\$44,587	\$30,084
British Columbia	\$69,995	\$80,788	\$111,736	\$51,056	\$31,255

Comox lone parents and non-economic families generate more median income than the CVRD and BC, while the differences in couple incomes varies across geographies. Notably, Comox Valley’s before-tax median income for both couple types is lower than Comox; whereas, BC’s are slightly higher. Comox incomes may be elevated by the presence of Canada Forces Base (CFB) Comox, which would offer relatively higher wages and is a significant employer of young adults.

15. Low-Income Measure (LIM) – After Tax

Low-Income Measures (LIMs) are a set of thresholds estimated by Statistics Canada that identify Canadians who belong to a household whose overall incomes are below 50 percent of median adjusted household income. “Adjusted” refers to the idea that household needs increase as the number of household members increase. Statistics Canada emphasizes that the LIM is not a measure of poverty, but identifies those who are substantially worse off than the average.

Overall, 10.4 percent of Comox residents fall below the after-tax LIM. Generally, younger cohorts experience greatest difficulty to meet their needs (or for their families to meet their needs) – 15.8 percent of children between 0 to 5 years belong to a household below the measure, compared to 14.8 percent of children between 0 to 17. This suggests that younger households (associated with younger children) have less available income, particularly as they navigate the through the transition to first time parenthood. Similarly, as cohorts age, their incomes and number of dependents decrease, thereby reducing the prevalence of low-income individuals. The prevalence of persons below the LIM in 2016 drops to 9.9 percent for persons 18 to 64, and to 8.9 percent for those 65 or older.

Figure Com 15.1: LIM After-Tax Status, 2016 – Comparison (Statistics Canada)

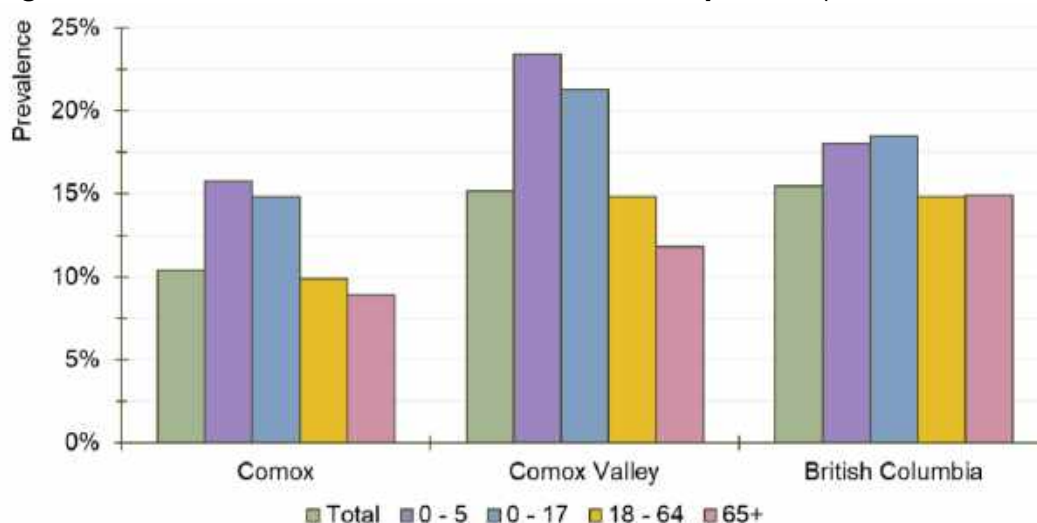


Table Com 15.1: LIM After-Tax Status by Age, 2016 (%) – Comparison (Statistics Canada)

COMMUNITY	Total	0 - 17	0 - 5	18 - 64	65+
Comox	10.4%	14.8%	15.8%	9.9%	8.9%
Comox Valley	15.2%	21.3%	23.4%	14.8%	11.8%
British Columbia	15.5%	18.5%	18.0%	14.8%	14.9%

Comox’s decreasing low income prevalence is not necessarily mirrored by all communities. The Regional District displays similar trends, though its rates are overall higher – total prevalence is 15.2 percent. On the other hand, the Province demonstrates a smaller rate for children between 0 to 5 than 0 to 17 (18.0 and 18.5 percent, respectively) while more persons 65 or older are deemed worse off than those 18 to 64.

Compared to both higher geographical levels, Comox’s residents and/or households are generally better suited to meet their needs.

16. Employment

Comox’s participation rate (the proportion of people in the labour force relative to the size of the total working-age population) hit 53.7 percent in 2016, down from 55.0 in 2006. The primary cause is the larger relative increase in people not participating (21.4 percent since 2006) compared to those participating (15.2 percent). Based on national trends, the trajectory of non-labour force individuals is largely due to ageing populations who are still considered of working-age (defined as 15 years or older) but are retiring at higher rates than increases in employment. Consequently, the employment rate also dropped, from 51.6 to 49.8 percent, even as the actual number of employed persons increased by about 715.

Figure Com 16.1: Historical Local Labour Metrics by Tenure, 2016 (Statistics Canada)

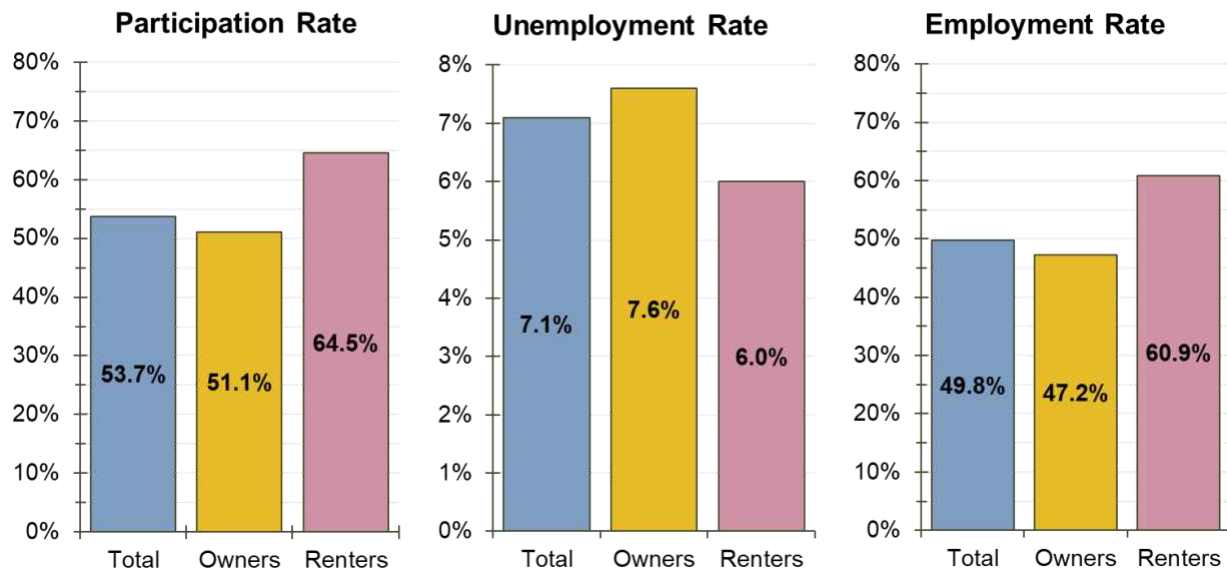


Table Com 16.1: Historical Local Labour Metrics by Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Population (15+ yrs)	9,955	11,295	11,740	8,070	9,255	9,535	1,885	2,040	2,200
In Labour Force	5,470	6,575	6,300	4,325	5,260	4,875	1,145	1,315	1,425
Employed	5,130	6,095	5,845	4,070	4,890	4,505	1,060	1,205	1,340
Unemployed	340	485	455	255	375	370	85	110	85
Not In Labour Force	4,480	4,720	5,440	3,740	3,995	4,660	740	725	780
Participation Rate	55.0	58.3	53.7	53.6	56.8	51.1	60.7	64.5	64.5
Employment Rate	51.6	54.0	49.8	50.5	52.8	47.2	56.2	59.1	60.9
Unemployment Rate	6.2	7.4	7.1	6.0	7.0	7.6	7.4	8.8	6.0

As the share of non-labour force individuals to total working-age persons increases, the share of people in the labour force decreases, impacting the unemployment rate (those unemployed and seeking employment divided by the total labour force). Accordingly, unemployment grew to 7.1 percent in 2016, up from 6.2 percent. However, this is not entirely due to an ageing population. In 2016, more people were unemployed relative to all working-age persons (3.9 percent) than in 2006 (3.4 percent), indicating that a rise in unemployment is also the consequence of other market forces not necessarily tied to demography.

Based on historical trends across tenures, it appears that the negative trends discussed above are mostly due to those experienced by owners (or those belonging to an owned household), who represent 81.2 percent of all people. Generally, all owner labour metrics worsened between 2006 and 2016; whereas, all renter metrics improved. These inconsistencies suggest changes can be associated with lifestyles common within the tenures – renters tend to be younger and seeking employment, while owners are comparatively older and nearing retirement. Previously discussed population tenure trends support this idea. Specifically, that about 88.1 percent of people older than the median age of 51 are in an owner household.

Figure Com 16.2: Labour Metrics, 2016 – Comparison (Statistics Canada)

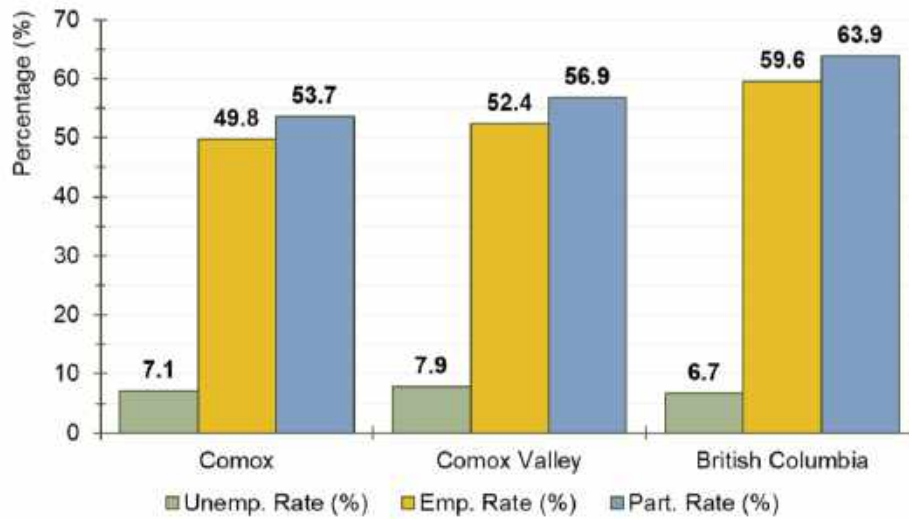


Table Com 16.2: Labour Metrics, 2016 – Comparison (Statistics Canada)

COMMUNITY	In Labour Force			Not Labour Force	Part. Rate (%)	Emp. Rate (%)	Unemp. Rate (%)
	Force	Employed	Unemployed				
Comox	6,300	5,845	455	5,440	53.7	49.8	7.1
Comox Valley	30,815	28,380	2,435	23,385	56.9	52.4	7.9
British Columbia	2,471,665	2,305,690	165,975	1,398,710	63.9	59.6	6.7

Comox demonstrates a better 2016 unemployment rate than the CVRD (7.9 percent), but higher than the Province. Interestingly, only Comox experienced overall improving employment conditions for renters – Comox Valley and BC had higher rates of unemployment since 2006. The former also had worsening employment and participation; whereas, the latter improved slightly in both metrics. All jurisdictions experienced worsening conditions for owner households.

A possible contributor to renter improvements is the presence of CFB Comox, whose personnel are typically renters themselves (either on or off base) since it is easier to change job locations quickly without being tied to real estate.

17. Industry

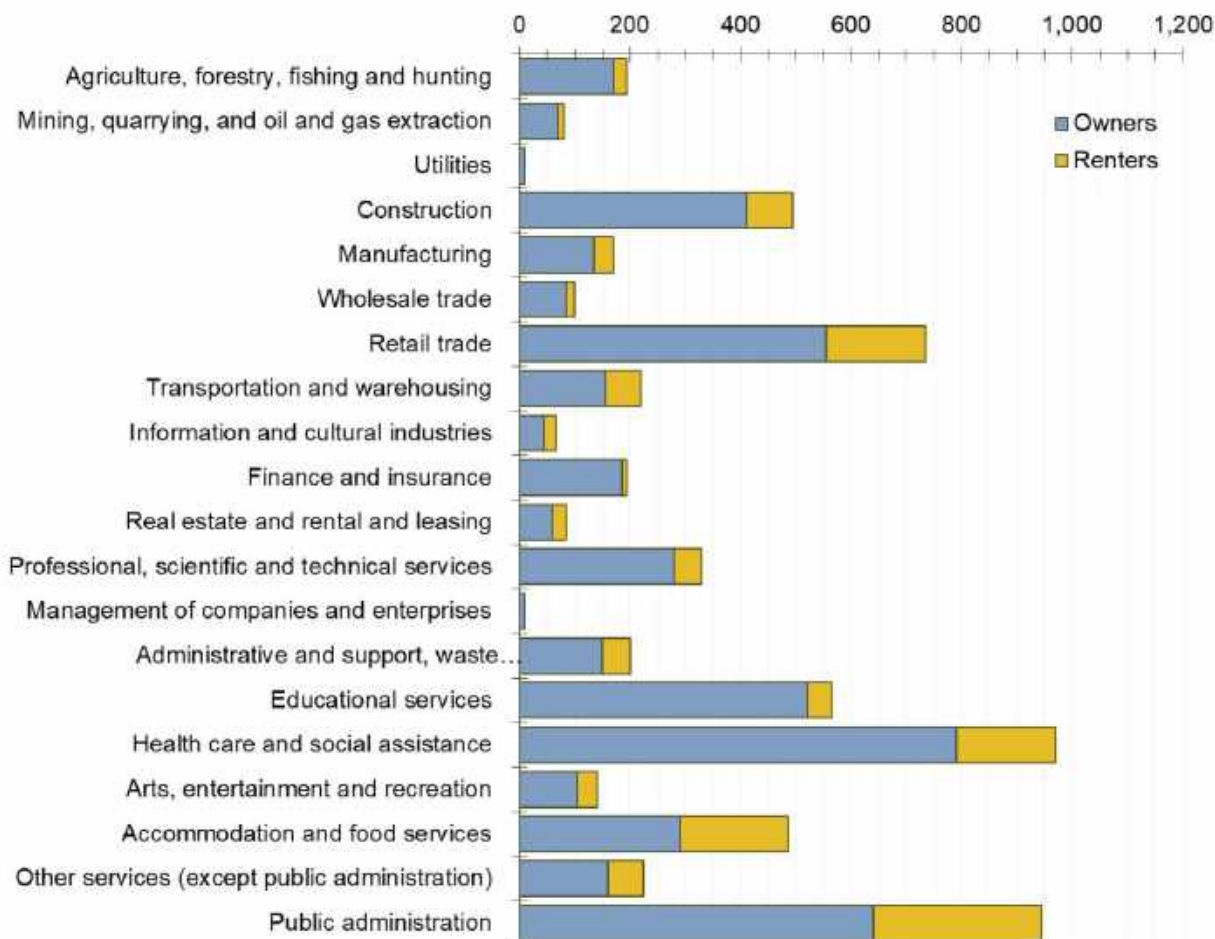
As of 2016, the industries that employed the most Comox residents were: (1) Health Care & Social Assistance – 965 people, (2) Public Administration – 950, and (3) Retail Trade – 740. Because changes between 2006 and 2016 include small totals, any increase or decrease will result in a significant percent change. Consequently, it is difficult to properly assess the condition of each individual industry. Nevertheless, there are some noteworthy trends.

Educational Services had a 43.0 percent increase since 2006, which occurred almost entirely thanks to owner households. Health Care’s rise by 31.3 percent is mostly associated with the new North Island Hospital situated in Courtenay, an effect experienced across the Region. Retail Trade grew by 23.3 percent, which was equal parts thanks to owner and renter households (about 70 persons added from each). Lastly, Construction grew 26.9 percent, likely attributed to increased residential construction activity within the last decade across the CVRD.

Table Com 17.1: NAICS Industry Employment Totals by Tenure, 2006 to 2016 (Statistics Canada)

	Total			'16 % of Total	Owners			Renters		
	2006	2011	2016		2006	2011	2016	2006	2011	2016
Labour Force	5,375	6,470	6,235	100.0%	4,270	5,170	4,830	1,110	1,295	1,405
Agriculture, forestry, fishing and hunting	255	270	195	3.1%	200	195	170	50	80	25
Mining, quarrying, and oil and gas extraction	30	50	80	1.3%	25	50	70	0	0	10
Utilities	10	45	0	0.0%	10	45	10	0	0	0
Construction	390	450	495	7.9%	325	380	410	70	65	85
Manufacturing	195	105	165	2.6%	150	80	135	40	25	35
Wholesale trade	60	85	100	1.6%	50	75	85	15	0	15
Retail trade	600	690	740	11.9%	485	580	555	115	105	180
Transportation and warehousing	165	260	225	3.6%	140	220	155	30	40	65
Information and cultural industries	105	80	70	1.1%	90	50	45	10	25	20
Finance and insurance	175	170	195	3.1%	140	150	185	35	20	10
Real estate and rental and leasing	85	150	85	1.4%	65	135	60	20	15	25
Professional, scientific and technical services	290	385	335	5.4%	270	335	280	20	55	50
Management of companies and enterprises	0	0	0	0.0%	0	0	10	0	0	0
Administrative and support, waste management	215	305	205	3.3%	180	210	150	35	95	50
Educational services	395	675	565	9.1%	360	640	520	35	30	45
Health care and social assistance	735	920	965	15.5%	635	720	790	100	200	180
Arts, entertainment and recreation	80	140	145	2.3%	60	105	105	15	40	35
Accommodation and food services	390	375	485	7.8%	260	285	290	135	95	195
Other services (except public administration)	220	255	225	3.6%	150	200	160	65	60	65
Public administration	980	1,045	950	15.2%	670	710	640	310	340	305

Figure Com 17.1: NAICS Industry Employment Totals by Tenure, 2016 (Statistics Canada)



18. Commuting

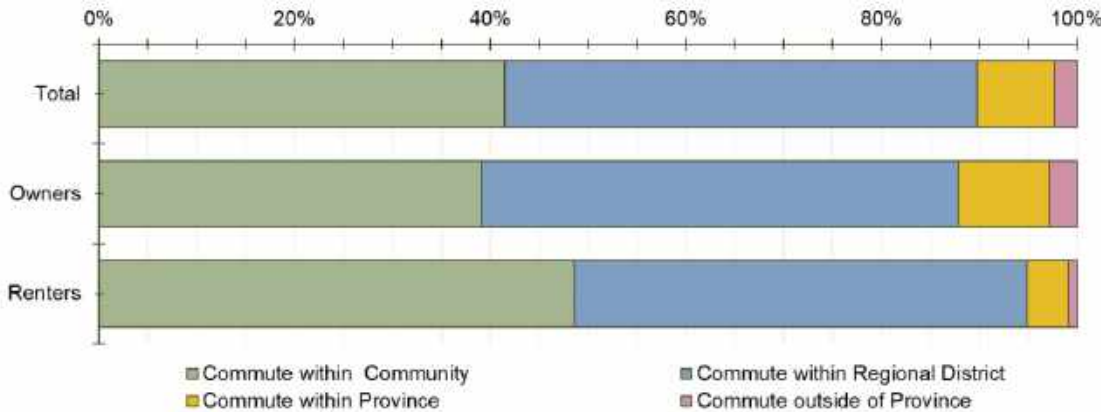
Commute data describes those patterns exhibited by “usual workers”, or those workers that report themselves of generally having the same workplace location at the beginning of each work day. For instance, an office job would typically be classified as a same or usual workplace, whereas contractors (e.g. landscaping or construction), truck drivers, or travelling salespeople would not.

Comox reported 4,565 usual workers in 2016, about 73.2 percent of the total employed labour force. Of those workers, 41.5 percent commuted within Comox, 48.2 percent commuted within CVRD, and 12.3 percent travelled even farther.

Table Com 18.1: Historical Commuting Patterns for Usual Workers (Statistics Canada)

	Total				Owners			Renters		
	2006	2011	2016	'16 % of Total	2006	2011	2016	2006	2011	2016
Total Usual Workers	3,970	4,455	4,565	100%	3,090	3,615	3,500	880	845	1,065
Commute within Community	1,720	1,960	1,895	41.5%	1,205	1,460	1,370	525	505	520
Commute within CVRD	2,050	2,125	2,200	48.2%	1,695	1,850	1,705	345	280	495
Commute within Province	145	300	365	8.0%	140	240	325	10	60	45
Commute outside of Province	50	65	105	2.3%	45	70	100	10	0	10

Table Com 18.1: Commuting Patterns for Usual Workers by Tenure, 2016 (Statistics Canada)



Among tenure types, renters were more likely to commute within the same community (48.8 percent versus 39.1 percent for owners) and less likely to travel external of the CVRD. However, renter commutes within the CVRD have jumped 43.5 percent since 2006, while owners remained constant. Interestingly, usual worker owners travelling outside of the CVRD grew 130 percent (185 to 425) over 10 years.

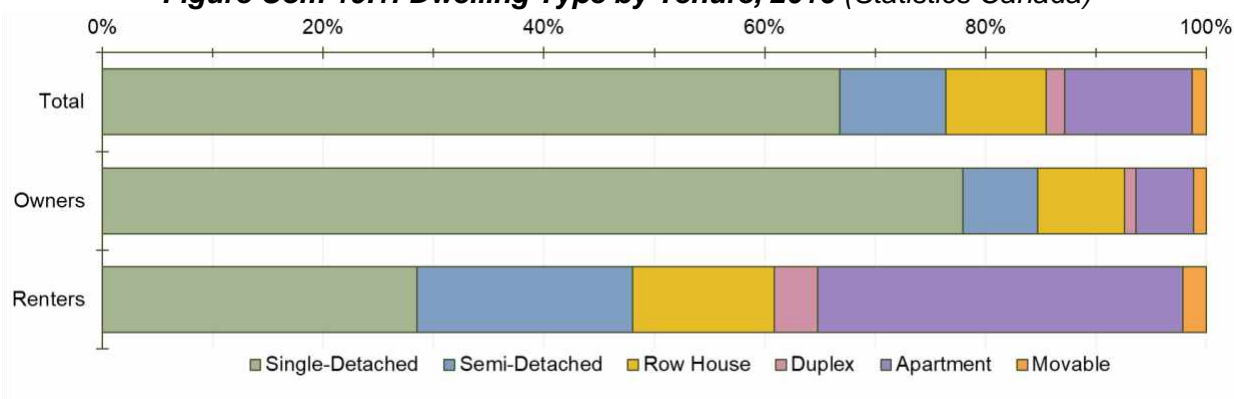
HOUSING

19. Dwelling Types

Comox’s most popular dwelling type is the single-detached home, holding a 66.8 percent share of occupied dwellings in 2016, totalling 4,150. Second is apartments with less than five storeys (there are none documented as equal to or above five storeys), reaching 715 (7.8 percent). Greatest percentage growth across dwelling types occurred in duplexes and movable dwellings,

increasing by 200 percent (to 105) and 167 percent (to 80), respectively. However, single-family homes achieved the greatest actual unit increase – 705 between 2006 and 2016.

Figure Com 19.1: Dwelling Type by Tenure, 2016 (Statistics Canada)



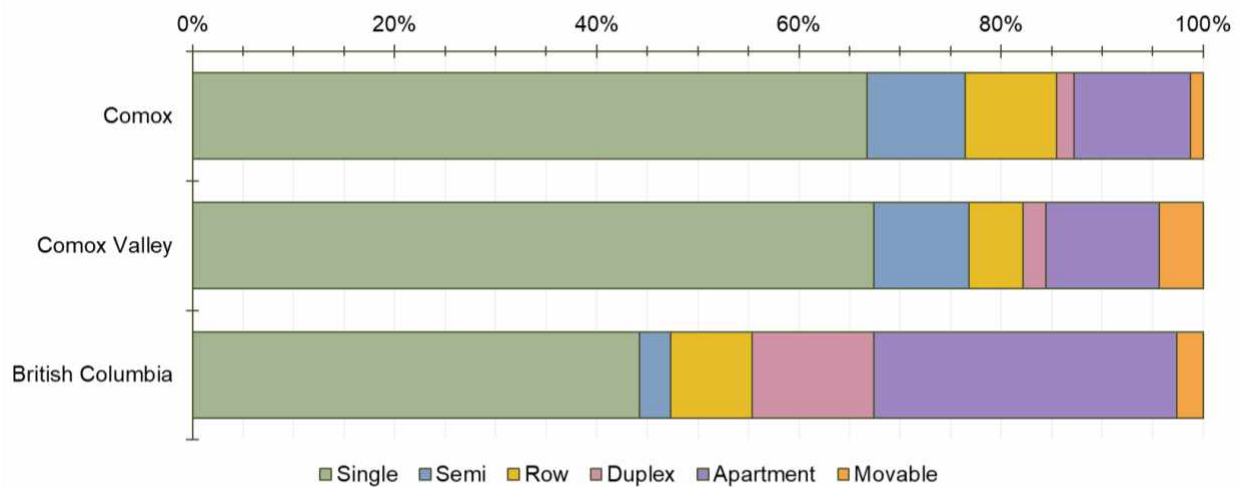
Accommodation tendencies follow the overall expectations of what owners and renters will occupy. Single-detached dwellings were most popular for owners, followed by row houses and semi-detached dwellings; whereas, renters mostly occupied apartments (33.0 percent), followed by semi-detached homes, and rowhouses. Comparatively, renters were about twice as likely (proportionally) to live in a movable dwelling.

Table Com 19.1: Historical Dwelling Type by Tenure (Statistics Canada)

	Total				Owners			Renters		
	2006	2011	2016	'16 % of Total	2006	2011	2016	2006	2011	2016
Total Occupied Dwellings	5,205	5,970	6,210	100%	4,000	4,655	4,795	1,205	1,320	1,410
Single-Detached	3,445	4,045	4,150	66.8%	3,125	3,680	3,745	320	360	400
Apartment (5+ storeys)	0	0	0	0.0%	0	0	0	0	0	0
Other	1,730	1,875	1,980	31.9%	850	925	1,000	880	950	975
Semi-Detached	425	530	600	9.7%	195	290	325	230	240	275
Row House	530	545	565	9.1%	350	340	380	185	205	180
Duplex	35	75	105	1.7%	30	35	50	0	35	55
Apartment (<5 storeys)	740	725	715	11.5%	285	250	250	460	475	465
Other single-attached	0	0	0	0.0%	0	0	0	0	0	0
Movable	30	55	80	1.3%	25	50	55	0	0	30

Overall, Comox closely follows the distribution of Comox Valley's occupied dwelling types with the exception of row house dwellings, which are atypical as a rural unit offering and are less abundant (proportionally) in the City of Courtenay. Conversely, Comox Valley demonstrates noticeably higher rates of movable dwellings, driven by available land in its rural areas that can accommodate the private water and septic requirements that generally serve this dwelling type.

Figure Com 19.2: Dwelling Type, 2016 – Comparison (Statistics Canada)

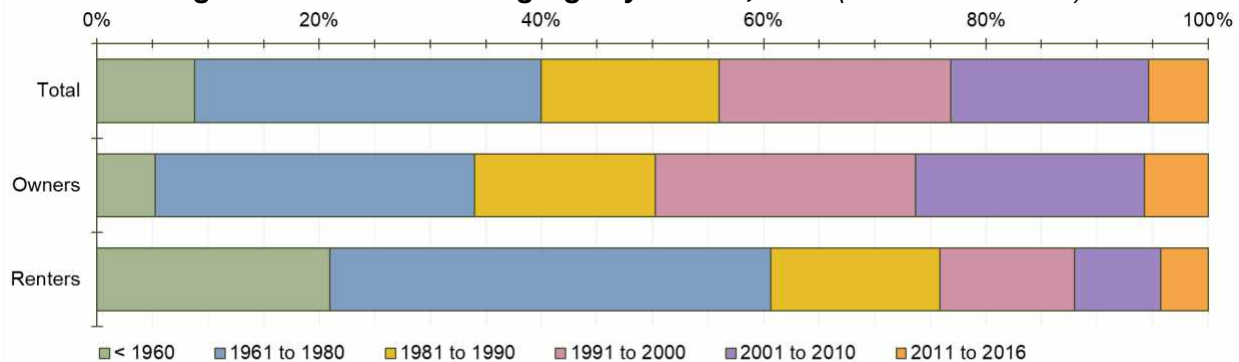


20. Dwelling Age

As of 2016, the most common Comox dwelling age belongs to units built between 1961 and 1980 (31.3 percent). However, a combination of both 1981 to 1990 and 1991 to 2000 measurements periods demonstrates that 36.9 percent of Comox households live in relatively new dwellings. Similarly, since 2001, 1,440 units were built (23.2 percent of total households).

Readers may notice in **Table Com 20.1** that household totals per reported year do vary between census periods. Decreases are partially due to demolished housing stock; however, discrepancies, for both decreases and increases, can be partially associated to changes in the quality of data collection between census periods.

Figure Com 20.1: Dwelling Age by Tenure, 2016 (Statistics Canada)



According to tenure data, 49.8 percent of owner households live in a dwelling built after 1991; whereas, 60.6 percent of renters live in housing pre-dating 1980. The difference reflects general market trends: greater affordability for renters is often found in buildings that have aged and require updating, while owners with sufficient disposable income seek out newer options that require less maintenance or repairs. Furthermore, Comox has historically built units predominantly intended for owners (e.g. 79.1 percent of units built between 2006 and 2016 were owner occupied), which results in relatively less rental housing stock. Accordingly, renter household options trend towards older buildings.

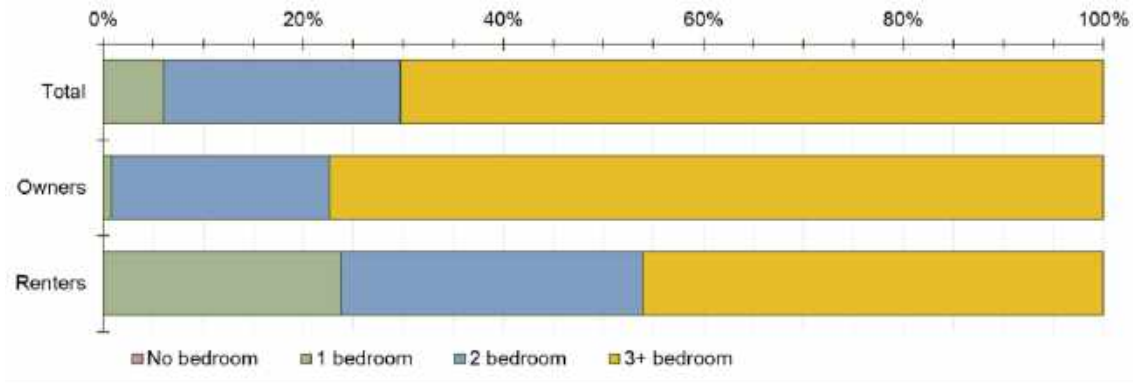
Table Com 20.1: Historical Dwelling Age by Tenure (Statistics Canada)

	Total				Owners				Renters			
	2006	2011	2016	'16 % of Total	2006	2011	2016	'16 % of Total	2006	2011	2016	'16 % of Total
Total Dwellings	5,205	5,975	6,205	100%	4,000	4,655	4,800	100%	1,205	1,315	1,410	100%
< 1960	575	650	545	8.8%	205	320	250	5.2%	370	330	295	20.9%
1961 to 1980	1,845	1,865	1,940	31.3%	1,415	1,320	1,380	28.8%	425	545	560	39.7%
1981 to 1990	905	935	995	16.0%	740	775	780	16.3%	165	155	215	15.2%
1991 to 2000	1,410	1,330	1,295	20.9%	1,250	1,160	1,125	23.4%	160	170	170	12.1%
2001 to 2010	465	1,195	1,105	17.8%	390	1,075	990	20.6%	80	115	110	7.8%
2011 to 2016	0	0	335	5.4%	0	0	275	5.7%	0	0	60	4.3%

21. Bedroom Number

As of 2016, housing units within Comox were typically 3 or more-bedrooms large, occupying 70.3 percent of housing supply. Three or more-bedroom units grew by 24 percent, the greatest change among types – a likely result of the overall increase in single-detached dwellings that can accommodate this number of bedrooms. Two-bedrooms grew by 14.8 percent and 1-bedroom units grew by 5.6 percent. Comox lost all 45 of its no-bedroom units since 2006, either by conversion or demolition.

Figure Com 21.1: Bedroom Number by Tenure, 2016 (Statistics Canada)



Owner housing stocks are predominantly dominated by 3 or more-bedroom units at 77.4 percent, attributed to dwelling type patterns tied to ownership. Owners more often live in singles, semis, or townhouses which can fit more bedrooms. Renter households still favoured 3 or more-bedrooms (46.1 percent), but had greater 1- and 2-bedroom unit options (23.8 and 30.1 percent, respectively). For both tenures, there were more 3 or more-bedroom units added since 2006 than the aggregate of all other sizes.

Table Com 21.1: Historical Bedroom Number by Tenure (Statistics Canada)

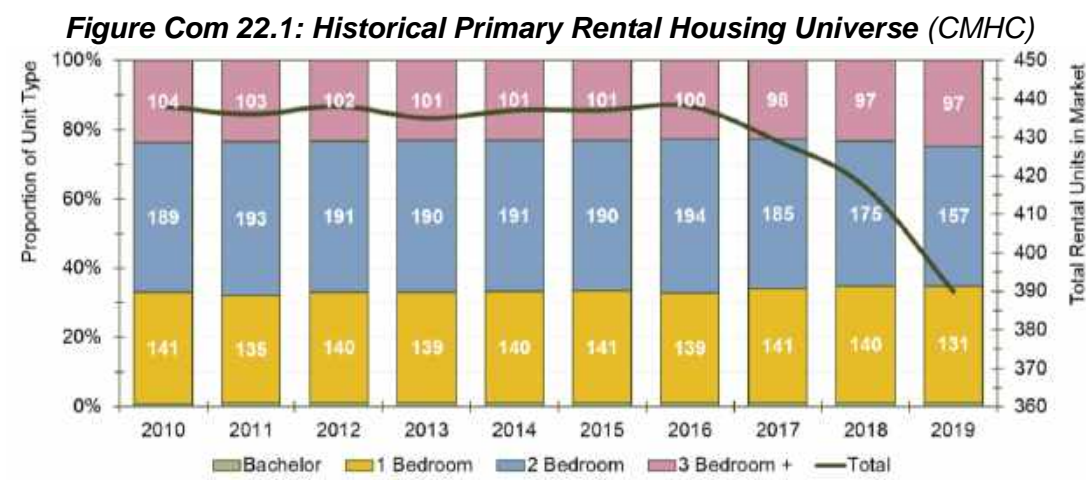
	Total				Owners			Renters		
	2006	2011	2016	'16 % of Total	2006	2011	2016	2006	2011	2016
Total Dwellings	5,205	5,975	6,210	100%	4,005	4,655	4,800	1,200	1,320	1,410
No bedroom	45	0	0	0.0%	0	0	0	45	0	0
1 bedroom	355	335	375	6.0%	70	50	40	285	280	335
2 bedroom	1,280	1,600	1,470	23.7%	875	1,150	1,045	405	450	425
3+ bedroom	3,520	4,035	4,365	70.3%	3,050	3,450	3,715	470	585	650

22. Rental Inventory

The primary rental universe (inventory of rental stock predominantly made up of purpose-built rental buildings) was static in size for most of the last decade. In recent years, this inventory of primary rental housing has decreased, likely due to conversions or demolitions, and this may be related to new development. Data for 2019 shows a total inventory of 390 units, down roughly

10% from the typical levels. However, this data would not yet reflect the addition of 86 new rental units completed in 2019. Adding these into the stock, Comox can be expected to have a total primary rental inventory of 476 units, which would be almost 10% higher than typical levels for the last decade. In other words, though data as of the date of this report shows a shrinking rental housing market, in reality the supply of purpose-built rental units is likely at an all-time high for the last 20 years.

Comparing this data to census Figure Coms on rental households, it can be concluded that most of the rental housing stock operates in the secondary universe; 1410 households reported as being housed in rental dwellings in the 2016 census, however the primary market that year was only 438 units in size, representing 31% of the rental market. Secondary rental market includes housing types such as single or semi-detached units which can easily flip between owner and renter occupied tenures, condominium apartments which are rented out by their owner, larger houses which have been internally converted to rental units, or other smaller multi-unit buildings, like duplexes, which are not captured by the CMHC survey.



The proportional breakdown of the primary rental market by bedroom count has been fairly steady over the past ten years. However, the recent reduction in stock reflected in the current data shows that most of the lost inventory consisted of 2-bedroom units. Data is not yet available to determine the unit types of those recently completed. There are virtually no bachelor/studio style apartment units. The primary rental market is generally focussed more on smaller dwelling units, with 32 percent attributed to 1-bedrooms in 2016 and 44 percent to 2-bedrooms. Secondary rental market units do provide contribute to the 1-bedroom and 2-bedroom unit styles; however, the majority of their stock consists of 3-bedroom or larger dwellings, at about 57 percent in 2016.

Overall, the secondary market contributed 68.9 percent of 2016 rentals, providing the majority of stock across all bedroom numbers: 58.5 percent of 1-bedroom, 54.4 percent of 2-bedroom, and 84.6 percent of 3 or more-bedroom units. The aforementioned numbers are summarized in **Table Com 22.1**, which is derived using 2016 Statistics Canada and CMHC data; anticipated supply is discussed as part of the Regional Context report.

Table Com 22.1: Primary & Secondary Rental Market Units, 2016 (Statistics Canada & CMHC data)

	Total	Rental	Primary		Secondary	
			Market	% of Total	Market	% of Total
Total	6,210	1,410	438	100%	972	100%
No Bedroom	0	0	0	0%	0	0%
1 Bedroom	375	335	139	32%	196	20%
2 Bedroom	1,470	425	194	44%	231	24%
3+ Bedroom	4,365	650	100	23%	550	57%

23. Recent Development Trends

Housing construction in Comox has been somewhat variable, with periods of low and high unit completions. Lower periods of construction typically average around 50 units/year while higher periods are usually in the 100-150 units/year range. Historically, these higher years are associated with both an increase in development of homeowner (freehold) units, as well as the addition of condominium (strata) units. Most of the last 10 years have been a period of low, predominantly single-detached, housing development.

Figure Com 23.1: Historical Unit Completions by Intended Tenure (CMHC)

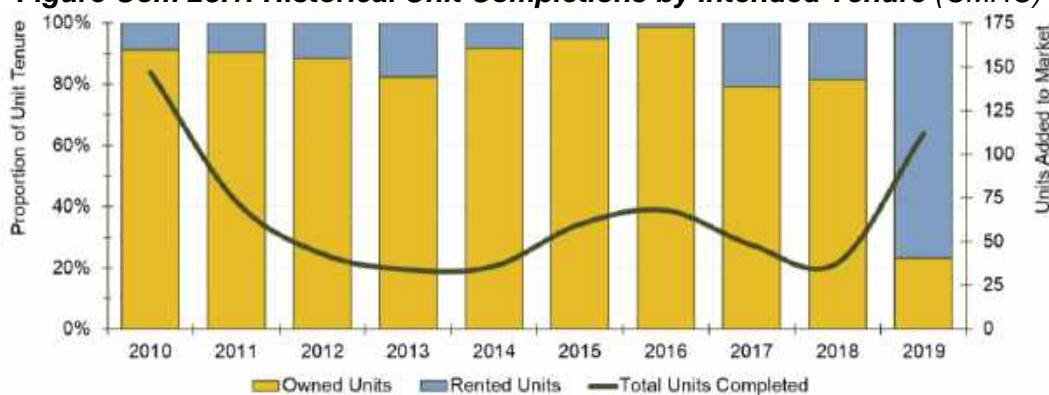


Table Com 23.1: Historical Unit Completions by Intended Tenure (CMHC)

	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total Units	147	73	43	34	36	60	68	48	38	112
Owned	134	66	38	28	33	57	67	38	31	26
Rented	13	7	5	6	3	3	1	10	7	86

Comox has historically built housing with an overwhelming focus on owner-occupied tenures. There was a notable shift in 2019, which saw the vast majority of completed units being intended for the rental market. This is likely the result of the completion of a small number of purpose-built rental projects, and data on housing starts in 2019 suggests completions in 2020 will not repeat this pattern.

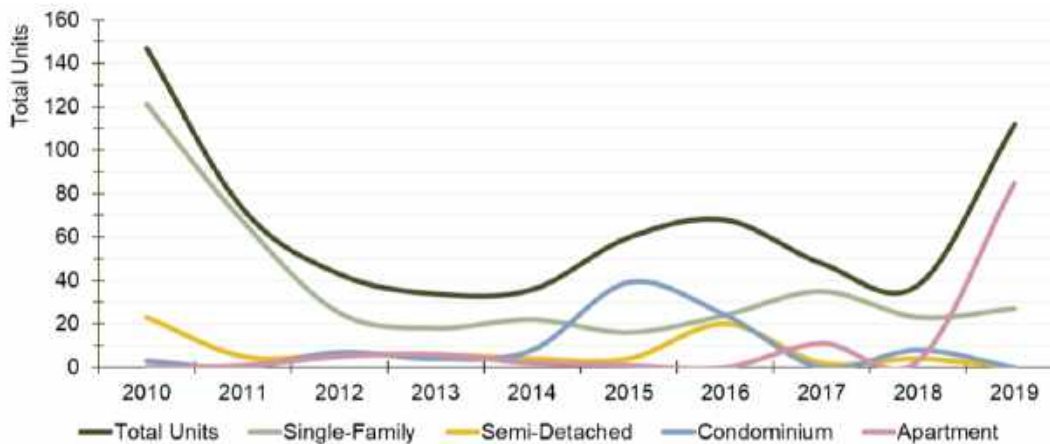
Table Com 23.2: Historical Unit Completions by Dwelling Type (CMHC)

	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total Units	147	73	43	34	36	60	68	48	38	112
Single-Family	121	67	25	18	22	16	24	35	23	27
Semi-detached	23	5	6	6	4	4	20	2	4	0
Condominium	3	0	7	4	8	39	24	0	8	0
Apartment	0	1	5	6	2	1	0	11	3	85

Single-family homes, typically owner-occupied, were the most frequently built dwelling type from 2010 to 2019. Apartment construction remained, as previously mentioned, relatively dormant over the last decade until 2019 where they made up 75.9 percent of the completed stock. Semi-detached homes were also quiet, with light surges in 2010 and 2016. Lastly, condominium apartment completions peaked in 2015, with similar activity in 2016. Some of the condominium increases may be thanks to converted rental apartments, whose numbers show a decline (discussed in the next section).

Please note that New Homes Registry data was collected from BC’s Data Catalogue; however, it offered only information for 2016 to 2018. Consequently, the above discussions use CMHC data, as compared to historical building permits, since historical data is available.

Figure Com 23.2: Historical Completions by Dwelling Type (CMHC)



24. Rental Market – Rent & Vacancy

Given the small size of the primary rental market in Comox, data on rents and vacancy, in particular, can be volatile. Similar data for secondary rental market is not directly available, however it is reasonable to assume that overall trends are similar to those observed in the primary market.

Typically, a primary rental market is considered healthy and balanced when vacancy rates are in the 3 to 5 percent range. Comox has had a persistently low vacancy rate, only rarely exceeding 2% over the last decade. Vacancy has generally been lowest in 3-bedroom units, or larger.

Figure Com 24.1: Historical Rental Housing Vacancy by Unit Type (CMHC)

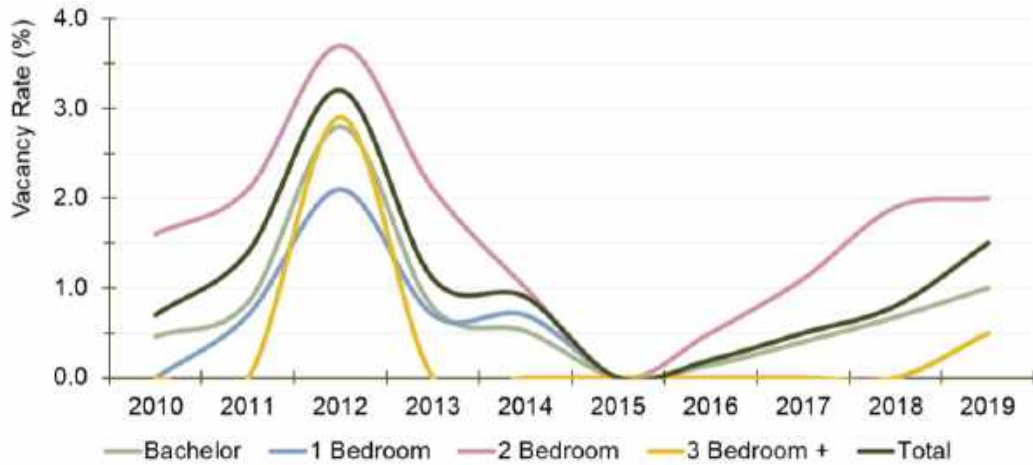


Table Com 24.1: Historical Rental Housing Vacancy by Unit Type (CMHC)

Unit Type	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total	0.7	1.4	3.2	1.1	0.9	0.0	0.2	0.5	0.8	1.5
Bachelor*	0.5	0.8	2.8	0.8	0.5	0.1	0.1	0.3	0.5	1.4
1 Bedroom	0.0	0.7	2.1	0.7	0.7	0.5	0.0	0.0	0.0	2.5
2 Bedroom	1.6	2.1	3.7	2.1	1.0	0.0	0.5	1.1	1.9	2.0
3+ Bedroom	0.0	0.0	2.9	0.0	0.0	0.0	0.0	0.0	0.0	0.5

* Due to the small number of units in total, bachelor style apartment vacancy data is generally suppressed by CMHC. Figure Coms above are estimates based on data available for other unit styles, and should be used cautiously.

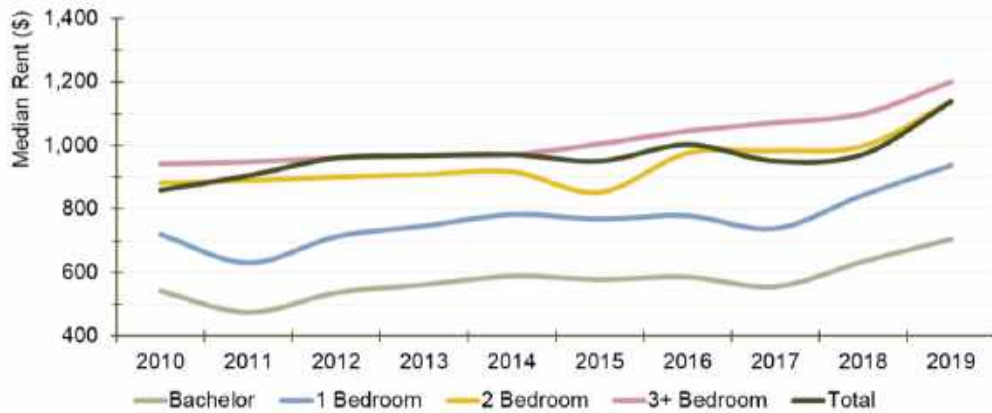
Vacancy rates are a measure of market demand, with low and declining vacancy signalling high, and increasing demand. Accordingly, declining vacancy is a leading indicator of market rents, as prices increase to balance the changing demand with available supply. That said, vacancy can decrease without major price changes, but once unit availability hits a critical threshold of very low vacancy, rents tend to react disproportionately. Within this context, price increases generally lag a year or more as the impact of low vacancy ripples through the market.

Despite consistently low vacancy rates, rents in Comox tended to increase gradually year to year. Market conditions did not get extremely tight until around 2015-2017. Accordingly, market rents have increased more rapidly in recent years: while the overall change in rents for the past decade is an increase of nearly 47 percent, more than half of the increase (26 percent) has occurred only since 2017.

Table Com 24.2: Historical Median Market Rents by Unit Type, 2019 dollars (CMHC)

Unit Type	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total	\$775	\$825	\$875	\$875	\$885	\$865	\$925	\$900	\$950	\$1,138
Bachelor	\$489	\$433	\$489	\$508	\$537	\$527	\$541	\$527	\$621	\$706
1 Bedroom	\$650	\$575	\$650	\$675	\$713	\$700	\$719	\$700	\$825	\$938
2 Bedroom	\$795	\$810	\$820	\$820	\$835	\$775	\$900	\$930	\$975	\$1,138
3+ Bedroom	\$850	\$865	\$875	\$875	\$885	\$915	\$965	\$1,015	\$1,075	\$1,200

Figure Com 24.2: Historical Median Market Rents by Unit Type, 2019 dollars (CMHC)



25. Ownership Market – Prices & Sales

The previously discussed trends in Comox’s rental market are likely a product of trends in its owner-occupied market. Conditions were fairly stable for most of the last decade; however, 2017 to 2019 saw a general strengthening trend in market conditions. As demand and prices increased across the board in the owner-occupied market, citizens at the lower end increasingly turned to the rental market for housing, resulting in the vacancy and price trends noted previously.

Days on market shows the length of time a property listing takes to find a buyer. It is therefore a measure of market demand; the ownership equivalent to vacancy rates. The early 2010s were largely stable, if declining slightly. In the latter part of the past decade, demand showed a significant increase, with days on market in 2017 to 2019 dropping by 50 to 80 percent depending on unit type. Single-family houses typically showed the strongest demand; however, between 2018 to 2019, this housing type had the longest days on market Figure Coms (though still very low).

Figure Com 25.1: Historical Average Annual Days on Market by Dwelling Type (Vancouver Island Real Estate Board - VIREB)

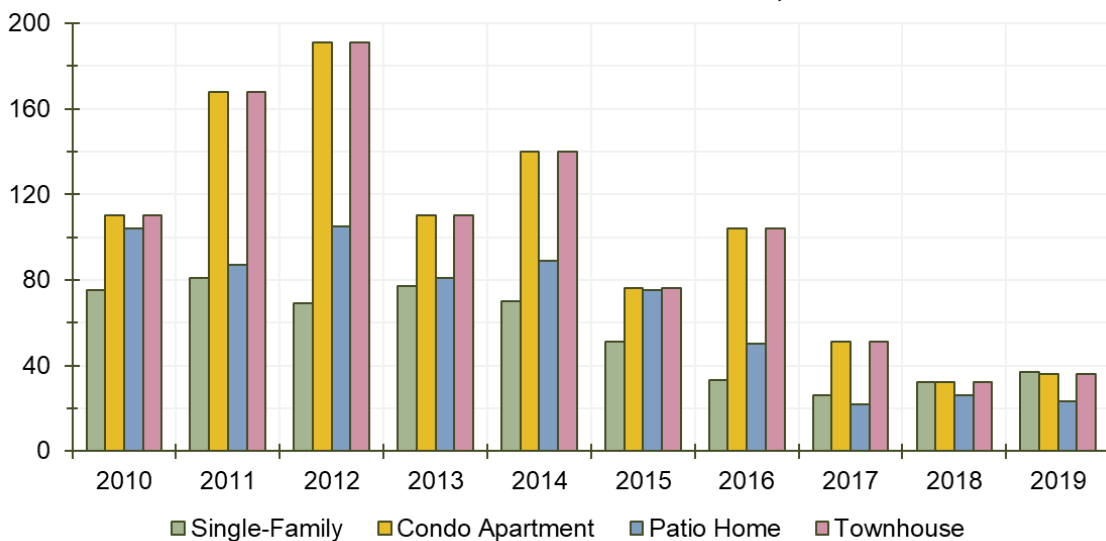


Table Com 25.1: Historical Average Annual Days on Market by Dwelling Type (VIREB)

Dwelling Type	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total	83	96	88	83	83	59	56	33	31	35
Single-Family	75	81	69	77	70	51	33	26	32	37
Condo Apartment	110	168	191	110	140	76	104	51	32	36
Patio Home	104	87	105	81	89	75	50	22	26	23
Townhouse	110	168	191	110	140	76	104	51	32	36

This period of increasing market demand also matches with notable patterns of market activity in terms of total number of sales. Coincident with days on market, total sales volumes were fairly stable for most of the last ten years in Comox. A notable decrease in total sales was observed for single-family dwellings, which was largely offset by an increase in the sales of all other dwelling types. Typically, this pattern is indicative of market prices for detached housing increasing beyond the reach of citizens who then choose to purchase other, less expensive housing types in its place. Notably, townhouse and condo apartments showed significant increases in sales volumes over this period. Additionally, this pattern could also indicate speculative behaviour on the part of current home owners who, observing strong price appreciation, choose to hold on to property in the hopes of achieving higher future sales prices.

Figure Com 25.2: Historical Annual Sales Volume by Dwelling Type (VIREB)

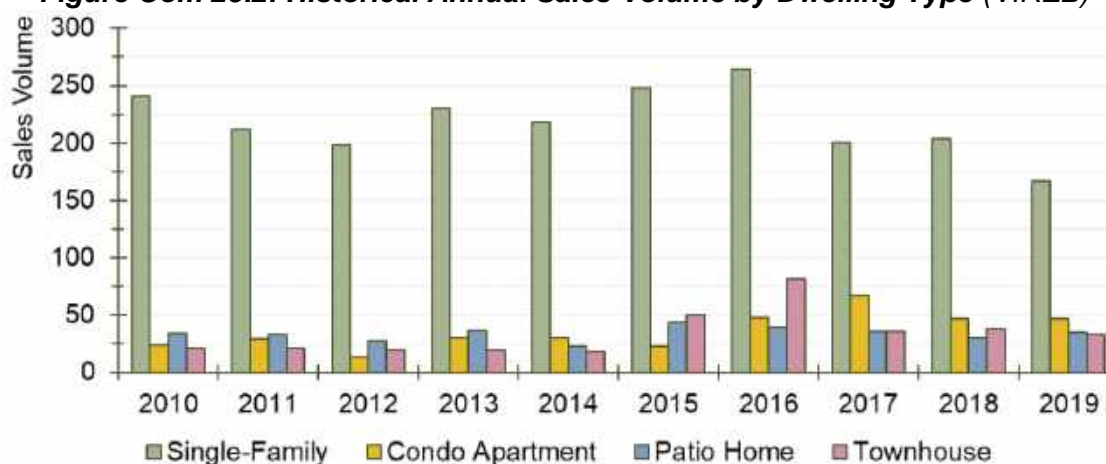


Table Com 25.2: Historical Annual Sales Volume by Dwelling Type (VIREB)

Dwelling Type	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total	320	295	259	317	289	365	434	340	319	282
Single-Family	241	212	198	230	218	248	264	201	204	167
Condo Apartment	24	29	13	30	30	23	48	67	47	47
Patio Home	34	33	28	37	23	44	40	36	30	35
Townhouse	21	21	20	20	18	50	82	36	38	33

Price action in Comox's housing market matches with the demand patterns already discussed. Annual price changes were mixed for the early 2010s, but showed an increase across all dwelling types starting in 2016, peaking in 2017 at a dramatic 20 to 30 percent year over year increase, and generally continuing at a lower pace to the present. The most recent year in particular indicated that the market price for most dwelling types remaining steady after the recent escalation. Condo apartments showed the strongest price appreciation and unlike all other types, continued to increase strongly in 2019. This is likely due to their comparatively lower starting point

for price, their relative affordability compared to other housing types, and possibly demographic factors driving demand to smaller housing forms.

Figure Com 25.3: Historical Year/Year Housing Price Change by Dwelling Type (VIREB)

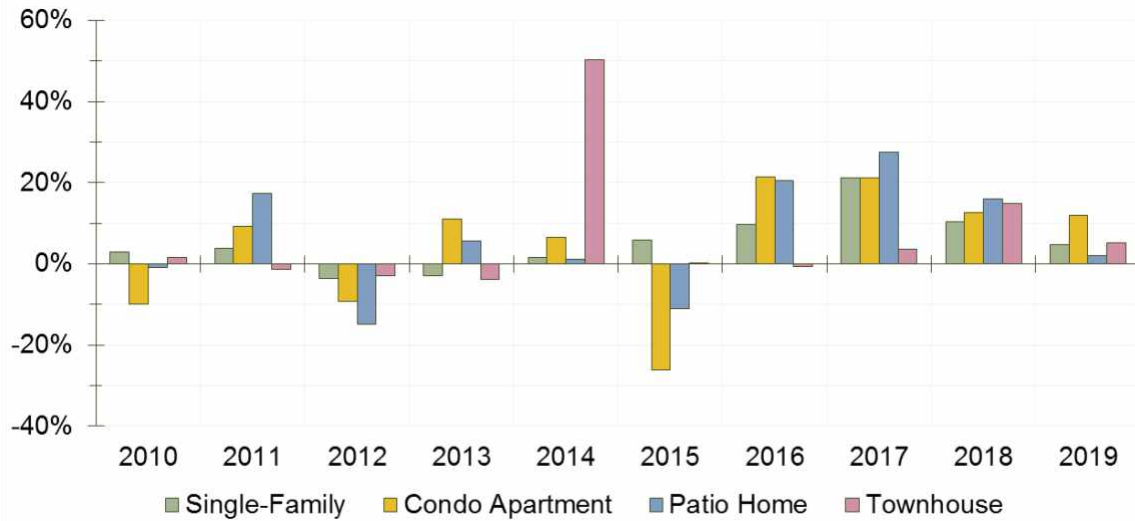


Table Com 25.3: Historical Year/Year Housing Price Change by Dwelling Type (VIREB)

Dwelling Type	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total	1%	4%	-4%	-2%	4%	0%	7%	19%	13%	4%
Single-Family	3%	4%	-4%	-3%	1%	6%	10%	21%	10%	5%
Condo Apartment	-10%	9%	-9%	11%	7%	-26%	21%	21%	13%	12%
Patio Home	-1%	17%	-15%	6%	1%	-11%	20%	28%	16%	2%
Townhouse	1%	-1%	-3%	-4%	50%	0%	-1%	4%	15%	5%

Accordingly, median sale price across all dwelling types in Comox was generally stable for most of the past 10 years, with a significant increase observed in 2017-2018, which slightly decreased in 2019. The overall price in 2019 was 32 percent higher than the 2010 to 2016 average.

Figure Com 25.4: Historical Median Sale Price by Dwelling Type, 2019 Dollars (VIREB)

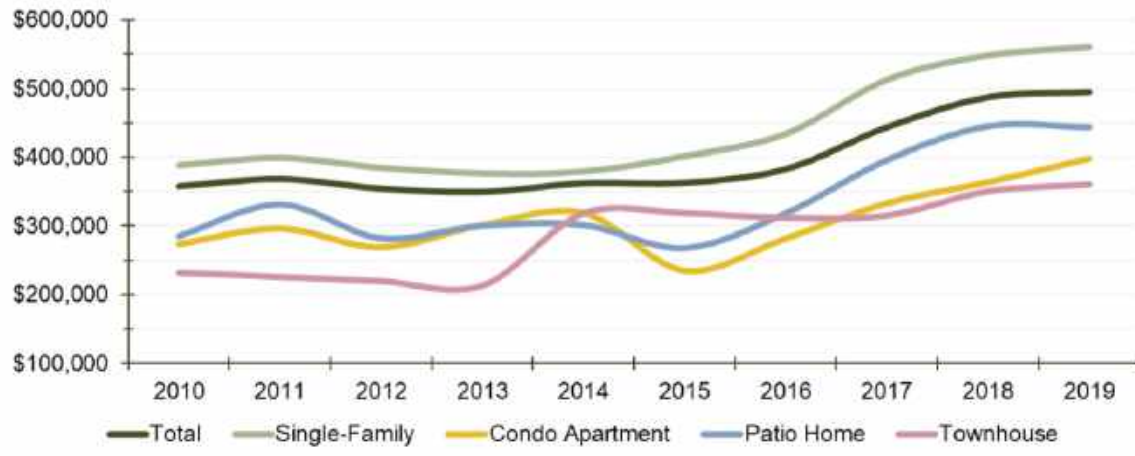


Table Com 25.4: Historical Median Sale Price by Dwelling Type, 2019 Dollars (VIREB)

Dwelling Type	2010	2011	2012	2013	2014	2015	2016	2017	2018	2019
Total	\$358,259	\$368,868	\$354,780	\$350,039	\$362,608	\$363,293	\$383,108	\$443,763	\$487,355	\$495,115
Single-Family	\$387,953	\$398,801	\$384,326	\$376,311	\$378,835	\$401,148	\$433,535	\$512,376	\$547,665	\$560,000
Condo Apartment	\$273,862	\$296,220	\$269,028	\$301,049	\$318,441	\$235,194	\$281,798	\$332,780	\$363,404	\$397,500
Patio Home	\$285,504	\$331,328	\$282,205	\$300,495	\$301,421	\$268,165	\$318,648	\$396,167	\$445,298	\$444,000
Townhouse	\$231,729	\$226,005	\$219,615	\$213,058	\$317,866	\$318,171	\$311,507	\$314,821	\$350,096	\$360,000

26. Short-term Rentals (AirBnB)

Over the last decade, short-term rentals (STRs) have grown significantly as a new form of residential property tenure. An STR is a more fluid and flexible use of residential dwelling space for temporary accommodations that blurs the line between rental housing and commercial hospitality use. At the epicentre of the STR boom is the technology company AirBnB, an internationally used STR marketplace that connects STR “landlords” and users. Since 2016 AirBnB, and the STR market with it, have experienced exponential growth worldwide.

Alongside this market growth is concern about the impact of STR units on traditional residential market sectors. There has been notable concern by local residents and governments in the Comox Valley region about STR impacts on the availability of long-term rental housing; specifically, whether STRs are removing traditional rentals from the market, thereby reducing supply and causing greater difficulty for households to find a suitable place to live. This concern is exacerbated by the general lack of authoritative data on the extent of local STR markets as AirBnB, and other platforms like it, are private companies that do not publish data on their users.

The following discussion aims to identify the actual number of units that are potentially being removed from the market, and whether the developing trends warrant immediate concern. To do so requires the use of third-party data provided by the company AirDNA, which provides monthly (as of January 2016) data on STR markets, scraped from the public-facing websites of several STR platforms, including AirBnB. This report analysed available data and applied the following definitions to the exercise:

Total market: all short-term rental units that were active (meaning, offering lodging) within a given time period.

Commercial market: all short-term rental units that were active within a given time period but are available more than 50 percent of the days that they have been active. For instance, if a property was active in 2017 and provided availability for 200 days (about 55 percent of the year), it would be considered as “commercial” as the primary use of the unit is for STR accommodations, rather than being a minority use of a residential dwelling. In other words, the 50 percent cut off is meant to separate residents using the service to create supplemental income from their dwellings, from non-resident STR operators using the unit principally for income/investment purposes.

Additional Notes

The data includes listings from several STR platforms. In examining the data, it was noted that AirBnB accounted for the vast majority of listings (>90%), with other platforms mostly serving as another avenue to advertise properties which were also available on AirBnB. To minimise double-counting units, only data for listings on AirBnB are used.

In this report, market types are divided into “entire unit” and “other.” The former means an STR listing that is the entirety of an apartment or dwelling, while the latter can be a room in

a dwelling, a hotel room, or other type. For the purpose of this analysis, only “entire unit” listings are considered to represent units that may be impacting traditional housing market sectors.

According to **Table Com 26.1**, the overall Comox STR market has grown to about 88 individual units in 2019, up 8 units since 2018 and 28 since 2017. Overall, 75 percent of the Comox STR market was “entire units.”

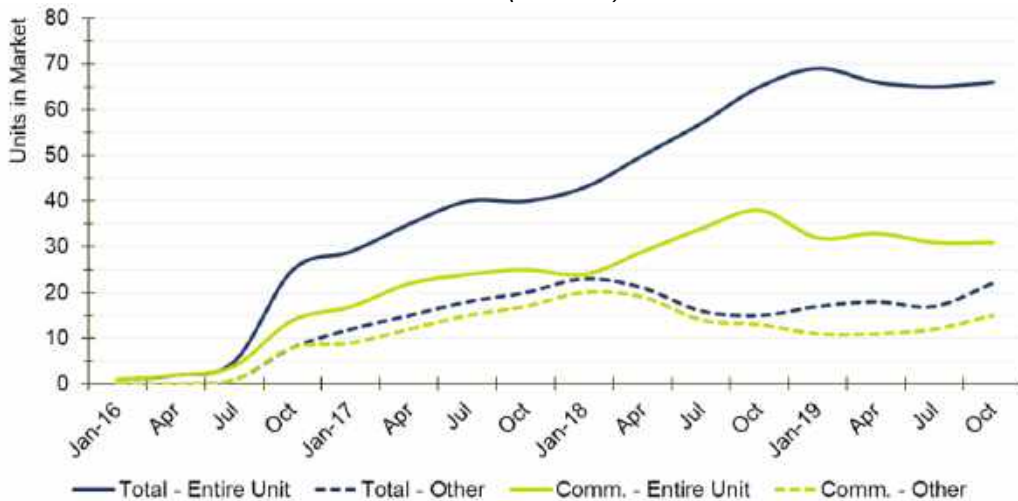
Table Com 26.1: Historical STR Market – Total versus Commercial Market (AirDNA)

	2016				2017				2018				2019			
	Jan	Apr	Jul	Oct	Jan	Apr	Jul	Oct	Jan	Apr	Jul	Oct	Jan	Apr	Jul	Oct
Total Market	1	2	6	33	41	50	58	60	66	71	73	80	86	84	82	88
Entire Unit	1	2	5	25	29	35	40	40	43	50	57	65	69	66	65	66
Other	0	0	1	8	12	15	18	20	23	21	16	15	17	18	17	22
Commercial Market	1	2	5	22	26	34	39	42	44	48	48	51	43	44	43	46
Entire Unit	1	2	4	14	17	22	24	25	24	29	34	38	32	33	31	31
Other	0	0	1	8	9	12	15	17	20	19	14	13	11	11	12	15

Both the overall and commercial market have maintained relatively steady growth over the last four years (see **Figure Com 26.1**), with the latter retaining about half the entire unit market. Total active commercial entire units peaked at 38 in October 2018. In 2019, commercial entire units made up approximately 47 percent of the overall entire unit market.

The Comox peak of 38 commercial units in mid-2018 represented about 0.5 percent of total unit demand, and 2.5 percent of rental demand. There is no way to conclude how many of these units would convert to renter or owner housing if they had not been listed on an STR website.

Figure Com 26.1: Historical Cumberland STR Market (Comox) – Total versus Commercial Market (AirDNA)

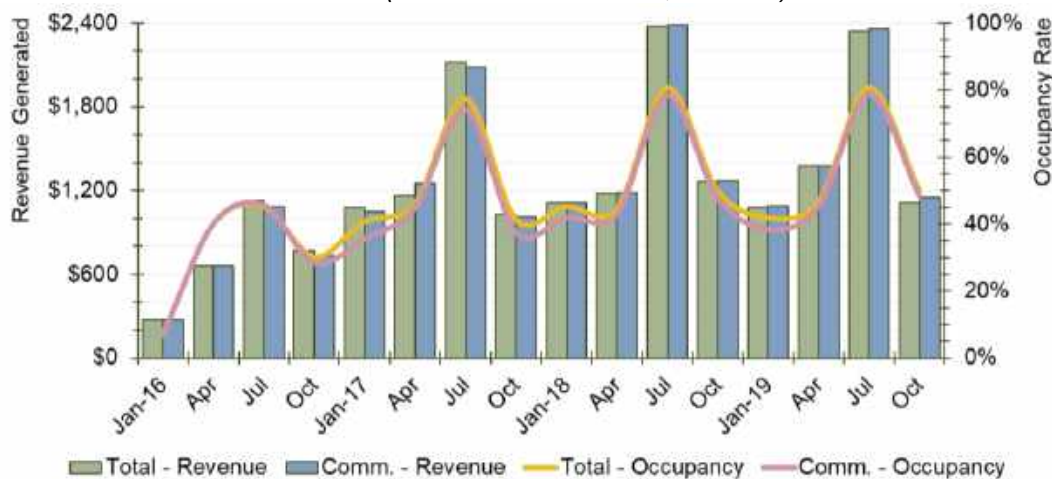


Regional revenue data provides interesting insights into the profitability of commercial AirBnBs. Specifically, that the median revenue of commercial units has remained at par with the total market (mostly since it holds the majority of units and thus influences the trend). Similarly, the median nightly asking price has remained relatively constant at around \$110 to \$120 (adjusted for inflation to October 2019). **Table** and **Figure Com 26.2** illustrate the parallel revenue generation and booking occupancy over time for both markets.

Table Com 26.2: Historical CVRD STR Occupancy & Revenue – Total versus Commercial Market (October 2019 dollars, AirDNA)

	2016				2017				2018				2019			
	Jan-16	Apr	Jul	Oct	Jan-17	Apr	Jul	Oct	Jan-18	Apr	Jul	Oct	Jan-19	Apr	Jul	Oct
Total Market																
Occupancy	7%	40%	45%	30%	41%	46%	77%	41%	45%	44%	81%	50%	42%	47%	81%	50%
Median Rate	\$136	\$70	\$98	\$99	\$106	\$106	\$111	\$105	\$104	\$108	\$120	\$107	\$122	\$113	\$121	\$106
Median Revenue	\$272	\$663	\$1,128	\$767	\$1,077	\$1,164	\$2,116	\$1,024	\$1,109	\$1,180	\$2,376	\$1,262	\$1,075	\$1,376	\$2,342	\$1,111
Commercial Market																
Occupancy	7%	40%	46%	29%	36%	45%	74%	38%	42%	43%	78%	48%	38%	45%	79%	48%
Median Rate	\$136	\$70	\$97	\$100	\$106	\$110	\$114	\$105	\$106	\$109	\$120	\$106	\$122	\$114	\$121	\$107
Median Revenue	\$272	\$663	\$1,083	\$736	\$1,051	\$1,252	\$2,083	\$1,012	\$1,109	\$1,184	\$2,387	\$1,270	\$1,091	\$1,378	\$2,362	\$1,150

Figure Com 26.2: Historical STR Occupancy & Revenue – Total versus Commercial Market (October 2019 dollars, AirDNA)



27. Non-Market Housing

The Town of Comox does not contain any non-market housing options associated with BC Housing in the form of emergency shelters, transitional and assisted living, or independent social housing units. Consequently, those seeking non-market options are generally directed towards the City of Courtenay, which is the major provider.

Nevertheless, Comox does have 129 households (as of March 2019) receiving BC Housing rental assistance program support; 32 families and 97 seniors.

Figure Com 27.1: Non-Market Housing, March 2019 (BC Housing)

	Comox	Comox Valley	% of Total
Emergency Shelter / Homeless Housing			
Homeless Housed	0	52	0.0%
Homeless Rent Supplements	0	60	0.0%
Homeless Shelters	0	14	0.0%
Transitional Supported / Assisted Living			
Frail Seniors	0	111	0.0%
Special Needs	0	31	0.0%
Women and Children Fleeing Violence	0	14	0.0%
Independent Social Housing			
Low Income Families	0	235	0.0%
Low Income Seniors	0	58	0.0%
Rent Assistance in Private Market			
Rent Assist Families	32	191	16.8%
Rent Assist Seniors	97	417	23.3%
Community Total	129	1,183	10.9%

There is a present need for more non-market housing options in Comox. As of January 2020, the BC Housing wait list for subsidised units has 31 applications from local households, specific to: 8 families, 12 residents with disabilities, 9 seniors, and 2 single persons.

28. Subsidized Housing

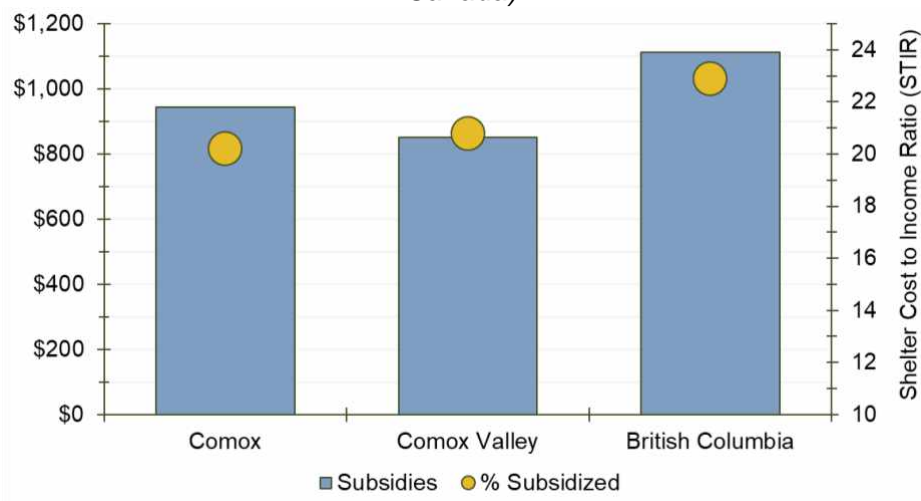
Of the 6,210 Comox households, about 22.7 percent are renters – a slight proportional decrease since 2006 but an actual household increase of 205 since the same year. In 2016, 13.8 percent of those renter households received a form of subsidy to help pay for their rental accommodation.

Table Com 28.1: Historical Median Shelter Cost & Renter Subsidized Housing (Statistics Canada)

	2006	2011	2016
Total - Owner & Renter	5,205	5,975	6,210
Median Shelter Cost	\$753	\$741	\$943
Renters	1,205	1,315	1,410
In Subsidized Housing	0	225	195
% Renters	23.2%	22.0%	22.7%
% Subsidized	0.0%	17.1%	13.8%

At 22.7 percent, Comox's renter population is the lowest, proportionally, when compared to CVRD and British Columbia – though only 1.2 percent off of the Region's 23.9 percent. Nevertheless, Comox reported the highest subsidy rate of the compared geographies. Given that Comox has little in the way of non-market housing options, it is not surprising that rental subsidies are comparatively common.

Figure Com 28.1: Renter Households versus Subsidized Households, 2016 (Statistics Canada)



29. Homelessness

Point-in-Time (PiT) counts of persons experiencing homelessness were produced in 2018 the Government of British Columbia and several public and private partners. The data illustrates what is occurring over the entirety of the Comox Valley Regional District, inclusive of the communities of Comox, Courtenay, Cumberland, and Denman Island. Because the data is regional in scope, it is discussed in greater detail within the CVRD Regional Profile Report.

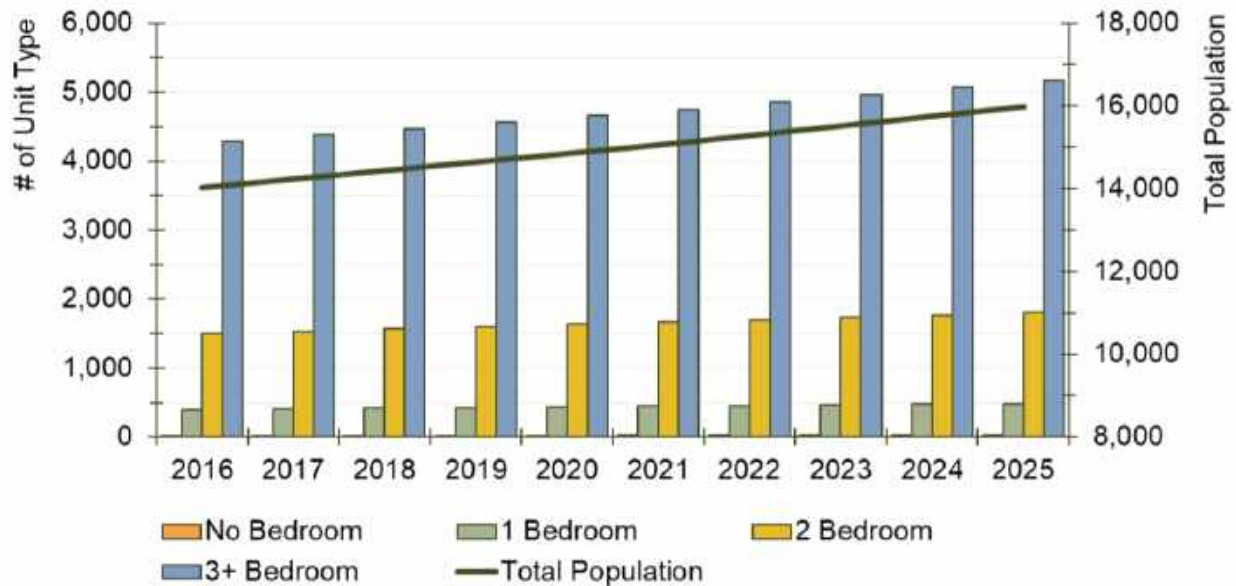
HOUSING NEED

30. Anticipated Household Demand

The housing market for Comox is functionally integrated with its neighbouring communities. Examining future housing demand, and supply in particular, solely on the basis of individual communities within the broader market can be misleading, and therefore this Housing Needs Analysis contains a fulsome discussion of housing demand and supply in the section specific to this broader context, the Comox Valley Regional District. This report section, specific to the Town of Comox, focusses on the projected housing demand in terms of units and tenure.

Projected demand for housing is derived from the population projections discussed in the Demographic section of this report. Using data for age-specific household sizes, the projected number of residents in Comox is translated into a projected number of households. This method takes into account both the changes in total number of people, as well as changes to the age profile of that population. Each household is anticipated to create demand for one dwelling unit, and the distribution of unit types and tenures is based on trends in the observed proportional breakdown of the housing stock for these factors. Finally, the total number of demanded units is adjusted to account for units required to house non-usual residents (e.g. student housing or second homes) and baseline 'slack' in the market.

Figure Com 30.1: Projected Population and Housing Demand by Unit Type (2016 to 2025)



Using this method, housing demand in Comox can be expected to reach 7,495 units in 2025, an increase of 865 units over 2019 for an average annual increase of 144 units. Overall, about 23 percent of this demand will be for rental-tenured units. Furthermore, anticipated housing demand versus total population will translate to declining household sizes, from 2016’s 2.2 to 2.08 in 2025.

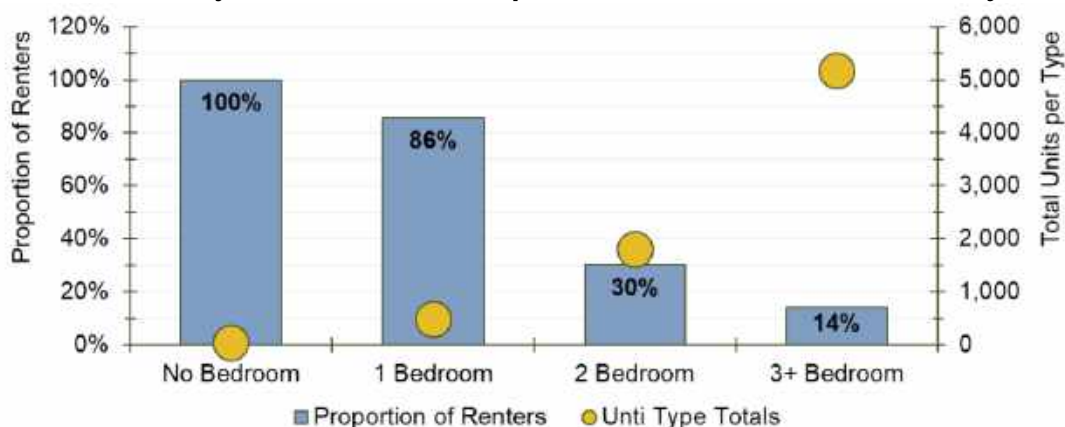
Table Com 30.1: Projected Housing Demand by Unit Type, Household Size, & Rental Proportion, 2016 to 2025

	2016	2017	2018	2019	2020	2021	2022	2023	2024	2025
Total Population	14,025	14,235	14,440	14,650	14,855	15,065	15,295	15,530	15,765	15,995
Total Households	6,210	6,350	6,490	6,630	6,770	6,895	7,045	7,195	7,345	7,495
No Bedroom	25	25	25	25	25	30	30	30	30	30
1 Bedroom	400	410	420	430	440	445	455	465	475	485
2 Bedroom	1,500	1,535	1,570	1,605	1,640	1,665	1,700	1,735	1,770	1,805
3+ Bedroom	4,285	4,380	4,475	4,570	4,665	4,755	4,860	4,965	5,070	5,175
Household Size	2.20	2.18	2.17	2.16	2.14	2.13	2.12	2.10	2.09	2.08
Renter Demand	22.9%	23.0%	23.0%	23.1%	23.1%	23.0%	23.0%	23.0%	23.0%	23.0%

Demand for rental units is not evenly spread through the total unit type projections. Applying the historical breakdown of owners and renters by unit type to the projected demand, it is evident that rental demand is highly concentrated in smaller unit sizes, though a sizable minority of larger, family-friendly rental units will also be required.

No-bedroom units (bachelor/studio style apartments) are a very minor segment of the current housing stock, and are expected to remain as such; all are anticipated to be rentals.

Figure Com 30.2: Projected Demand & Proportion of Rental Tenure in 2025 by Unit Type



31. Housing Condition (Adequacy)

In 2016, Statistics Canada reported that 4.3 percent of households lived in a dwelling inadequate for their needs. Statistics Canada defines “adequacy” as a structure that requires only minor repair or periodic maintenance. Accordingly, any unit that requires major repair is “inadequate.”

Table Com 31.1: Historical Inadequate Housing by Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Households	5,095	5,855	6,100	3,930	4,590	4,740	1,170	1,260	1,355
Below Adequacy Standard	235	275	265	175	200	160	65	75	105
1 person household	90	75	80	70	50	35	15	30	50
2 persons household	75	115	95	45	100	50	35	0	40
3 persons household	30	35	40	20	0	40	10	0	0
4 persons household	30	25	35	25	15	25	10	0	0
5+ persons household	15	0	20	10	0	15	0	0	0
Inadequate Housing (%)	4.6%	4.7%	4.3%	4.5%	4.4%	3.4%	5.6%	6.0%	7.7%

Housing adequacy is closely tied to the age of the housing stock within a community. For instance, owner households experienced a relative drop in inadequate housing since 2006 (4.5 to 3.4 percent), while renters had worsening conditions (5.6 to 7.7 percent). Relatedly, owners typically occupy new housing stock (newer than 1991); whereas, the majority of renters live in units built before 1980. Generally, older buildings will require greater repair or maintenance than newer construction, which amplifies over time if necessary, improvements are not made. In 2016, renters were more than two times more likely to experience inadequate housing than owners.

Figure Com 311: Historical Inadequate Housing by Tenure, % (Statistics Canada)

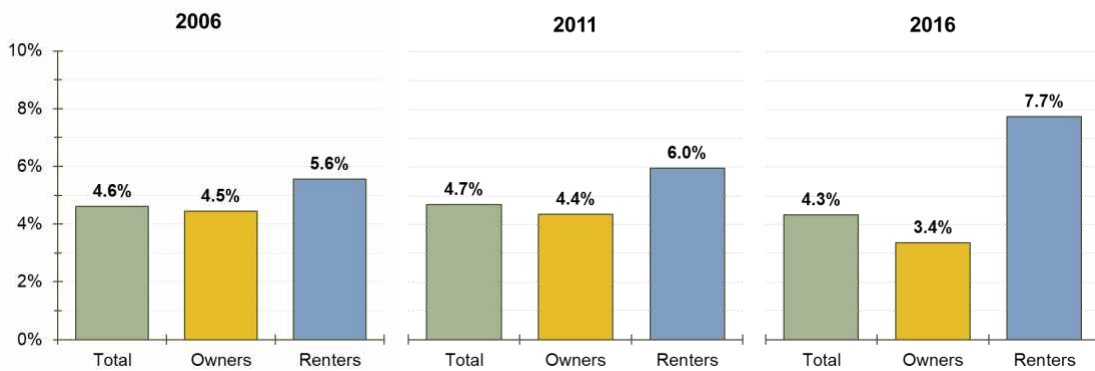
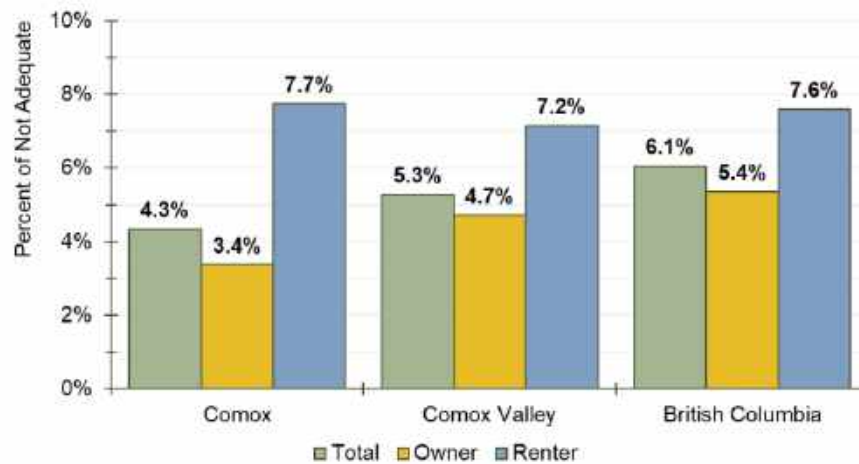


Figure Com 31.2 - Inadequate Housing by Tenure, 2016 – Comparison (Statistics Canada)



Overall, Comox demonstrates a noticeably lower rate of inadequacy compared to CVRD and BC – 5.3 and 6.1 percent, respectively. Better housing conditions in Comox are mostly supported by owner households; Comox dwellings occupied by renters have greater need for repair than both the Region and Province. In addition, Comox was the only jurisdiction compared above that had increasing inadequacy since 2006.

32. Overcrowding (Suitability)

In 2016, 0.6 percent of Comox households lived in an unsuitable dwelling. Statistics Canada defines “suitability” as whether a structure has enough bedrooms for the size and composition of the household. Accordingly, any unit that does not have enough bedrooms is “unsuitable.”

Table Com 32.1: Historical Unsuitable Housing by Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Households	5,095	5,855	6,100	3,930	4,590	4,740	1,170	1,260	1,355
Below Suitability Standard	70	115	35	30	70	10	40	40	20
1 Person	0	0	0	0	0	0	0	0	0
2 Persons	15	0	0	0	0	0	10	0	10
3 Persons	15	30	10	10	0	0	0	0	0
4 Persons	10	0	10	0	0	0	10	0	10
5+ Persons	30	65	10	15	40	10	15	25	0
Unsuitable Housing (%)	1.4%	2.0%	0.6%	0.8%	1.5%	0.2%	3.4%	3.2%	1.5%

Both owner and renter households experienced decreases in their proportions of unsuitable housing since 2006. Owners dropped from 0.8 to 0.2 percent, while renters dropped from 3.4 to 1.5 percent. Unsurprisingly, 3 or more person households had greater probability of experiencing unsuitable housing than smaller household sizes.

Figure Com 32.1: Historical Unsuitable Housing by Tenure, % (Statistics Canada)

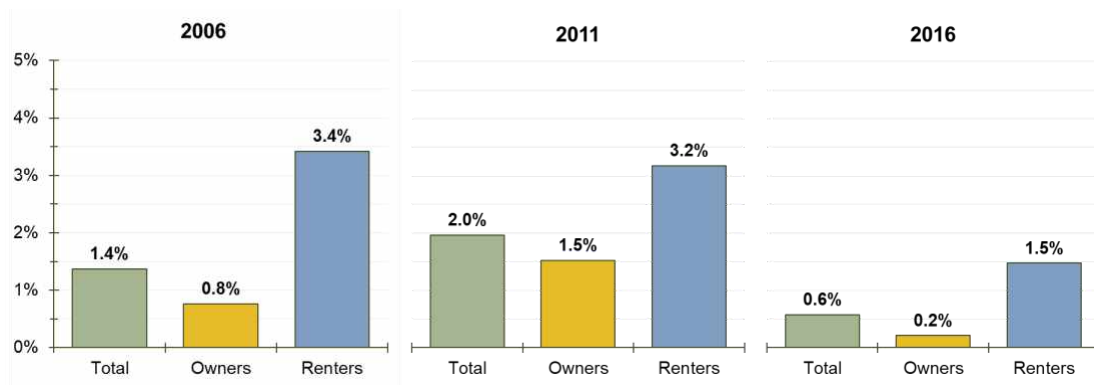
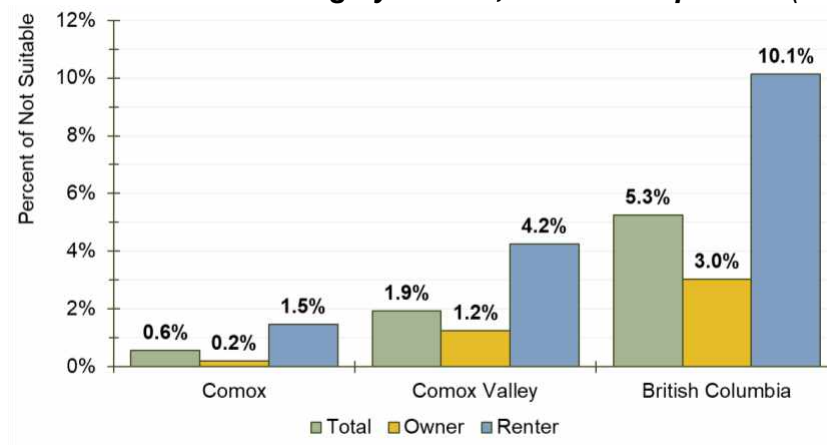


Figure Com 32.2: Unsuitable Housing by Tenure, 2016 – Comparison (Statistics Canada)



For all tenures, Comox has lower rates of unsuitability than the CVRD and BC, which experience 1.9 and 5.3 percent rates, respectively. All jurisdictions improved from 2006, suggesting that either new construction is satisfying market demand or that households have overall moved to alternative housing that meets their needs.

33. Affordability

Statistics Canada defines “affordable” as whether a household spends less than 30 percent of its overall income on shelter expenses (including utilities, taxes, condo fees, rent, or mortgage payment). Accordingly, any household spending equal to or more than 30 percent is considered as experiencing a housing affordability problem.

Table Com 33.1: Historical Unaffordable Housing by Tenure (Statistics Canada)

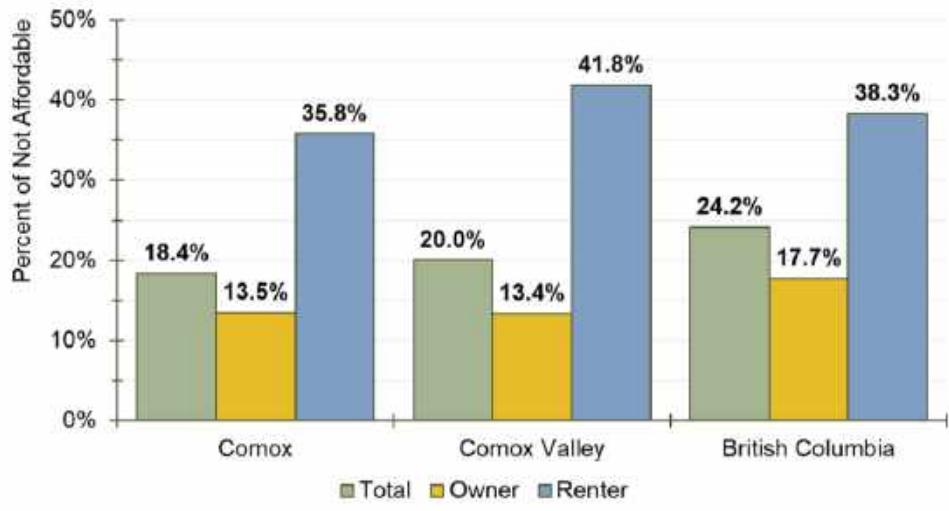
	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Households	5,095	5,855	6,100	3,930	4,590	4,740	1,170	1,260	1,355
Above Affordable Threshold	780	1,090	1,120	365	555	640	410	535	485
1 person household	425	550	555	175	185	260	250	365	290
2 persons household	205	295	320	105	225	205	100	70	110
3 persons household	85	135	105	35	50	55	50	80	50
4 persons household	60	85	95	45	75	75	10	0	25
5+ persons household	0	25	50	10	15	45	0	0	10
Unaffordable Housing (%)	15.3%	18.6%	18.4%	9.3%	12.1%	13.5%	35.0%	42.5%	35.8%

Between 2006 and 2016, the proportion of households living in unaffordable accommodation rose from 15.3 percent to 18.4 percent, reaching 1,120. Both owners and renters experienced worsening affordability conditions, though owners appear to have taken the most significant hit. Owner unaffordability rose 4.2 percent; whereas, renters rose 0.8. As has been previously discussed, the price of both owner and rental market housing has been increasing over time, adjusted for inflation. Large appreciations in housing prices over the last decade have made owner housing particularly more expensive, driven by higher mortgage principals and associated mortgage payments.

Figure Com 331: Historical Unaffordable Housing by Tenure (Statistics Canada)



Figure Com 33.2: Unaffordable Housing by Tenure, 2016 – Comparison (Statistics Canada)



Compared to the CVRD and BC, Comox appears more affordable, particularly for renter households. Although this demonstrates a positive for the Town, there is a lingering question of how long Comox households will remain better off for shelter costs than larger markets. To explain, Comox was the only compared geography to have an increase in its unaffordability rates; both the CVRD and BC declined slightly during the same time, meaning they are becoming more affordable over time. For now, Comox is technically more affordable, but is not progressing as it should relative to provincial trends.

34. Core Housing Need

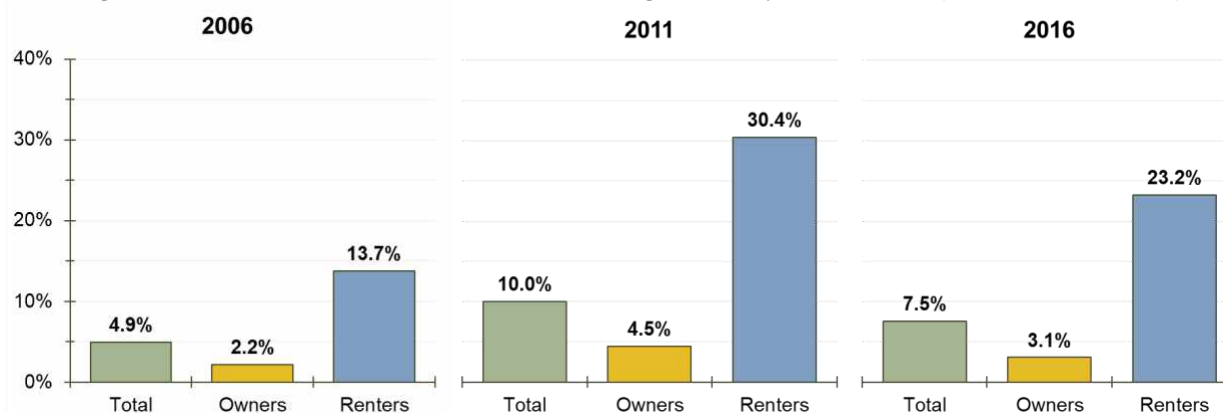
Statistics Canada defines “Core Housing Need” as a household whose dwelling is considered inadequate, unsuitable, or unaffordable, and whose income levels are such that they could not afford alternative housing in their community. In other words, it considers the three variables previously discussed and contextualises them within the greater context of the community.

Table Com 34.1: Historical Core Housing Need (CHN) by Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Households	5,095	5,850	6,095	3,930	4,590	4,740	1,165	1,265	1,355
Household not in CHN	4,845	5,265	5,635	3,845	4,380	4,595	1,005	880	1,040
Household in CHN	250	585	460	85	205	145	160	385	315
1 person household	105	325	255	55	90	70	55	235	180
2 persons household	85	130	130	20	75	45	65	55	85
3 persons household	40	95	50	10	0	20	30	85	35
4 persons household	20	20	10	10	20	0	10	0	10
5+ persons household	0	15	15	0	0	10	0	0	10
Household in CHN (%)	4.9%	10.0%	7.5%	2.2%	4.5%	3.1%	13.7%	30.4%	23.2%

In 2016, the Town of Comox had 460 households (7.5 percent) that were in Core Housing Need, up from 4.9 percent in 2006. Proportional to their respective totals, both owners and renters are now worse off than they were in 2006 – owner need rose from 2.2 to 3.1 percent, while renters nearly doubled from 13.7 to 23.2 percent. The most considerable increase, from both a unit and percent change perspective, occurred in 1-person renter households; this accounted for 59.5 percent of the overall increase.

Figure Com 34.1 - Historical Core Housing Need by Tenure, % (Statistics Canada)

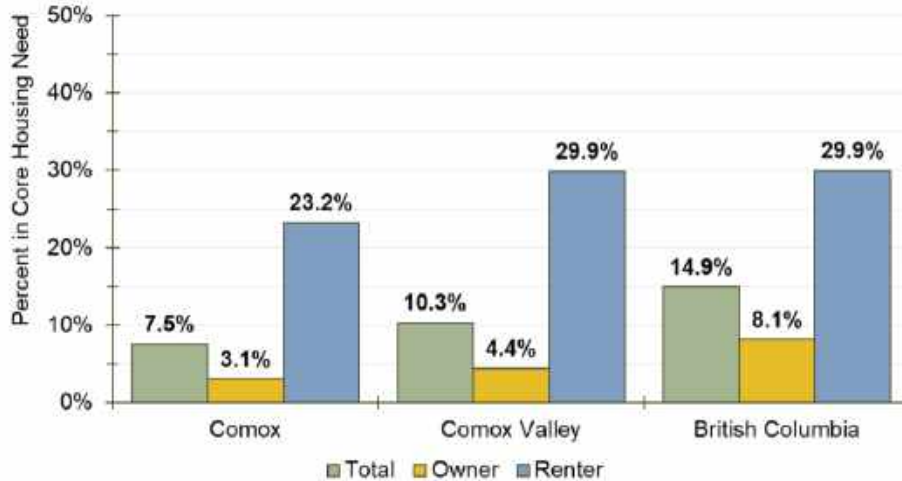


It is important to note that if no household had an alternative housing option for their relative income, then the rate of Core Housing Need would equate to the highest percentage between inadequate, unsuitable, and unaffordable households. For instance, Comox’s rate of unaffordable

housing is 18.4 percent, yet its rate of Core Housing Need is 4.9 percent, suggesting that the 13.50 percentage point difference could be due to households having other, more affordable options elsewhere in the community (according to Statistics Canada).

The difference between the unaffordable and Core Housing Need rates increased marginally since 2006, which had a 10.4 percentage point margin, further suggesting that the affordability problem may not be solely related to an unaffordable housing stock, but partially to households specifically deciding to spend more (perhaps in exchange for quality, size, or location of the unit).

Figure Com 34.2 - Core Housing Need by Tenure, 2016 – Comparison (Statistics Canada)



Like the rates of unaffordability, Comox has better Core Housing Need metrics than that of the Regional District and the Province, for both owner and renter households. What differs from unaffordability is that all compared geographies have increasing rates of overall Core Housing Need. CVRD and BC did experience slight decreases in owner need, but rose for renter need. Comox’s degree of worsening for renter need does mark a significant difference from the other jurisdictions; however, the degree of change is partially attributed to the smaller sample size for which small deviations are amplified.

Based on Provincial data, recent immigrants face considerable need at 25.2 percent. However, Comox and Comox Valley have lower immigrant rates than the Province, signifying that need may be most dire in particular age cohorts. According to 2016 census information for BC, 15.5 percent of children between 0 to 14 had greatest Core Housing Need (the highest of any cohort). This may indicate that those households most in need are young families with children (whether couples or lone parent).

35. Extreme Core Housing Need

Extreme Core Housing Need modifies the definition of Core Housing Need via its affordability metrics; instead of measuring affordability by a 30 percent threshold, it uses 50 percent. The result is a demonstration of how many households are truly experiencing dire housing circumstances. As discussed above, some households may actually choose to live in more expensive circumstances; however, the 50 percent adjustment largely removes these situations from consideration, apart from a few outliers.

Table Com 36.1 - Historical Extreme Core Housing Need (ECHN) by Tenure (Statistics Canada)

	Total			Owners			Renters		
	2006	2011	2016	2006	2011	2016	2006	2011	2016
Total Households	5,095	5,850	6,095	3,930	4,590	4,740	1,165	1,265	1,355
Household not in ECHN	4,875	5,340	5,620	3,870	4,375	4,585	1,000	995	1,060
Household in ECHN	110	255	235	30	115	85	80	140	150
1 person household	60	175	140	15	65	40	45	105	95
2 persons household	30	45	60	15	35	30	20	0	25
3 persons household	20	35	20	0	0	0	20	25	15
4 persons household	0	0	10	0	0	0	0	0	10
5+ persons household	0	0	10	0	0	0	0	0	0
Household in ECHN (%)	2.2%	4.4%	3.9%	0.8%	2.5%	1.8%	6.9%	11.1%	11.1%

In 2016, 235 Comox households were in Extreme Core Housing Need (3.9 percent), up from 2.2 percent in 2006. Proportional to their respective totals, both owners and renters are worse off than they were in 2006 – owner extreme need rose from 0.8 to 1.8 percent (85 households), while renter extreme need jumped from 6.9 to 11.1 percent (150 households). Renters are about 6 times more likely to experience Extreme Core Housing Need.

Figure Com 35.1: Historical Extreme Core Housing Need by Tenure, % (Statistics Canada)

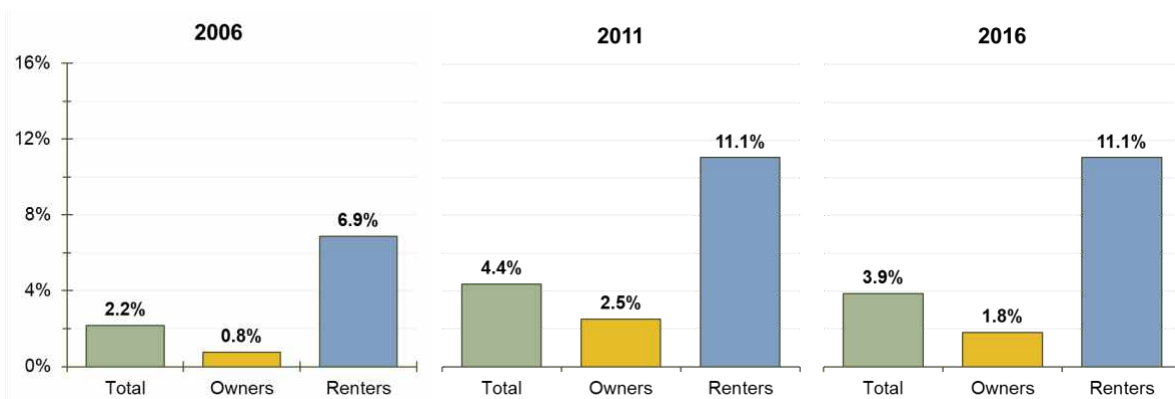
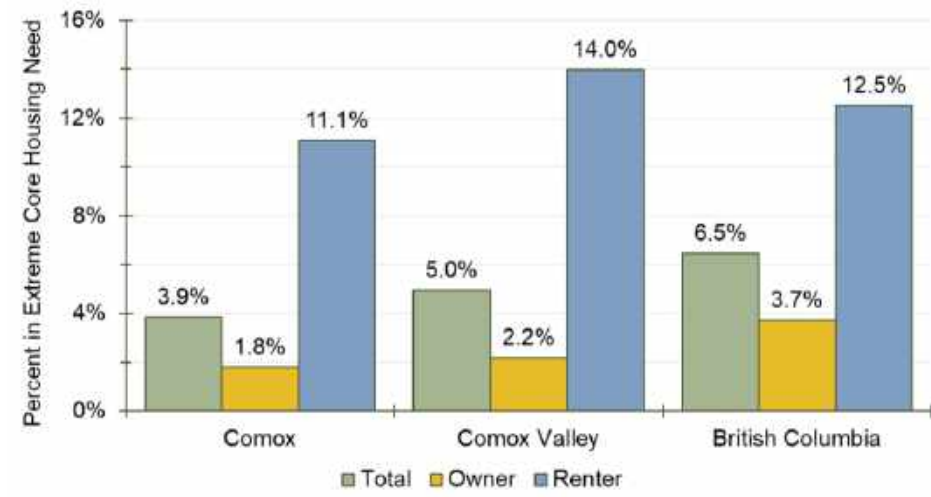


Figure Com 35.2: Extreme Core Housing Need by Tenure, 2016 – Comparison (Statistics Canada)



Comox demonstrates lower rates of Extreme Core Housing Need than both CVRD and BC – 5.0 and 6.5 percent, respectively. Comox Valley’s overall rate fell from 2006 to 2016 for both renter and owner households, while BC’s rose slightly, mostly due to a small rise in dire rental affordability. Much like traditional Core Housing Need, Comox’s degree of worsening for renters is significant compared to the other geographies, though it is once again partially attributed to the smaller sample size for which small deviations are amplified.

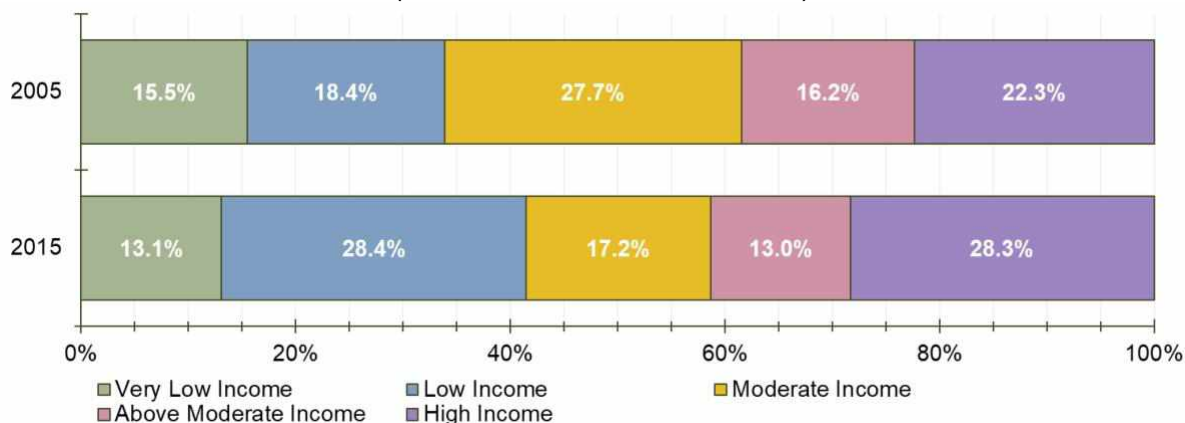
36. Affordability Gap

Each individual or household has a different financial relationship with the accommodation that they occupy. Some live in dire financial circumstances that cannot be avoided due to the market; whereas, others voluntarily choose a type of dwelling that exceeds typical thresholds of affordability, despite the presence of less expensive housing options if they feel it is a compromise that better meets their lifestyle needs. Since it is impossible to express every household’s experience, this report chooses to develop specific income categories. The intent is to facilitate discussion around groups of households with different financial capacity.

The household income categories are defined as follows:

- very low income** – making less than 50 percent of median income;
- low income** – making between 50 and 80 percent of median income;
- moderate income** – making between 80 and 120 percent of median income;
- above moderate income** – making between 120 and 150 percent of median income; and
- high income** – those making above 150 percent of median income.

Figure Com 37.1: Historical Before-Tax Income Categories, 2015 dollars
(derived from Statistics Canada)



As depicted in **Figure Com 36.1**, the share of households earning a high-income increased by about 6 percent since 2005. The only other category to rise (proportionally) were those in low income, up 10 percent over the same period.

Households in very-low-income decreased over the 10-year period by 2.4 percent. This would normally be indicative of a positive trend; however, the actual change in total very-low-income households negligibly changed from 2005 to 2015; 810 households. This indicates that the change is mostly due to increasing total households that earn higher incomes. Notably, the number of high-income households grew 51.3 percent, exceeded only by the low-income growth of 84.3 percent.

Table Com 36.1: Historical Households Before-Tax Income Categories, 2015 dollars
(derived from Statistics Canada)

Year	Very Low		Above Moderate		High
	Low	Moderate	Moderate	High	
2015	810	1,760	1,065	805	1,755
2010	940	1,165	1,490	930	1,450
2005	805	955	1,440	840	1,160

Decreases in moderate and above moderate households suggests there has been movement in the amount of before-tax income that households are earning, whether decreasing or increasing. The changes can be due to individuals having worked longer, thus commanding greater salaries, or people retiring which would typically reduce annual earnings. Regardless, the greatest impact appears to be from the number of people entering the market.

As discussed, the chosen income categories are defined by thresholds related to median income (e.g. very low is below 50 percent of the median). Based on those thresholds, we can:

- 1) determine the maximum income achievable by a particular group;
- 2) calculate what an affordable monthly payment or dwelling price would be (based on the 30 percent affordability threshold); and
- 3) compare these calculations to median market rents and median house prices.

Please note that this exercise rounds rents and dwelling prices for simplicity; that affordable dwelling values assume a 10 percent down payment, a 3 percent interest rate, and a 25-year amortization period; and that median income will grow by the historical growth rate until 2019 to facilitate a comparison.

Table Com 36.2: Income Level Ownership & Rental Cost Gaps, 2019 dollars

Income Category	Affordable (30%)			Rent Gap				Sale Price Gap			
	Maximum Income	Monthly Payment	Dwelling Value	Bachelor	1-Bedroom	2-Bedroom	3+ Bedroom	Single Family	Condo Apt.	Patio Home	Town House
Very Low	\$35,176	\$879	\$206,050	\$279	-\$61	-\$271	-\$321	-\$353,950	-\$191,450	-\$237,950	-\$153,950
Low	\$56,282	\$1,407	\$329,680	\$807	\$467	\$257	\$207	-\$230,320	-\$67,820	-\$114,320	-\$30,320
Moderate	\$84,423	\$2,111	\$494,520	\$1,511	\$1,171	\$961	\$911	-\$65,480	\$97,020	\$50,520	\$134,520
Above Moderate	\$105,528	\$2,638	\$618,151	\$2,038	\$1,698	\$1,488	\$1,438	\$58,151	\$220,651	\$174,151	\$258,151
Median Income	\$70,352	\$1,759	\$412,100	\$1,159	\$819	\$609	\$559	-\$147,900	\$14,600	-\$31,900	\$52,100

The results of **Table Com 36.2** illustrate which income categories can or cannot afford certain accommodation types, and by how much. Red table cells indicate that the particular household would exceed their affordable budget for that unit by the dollar value provided; green cells indicate when the unit is below budget.

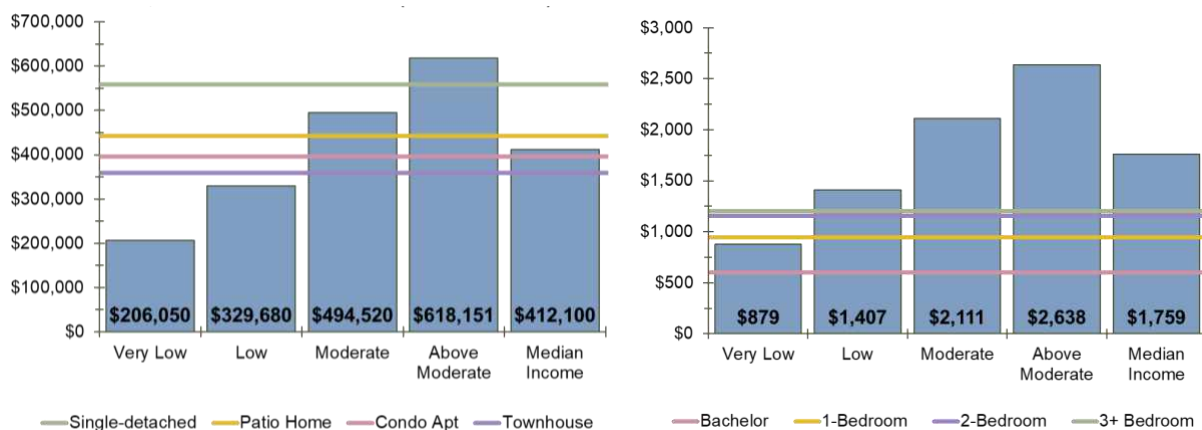
To summarize, a very-low-income household (of which there are a maximum of 810) could potentially afford a bachelor unit, but cannot afford any other rental size or conventional dwelling type. All other income groups can reasonably afford all rental types (based on maximum attainable incomes). For home ownership, very-low- and low-income households cannot reasonably afford all dwelling type prices; all higher categories can afford to own, with the exception of single-family homes for moderate-income households.

Figure Com 36.2 graphically represents the result of **Table Com 36.2**. For instance, the left graphic for ownership shows that a moderate-income household cannot afford a single-detached

home at its maximum income since the affordable purchase price generated by said income does not surpass the horizontal line attributed to that dwelling type.

Please note that high income households are not displayed in either the table or graph since no maximum can be reasonably set for this category.

Figure Com 36.2: Affordable Prices (blue) by Income Level versus Home Ownership (left) & Rental (right) Costs, 2019 dollars (Statistics Canada, VIREB, CMHC)



Similarly, we can calculate which specific economic family types can or cannot afford certain types of accommodation based on the same approach as used above. Using the before-tax median incomes provided earlier in this report, adjusting them to 2019 dollars, calculating affordable monthly payments and purchase values, and comparing these to market rental and ownership prices, we obtain the result of **Table Com 36.3**.

Table Com 36.3: Economic Family Ownership & Rental Cost Gaps, 2019 dollars

Economic Families	Median Income	Affordable (30%)		Rent Gap				Sale Price Gap			
		Monthly Payment	Dwelling Value	Bachelor	1-Bedroom	2-Bedroom	3+ Bedroom	Single Family	Condo Apt.	Patio Home	Town House
Non-econ. family	\$37,058	\$926	\$217,077	\$326	-\$14	-\$224	-\$274	-\$342,923	-\$180,423	-\$226,923	-\$142,923
Lone parent	\$55,211	\$1,380	\$323,407	\$780	\$440	\$230	\$180	-\$236,593	-\$74,093	-\$120,593	-\$36,593
Couple w/ child	\$113,039	\$2,826	\$662,149	\$2,226	\$1,886	\$1,676	\$1,626	\$102,149	\$264,649	\$218,149	\$302,149
Couple w/o child	\$80,012	\$2,000	\$468,684	\$1,400	\$1,060	\$850	\$800	-\$91,316	\$71,184	\$24,684	\$108,684
Median Income	\$70,352	\$1,759	\$412,100	\$1,159	\$819	\$609	\$559	-\$147,900	\$14,600	-\$31,900	\$52,100

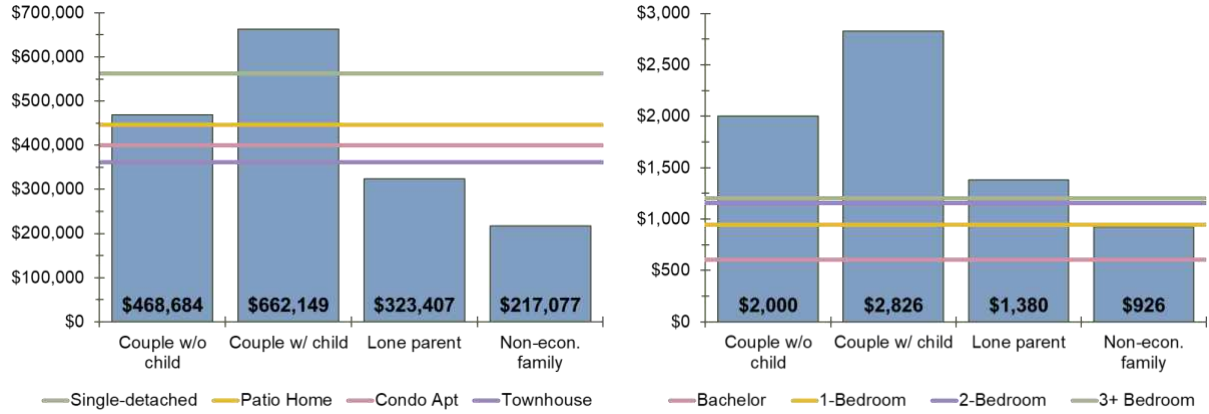
At least 50 percent of non-economic families can only afford a bachelor unit within the overall market; however, they are relatively close to affording the median rent of a 1-bedroom apartment. About half of lone parents can afford all rental units, but cannot reasonably afford any of the defined dwellings within the ownership market. Couples with children can generally afford any unit, while those without children have difficulty paying for single-family homes.

Figure Com 36.3 graphically represents the result of **Table Com 36.3**. For instance, the left graphic for ownership shows that half of lone parent households (because median defines the midpoint) cannot afford any unit type since the associated affordable purchase price does not surpass any of the horizontal lines demarcating a dwelling type. Conversely, the right shows that at least half of lone parents can afford all rental types.

Once again, please note that this discussion considers “reasonable affordability” as not paying more than 30 percent of before-tax household income. It is still possible for the defined categories

or families to rent or purchase a unit; however, the greater the discrepancy between the affordable budget and said prices, the greater the financial impact on that household.

Figure Com 36.3: Affordable Prices (blue) by Economic Family Type versus Home Ownership (left) & Rental (right) Costs, 2019 dollars (Statistics Canada, VIREB, CMHC)



ATTACHMENT 8
2012 SOLID WASTE MANAGEMENT PLAN

Comox Strathcona Waste Management

2012 Solid Waste Management Plan

Prepared by:

AECOM

3292 Production Way, Floor 4 604 444 6400 tel
Burnaby, BC, Canada V5A 4R4 604 294 8597 fax

www.aecom.com

In association with:

Maura Walker and Associates Environmental Consultants
Jan Enns Communications

Project Number:

60141938-10

Date:

December 2012

Executive Summary

The 2012 Solid Waste Management Plan is a long term vision for solid waste management in the Comox Strathcona waste management (CSWM) area and is an update to the previous plan that was approved in 2003. This plan presents the programs, services, infrastructure and policies that will serve to guide the design and implementation of solid waste services and disposal over the next several years.

The contents of this Plan reflect the input received from the Solid Waste Management Plan Advisory Committee over the past two years, and input received during the community consultation phase held in March and April of 2012. A special thanks to the contribution of all of the members of the Advisory Committee is extended on behalf of CSWM services.

Implementation of the waste minimization components of the Plan presented herein is designed to achieve a waste diversion rate of over 70%, primarily through:

- Increasing access to recycling at multi-family buildings;
- Increasing the amount of waste recycled by industrial, commercial and institutional activities; and
- Increasing diversion of organic waste, with a focus on diversion of food waste.

The waste that cannot be reduced, reused or recycled is referred to as “residual waste”. The proposed long-range plan is to pursue energy recovery from the residual waste through Waste to Energy technologies. In addition, two regional engineered sanitary landfills are planned, one in each regional district, located adjacent to the existing regional landfill sites.

Closure of the current landfills is planned for all CSWM landfills. Updated operating and closure plans will be developed for the landfills in Tahsis, Zeballos and Gold River. The closure of the Tahsis and Zeballos landfills is planned to begin in 2017, to meet BC Ministry of Environment requirements. Upon full closure, each of these sites may be replaced with a transfer station. Closure of the current landfills at the Comox Valley waste management centre and Campbell River waste management centre is scheduled to commence in 2012 and 2013 respectively.

The actions in this plan will be implemented through Solid Waste Board authorization over the next ten years. During this period, the funding of CSWM operations and capital projects is anticipated to be done through revenues received from tipping fees and financial reserves (a portion of previously collected tipping fees).

As part of plan implementation, CSWM will be considering opportunities for integrated resource recovery. In particular, integrated resource recovery will be considered as part of assessing organics processing and waste-to-energy options.

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1. Background

In British Columbia, each regional district is mandated by the Provincial Environmental Management Act to develop a Solid Waste Management Plan that provides a long term vision for solid waste management, including waste diversion and disposal activities. Plans are updated on a regular basis to ensure that the plan reflects the current needs of the regional district, as well as current market conditions, technologies and regulations.

The Comox Valley Regional District (CVRD) is responsible for solid waste management planning in both the CVRD and the Strathcona Regional District (SRD) geographic areas. The service is governed by a board of directors that includes elected officials from member municipalities and electoral areas of both regional districts and is branded “Comox Strathcona waste management” (CSWM).

Over the past two years, CSWM has engaged in a process to update the 2003 Comox Strathcona Solid Waste Management Plan (SWMP or Plan) to reflect current and future waste management needs. The process to update the Plan was conducted in three stages. The first stage was a review of the current system and a report on the implementation status of the 2003 Plan. This stage created the baseline for development of the new Plan. The second stage was a review of options to address the region’s future solid waste management needs and the selection of preferred options through the Solid Waste Management Plan Advisory Committee. The third stage involved community consultation to obtain input on the selected options.

The planning process and the development of this Plan were completed in accordance with the BC Ministry of Environment (MoE) document entitled “Guide to the Preparation of Regional Solid Waste Management Plans for Regional Districts” (BC MoE, 1994).

1.1 Plan Objectives

There are three main objectives associated with this new solid waste management plan:

1. The initiatives outlined in the plan work towards a goal of zero waste¹ and aim to minimize the amount of waste buried in landfills.
2. Improvements to the solid waste management system will reduce greenhouse gases emissions from solid waste management activities.
3. All CSWM landfills will be designed and operated to minimize impact on the environment and the surrounding community and to satisfy the BC Landfill Criteria for Municipal Solid Waste.

1.2 Guiding Principles

The following guiding principles were developed by the Ministry of Environment and adopted by the Solid Waste Management Plan Advisory Committee and the CSWM board to help direct the selection of Plan options:

- Solid waste is a resource.

¹ As defined by the Recycling Council of BC, zero waste is a philosophy that views solid wastes as resources, and recognizes the importance of “closing the loop”(putting waste materials back into the production cycle). Zero waste requires that products and processes be designed so that their components can be dismantled, repaired and recycled. It means linking communities, businesses and industries so that one’s waste becomes another’s feedstock. It means preventing pollution at its source. It means new local jobs in communities throughout British Columbia.

- The consumption of material and energy resources should be set at a level which is ecologically sustainable.
- The regional solid waste stream should be reduced to the greatest extent possible, in accordance with the hierarchy of reduce, reuse, recycle, recovery and residual waste management and consistent with local resources and the nature of the regional solid waste stream.
- The goal of environmental policy is to strive towards zero pollution and the strategies for achieving that goal are in accordance with the precautionary principle.
- Individuals and firms will be enabled to make environmentally sound choices about consumption of resources and generation of waste through provision of appropriate information, including user-pay and market-based incentives wherever possible.
- Reduction policies and strategies will be developed through public consultation and are socially acceptable and cost-effective, based on full accounting of costs and benefits, both monetary and non-monetary.

1.3 Participants in the Planning Process

The planning process involved a number of stakeholders and the general public through a variety of different activities.

The CVRD hired AECOM as their technical consultant for the duration of the process to update the plan. AECOM, with the assistance of Maura Walker and Associates and Jan Enns Communications, guided the process, provided technical input on the options, prepared the planning documents, and assisted with the consultation process.

A Solid Waste Management Plan Advisory Committee was formed at the beginning of the planning process to provide community-based and technical input into the planning process and to provide recommendations to the solid waste Board. The advisory committee participants included representatives from member municipalities, the private waste management sector, an environmental group from a local high school, BC Ministry of Environment (MoE), CVRD staff, SRD staff, and the solid waste Board.

Stage three of the planning process involved an extensive community consultation process that included:

- Public open houses and presentations held in 14 locations throughout the CSWM area;
- Feedback forms completed at the open houses and on-line (700 received);
- A telephone survey of 600 randomly selected homes covering all communities in the CSWM area to achieve an accuracy of $\pm 4.0\%$, with a 95% confidence interval; and
- Presentations to all municipal councils and the electoral area sub-committees of the CVRD and SRD.

Additionally, a separate consultation process was undertaken with First Nation communities in the CSWM area. Details on the consultation processes are provided in two separate reports entitled "Public Consultation Report" (AECOM, 2012) and "First Nations Consultation Report" (TRI, 2012).

2. Plan Area

The CVRD and SRD were established on February 15, 2008, as part of the provincial restructuring of the Comox Strathcona Regional District. Regional solid waste services are provided to the combined CVRD and SRD geographic areas by the CVRD. The service is branded as Comox Strathcona waste management (CSWM).

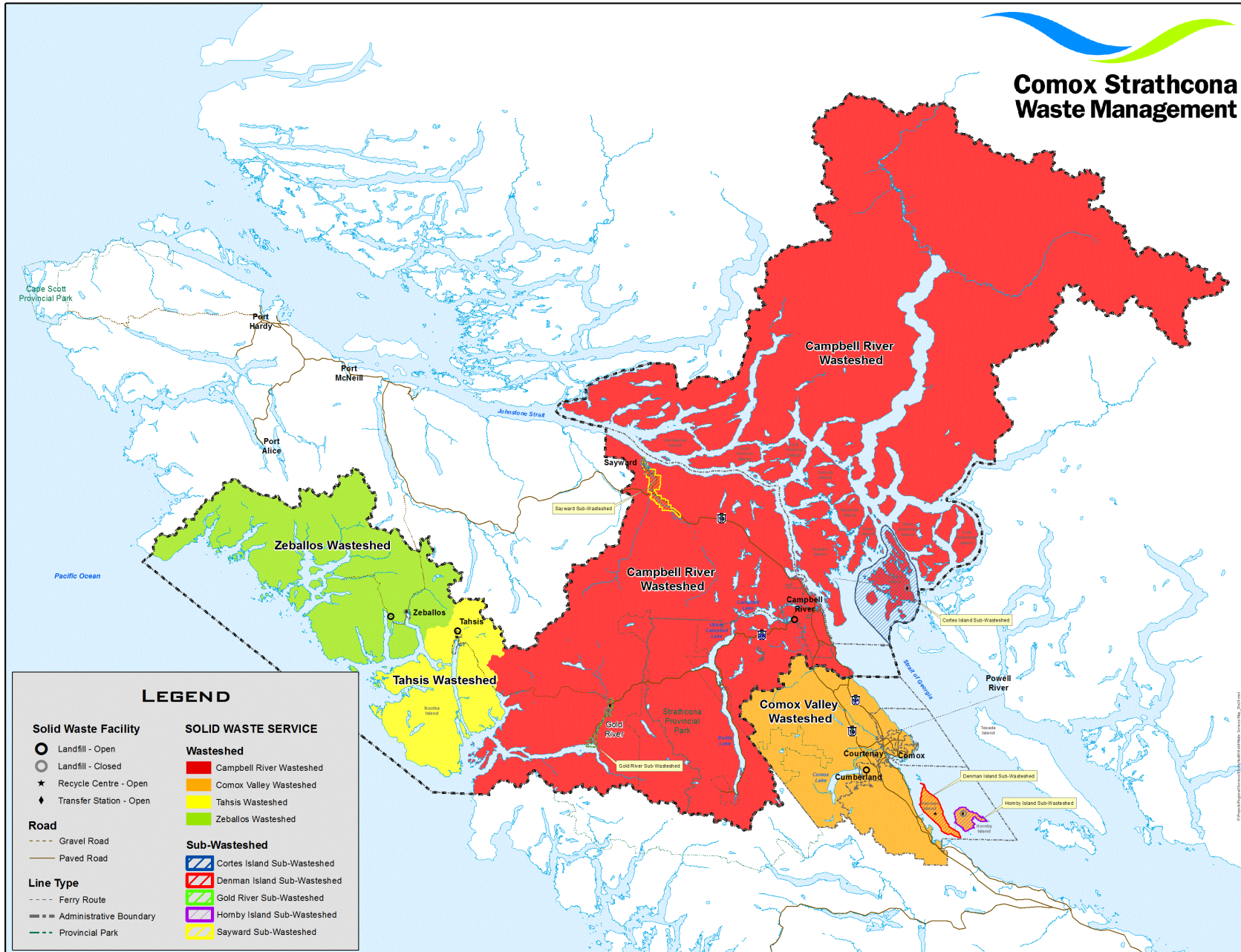
The CVRD covers approximately 1,725 km² and the electoral boundaries include the Town of Comox, the City of Courtenay, the Village of Cumberland, Electoral Area 'A' (Baynes Sound – Denman / Hornby Islands), Electoral Area 'B' (Lazo North), and Electoral Area 'C' (Puntledge-Black Creek). The SRD covers approximately 20,000 km². The electoral boundaries of the SRD include the City of Campbell River, the Village of Gold River, the Village of Sayward, the Village of Tahsis, the Village of Zeballos, Electoral Area 'A' (Sayward – Kyuquot / Nootka), Electoral Area 'B' (Cortes Island), Electoral Area 'C' (Discovery Islands – Mainland Inlets), and Electoral Area 'D' (Oyster Bay – Buttle Lake).

In addition, there are 14 First Nations with reserve lands located in the Plan area.²

Figure 2-1 shows the CSWM wastesheds. Each wasteshed indicates the geographical areas served by each of the CSWM Waste Management Centres.

² TRI. *First Nation Consultation Final Report. 2012*

Figure 2-1. CSWM Wasteshed Area



2.1 Physical Description and Constraints

The CSWM area extends from Kyuquot-Nootka on the west coast of Vancouver Island, across the islands of Cortes, Quadra, Denman, Hornby and the Discovery Islands, into a portion of the British Columbia mainland north of Powell River. The terrain includes rugged coastline, remote inlets, populous valleys, and the highest peaks of the Vancouver Island Ranges. Constraints that were considered during the planning process relate to the terrain and waterways, and their impact on access to communities and transportation distances.

The region's climate is one of the mildest in Canada due to moderation by the Pacific Ocean, which also contributes heavy precipitation to the western coast of Vancouver Island. The eastern portion of the CSWM area receives some relief due the rain shadow effect of the Vancouver Island Ranges. As the climates in the western and eastern coastal areas are unique, the precipitation patterns vary greatly across the planning region.

The planning region lies predominantly in the temperate rainforest biome, home to large conifers like the western hemlock, western red cedar, pacific silver fir, yellow cedar, Douglas fir, grand fir, Sitka spruce, and western white pine. The fauna is similar to that of the mainland coast, and the rivers, lakes and coastal regions are renowned for trout, salmon, and steelhead.

2.2 Population

The population in the CSWM in 2011 was 104,950, based on 2011 census data. Table 2-1 provides the permanent population in 2011 by area.

Table 2-1. CSWM Estimated 2011 Population

Southern Wasteshed Area	Population
Comox	13,627
Courtenay	24,099
Cumberland	3,398
Baynes Sound-Denman/Hornby Islands (Electoral Area A)	6,899
Lazo North (Electoral Area B)	6,939
Puntledge-Black Creek (Electoral Area C)	8,325
Sub Total	63,287
Northern Wasteshed Area	
Campbell River	31,186
Gold River	1,267
Sayward	317
Tahsis	316
Zeballos	125
Kyuquot/Nootka-Sayward (Electoral Area A)	807
Cortes (Electoral Area B)	1,007
Discovery Islands-Mainland Inlets (Electoral Area C)	2,601
Oyster Bay-Buttle Lake (Electoral Area D)	4,037
Sub Total	41,663
Total	104,950

Source: BC Stats

An estimated additional 5,200 people live on First Nation Reserves in the CSWM area.³

2.2.1 Population Projections

Table 2-2 provides the estimated population projections for the CSWM area as provided by BC Stats. The southern watershed population is expected to grow at an average rate of 1.5% per year and the northern watershed population is expected to grow at an average rate of 0.7% per year.

Table 2-2. CSWM Population Projections (2011-2031)

Year	Southern Watershed Area	Northern Watershed Area	Total CSWM Area
2011	63,287	41,663	104,950
2012	64,367	41,926	106,293
2013	65,395	42,192	107,587
2014	66,652	42,457	109,108
2015	67,737	42,729	110,466
2016	68,831	43,016	111,847
2017	69,887	43,344	113,232
2018	70,976	43,673	114,648
2019	72,079	44,003	116,082
2020	73,159	44,362	117,521
2021	74,245	44,726	118,970
2022	75,301	45,082	120,383
2023	76,366	45,428	121,794
2024	77,454	45,774	123,227
2025	78,441	46,109	124,549
2026	79,512	46,427	125,940
2027	80,582	46,733	127,316
2028	81,636	47,023	128,659
2029	82,686	47,302	129,988
2030	83,685	47,567	131,252
2031	84,711	47,828	132,539

Source: <http://www.bcstats.gov.bc.ca>

2.3 Economic Data

Based on 2011 census data, there were approximately 46,500 private dwellings in the CSWM area. Based on 2006 census data⁴, housing consists of roughly 69% single households, 1% multi-family dwellings and 30% non-family dwellings. This is consistent with figures for the entire province.

According to the 2006 census data, the main industries (by labour force) for the region are retail trade, health care and social assistance, agriculture / forestry / fishing / hunting, accommodation and food services, and construction.

³ TRI. First Nation Consultation Final Report. 2012

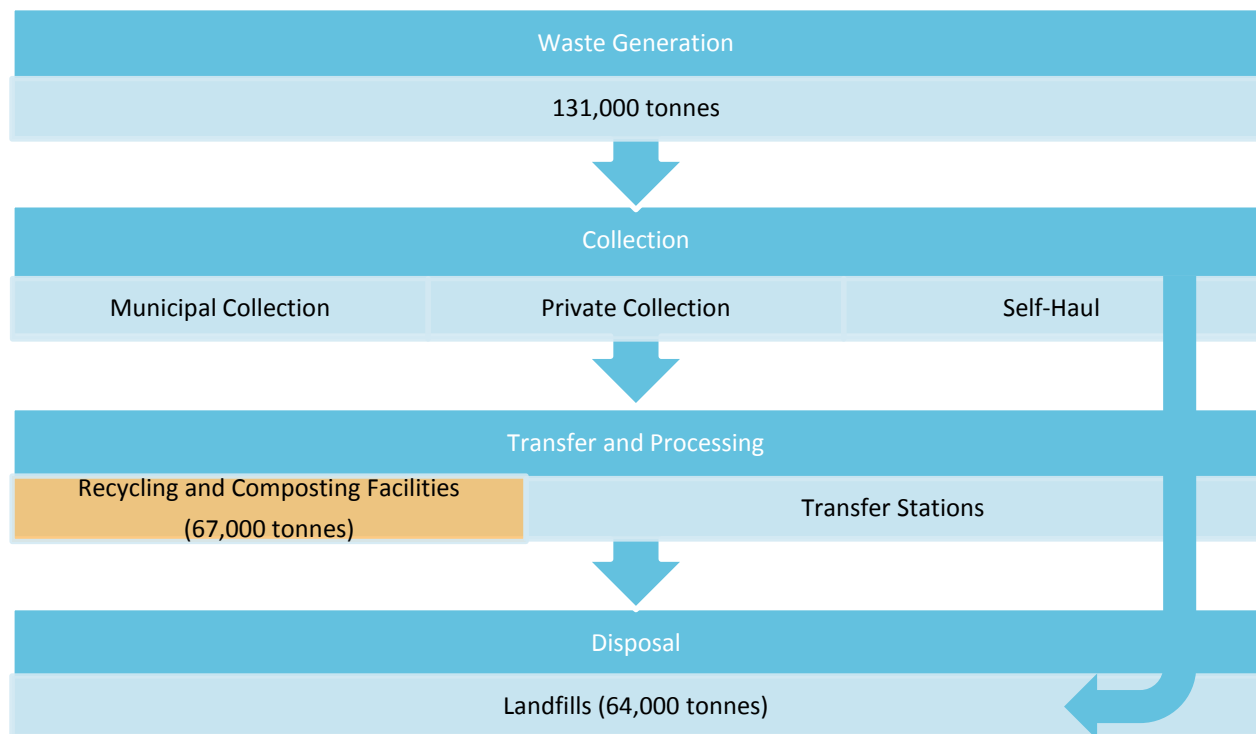
⁴ Detailed 2011 census data on housing and the economy was not available at the time of preparing this report.

The mean age of the population in the CVRD is increasing and associated with this is an increasing number of retired persons. In contrast, the mean age of the SRD is younger than that in the CVRD and there are a greater proportion of people working in the primary resources sector.

3. Existing Solid Waste Management System and Waste Characterization

Figure 3-1 shows the general flow of waste from the point of generation (at homes, work places or at construction / demolition sites), through to waste diversion facilities (recycling processors and composting facilities) or disposal facilities (landfills), based on the waste management system in 2011.

Figure 3-1. Waste Flows



There is a broad range of solid waste management programs and infrastructure in the CSWM area including:

Education and Promotion

- CSWM's "Power of R" program;
- CSWM's Compost Education Centres with contract educators;
- CSWM website with a directory of recycling, composting and local disposal options and additional information on recycling and waste management; and
- Most municipalities and not-for-profit waste management centres maintain web-based information to assist their communities and help visitors understand their options for recycling and reuse.

Reduction and Reuse Programs

- Backyard composting is encouraged through education and supply of low cost backyard composters;
- Municipalities and communities have garbage “can limits” for their curbside collection programs which limit the weekly allotment of waste to one or two containers per week;
- There are reuse centres / free stores at the waste management centres on Hornby, Denman and Cortes Islands, and in Gold River and Tahsis; and
- Several private and not-for-profit entities provide services for the salvage and reuse of goods.

Recycling

- Curbside collection of recyclables is provided to residents of Campbell River, Courtenay, Comox, Cumberland and Royston;
- There are recycling drop-off depots located throughout the CSWM service area;
- There are staffed recycling centres on Hornby, Denman and Cortes Islands, and in Gold River and Tahsis;
- Most CSWM waste management centres offer a broad range of recycling opportunities, including scrap metal, appliances and other items that are not collected at curbside or through drop-off depots;
- Curbside yard waste collection is provided to residents of Campbell River, Courtenay, Comox and Cumberland; and
- There are private collection companies that provide recycling collection services to businesses and multi-family residential buildings.

Composting

- The CVRD owns and operates a biosolids composting facility that uses clean, chipped wood waste in the composting process; and
- There are two private yard waste composting operations.

Garbage Collection

- Residential curbside garbage collection service is provided in the communities of Campbell River, Courtenay, Comox, Cortes Island, Cumberland, Denman Island, Gold River, Royston, Sayward, and Tahsis;
- The Village of Zeballos provides centralized, bear-proof bins located throughout the community for the collection of garbage;
- In Courtenay and Comox, multi-family and ICI buildings receive garbage collection service through the municipality. In all other areas, multi-family and ICI buildings must contract their own garbage collection service; and
- In the following areas, residents must hire their own garbage collection company or “self haul” their garbage to a local waste management centre:
 - Quadra Island
 - Union Bay
 - Merville
 - Black Creek
 - Oyster River
 - Oyster Bay
 - Dove Creek
 - Puntledge
 - Lazo North
 - Hornby Island
 - Fanny Bay
 - Stories Beach
 - Buckley Bay

Transfer Stations

- There are CSWM transfer stations located in Gold River, Campbell River, on Cortes Island and Hornby Island. Waste from these transfer stations is hauled to either the Comox Valley or Campbell River waste management centres.

Landfills and Other Disposal Facilities

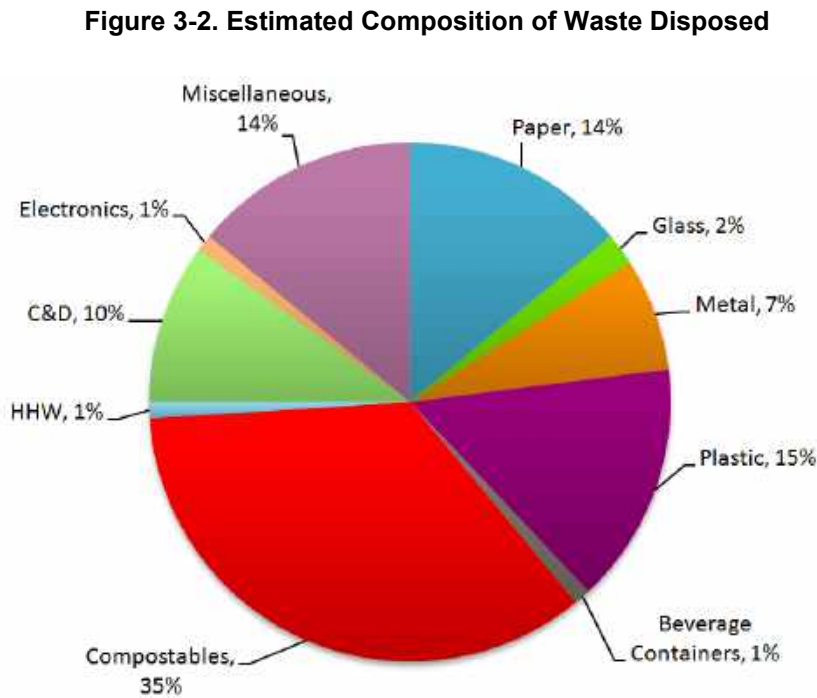
- There are five active CSWM landfills located at or near the following communities: Campbell River, Cumberland, Gold River, Tahsis and Zeballos;
- The Gold River Landfill, which is closed as a municipal solid waste landfill, continues to receive inert construction and demolition waste;
- There are closed landfills in Sayward and on Cortes, Hornby and Denman Islands; and
- There are four active private disposal facilities that receive wood waste and / or inert construction / demolition / land clearing waste.

Household Hazardous Waste

- Household hazardous waste (HHW) disposal is managed, to a large extent, through BC product stewardship programs which have set up collection programs for the majority of household hazardous waste products, such as paint, pesticides, solvents, and used motor oil. To supplement the stewardship activities, CSWM has held collection events.

3.1 Composition of CSWM Waste Disposed

Figure 3-2 shows the estimated weight-based composition of the CSWM waste currently going to disposal. Based on this estimate, roughly one-third of the disposed waste is recyclable (e.g., paper, metal, plastic, beverage containers) and one third is compostable.



3.2 Disposal, Diversion and Waste Generation

Table 3-1 provides a summary of the current estimated waste disposal, waste diversion quantities, the disposal facilities and methods of diversion. In 2011, CSWM disposed of 64,292 tonnes of waste (based on scale house records and an estimate of waste disposal for Tahsis and Zeballos) and diverted from landfilling an estimated 67,126 tonnes of material, resulting in a waste diversion rate of 51%.

Table 3-1. Current Disposal and Diversion Estimates

	Tonnes
Disposal	
Comox Valley Waste Management Centre Landfill	38,445
Campbell River Waste Management Centre Landfill	24,921
Tahsis Landfill (estimated)	300
Zeballos Landfill I (estimated)	300
Gold River Inert Waste Landfill	326
Total landfill disposal	64,292
Diversion	
Curbside blue box	3,205
Recycling depots	4,146
Recycling at landfills	11,975
Municipal yard waste programs	4,690
Private recycling activities (estimated)	38,650
Encorp Return-It (Beverage Containers)	2,253
Product Care(HHW)	70
Tire Stewardship BC	425
BC Used Oil Management	1,220
ESABC (Electronics)	492
Total diversion	67,126
Total waste generation (disposal + diversion)	131,418
Diversion rate (diversion / waste generation)	51%

The disposal rate for the CSWM in 2011 was 0.61 tonnes per person.

3.3 Sources of Waste Disposed

Table 3–2 shows the major sources of landfilled waste at the Comox Valley and Campbell River waste management centres (CVWMC and CRWMC) and the relative quantity from each source. This information is based on 2011 scale data for each site.

Municipal waste collection contributes 39% of the waste landfilled at the CVWMC by weight, 32% is from commercial collection and customers who self-haul, and 29% is from the construction industry. The CRWMC has a different

profile as it receives 18% from municipal collection, 15% from the construction industry and 67% from commercial collection, self-haul customers and waste transferred from the Gold River and Cortes Island transfer stations. Much of this difference can be attributed to the different municipal collection services in Courtenay, Comox and Campbell River. In Courtenay and Comox, the municipal service includes collection from businesses and institutions as well as residences. In Campbell River, the municipal service is provided only to residences.

Table 3-2. Sources of Landfilled Waste (2009)

Waste Source (Generator)	Comox Valley Waste Management Centre	Campbell River Waste Management Centre
Municipal collection	39%	18%
Commercial collection, transfer stations and self-haul customers	32%	67%
Construction	29%	15%
Total	100% of 38,455 tonnes	100% of 24,921 tonnes

4. Future Solid Waste Management System

The future solid waste system builds on the existing framework of services and programs while seeking to improve the delivery of those services to reduce the quantity of waste sent to disposal. The proposed programs, infrastructure and policies for the updated Solid Waste Management Plan are outlined in Section 5 through Section 18 and are presented in accordance with the waste management hierarchy shown in Figure 4-1.

Figure 4-1. Waste Management Hierarchy



The waste management hierarchy presents the various means of managing solid waste, from most desirable at the top, to least desirable at the bottom, as described below:

- “Reduce” is the most important part of waste minimization. Waste reduction avoids the unnecessary use of resources such as materials, energy and water and means there is less waste to manage.

- “Reuse” is the second level in the waste management hierarchy. Reuse is defined as the repeated use of a product in the same form, but not necessarily for the same purpose.
- “Recycle” involves some form of reprocessing of waste materials to produce the same or another product.
- “Recover” is defined as the reclamation of energy or recyclable materials from the remaining waste stream.
- “Residuals” management is the final treatment and / or disposal of a waste that cannot be used in any other way. For CSWM, residual management of solid waste is presently undertaken through landfilling.

The hierarchy has been applied to the development of the Solid Waste Management Plan options with the intent of minimizing the amount of residual waste that must be landfilled. In 2010, the Province developed a policy⁵ related to recovering energy from the waste stream: that regional districts must plan to achieve at least 70% waste diversion through the first 3Rs (reduce, reuse and recycle) prior to considering the 4th R of “recover” (e.g. waste-to-energy facilities). This policy has also been considered in the development of the Plan’s options and it is expected that the actions in this plan can achieve 70% diversion upon full implementation.

4.1 Diversion Estimates

For each plan component described in sections 5 through 16, an estimate of how much diversion can be attributed to the component is included. These estimates are based on the waste composition data (provided in Section 3-1), current waste disposal data, and expected diversion performance. The anticipated diversion associated with each plan component are intended to illustrate how 70% diversion can be achieved and to show the relative impact of each plan component on the overall diversion estimate provided in Section 17.

5. Reduction / Reuse Actions

Reduction and reuse policies, programs and facilities minimize the amount of waste that must be managed by the waste management system. There are several reuse facilities available, including thrift stores operated by the not-for-profit sector and “free stores” operated at some of the local waste management centres. To further encourage reduction and reuse, the following actions are planned:

- **On-Line Reuse:** Incorporate reuse services into the existing CSWM on-line recycling directory.
- **Reduction and Reuse Promotional Campaign:** Develop a campaign focused on “reduce” as a key part of a responsible lifestyle – linking solid waste reduction with other responsible behaviours such as energy use and water use.
- **Professional Salvage in Support of Community Organizations:** Salvageable materials are frequently observed in waste delivered to the landfill, particularly associated with source-separated construction wood waste and metal goods (appliances, bicycles, etc.). Although there are safety and liability concerns that prohibit salvage of materials by the general public at the waste management centres, professional salvage operators have the skills to recover materials for reuse and repair purposes. Recovery from source-separated waste piles at the CSWM-managed waste management

⁵ *Considerations for the Inclusion of Waste-to-Energy Facilities (WTE) in Solid Waste Management Plans, MoE, 2010*

facilities by pre-approved⁶ salvage operators that are associated with not-for-profit community organizations will be supported.

- **Reuse at Regional Waste Management Centres:** Assess the potential of reuse facilities at the regional waste management centres.

These actions have the potential to divert an estimated 300-500 tonnes per year from landfilling.

6. Residential Waste Management

6.1 Curbside Services

In each community that receives curbside garbage, recycling and yard waste collection through their local government, this service will be maintained through local bylaws. Policies that support participation in available recycling and composting programs should also be maintained, including:

- Limiting the amount of garbage set out for weekly collection (in some communities garbage volumes that are “over limit” must be accompanied by a pre-purchased tag).
- Restricting the inclusion of recyclables, yard waste and materials managed under product stewardship programs in garbage containers.

Curbside collection services are local services that are typically funded through local user fees. The cost per household of the curbside services varies slightly between the service areas depending on service variables and contract arrangements.

6.1.1 Food Waste Collection

The curbside collection of food waste (also referred to as kitchen scraps) can significantly reduce the amount of residential waste sent to disposal and is a critical element for achieving waste diversion goals.

Residential food waste collection has the potential in the CSWM to divert roughly 3,250 tonnes of organics from residual waste across the service area. For the purposes of estimating diversion, it is assumed that Courtenay, Campbell River, Comox and Cumberland would participate in a residential food waste collection program. Each jurisdiction should assess the suitability of adding food waste collection to its curbside service.

It is anticipated that food waste collection will result in no (or nominal) increases in curbside collection costs for residents. However, there will likely be a capital cost associated with the provision of food waste collection containers. Most island communities that provide curbside food waste collection have opted to provide each home with a kitchen container and a small green wheeled cart, as shown in Figure 6-1. The estimated cost of these two containers is approximately \$37. These costs are based on recent collection contracts awarded in the Regional District of Nanaimo.

The development of local food waste processing capacity is described in Section 9.

⁶ *Salvage operator has proven that they have the skills and insurance to safely salvage from specific waste streams.*

Figure 6-1. Food Waste Collection Containers

Example of a Countertop Bin to Collect Food Scraps



Example of a Curbside Green Cart for Food Waste Collection

6.2 Multi-Family Waste Management

Multi-family buildings may include apartment buildings, condominiums, townhouse complexes, mobile home parks or any other residential building considered “multi-family” under local bylaws. Due to the variation in building configurations from property to property, collection of waste and recyclables from multi-family buildings is generally not part of the residential curbside collection program but rather, it is undertaken by the same vehicles that service commercial buildings.

The following mandatory multi-family recycling criteria have been effective in other communities:

1. Mandate all multi-family buildings to implement a recycling collection service by a defined date. This approach has been used by the Cities of San Francisco and Portland and requires each building to set up a recycling service with a private contractor.
2. Provide recycling collection services to multi-family buildings as a municipal service. Participation in the program may be mandatory or voluntary for multi-family buildings. This approach has been used by the Cities of Vancouver, Richmond, Burnaby and, is currently being used by the Town of Comox.

With Board authorization, CSWM is planning to provide promotion and education assistance to support the multi-family collection programs to ensure consistency in communications related to multi-family recycling. Promotion and education assistance includes:

- Developing and distributing promotion / education materials targeted to multi-family building managers and residents;
- Providing assistance to building managers with tenant outreach; and
- Developing web-pages related to the program that can be linked to municipal web sites.

The provision of recycling services to multi-family buildings could divert an estimated 740 tonnes of waste per year based on the projected level of participation of the municipalities of Comox, Courtenay and Campbell River.

6.3 Recycling Depots

CSWM provides recycling depots, collection and transportation services for recyclables across the service area. Recycling depots are operated by local communities and through contracts managed by the CSWM. Recycling depots and services will continue to be provided where supported by the community and deemed necessary.

CSWM will monitor progress in the development of an industry-led stewardship plan for packaging and printed paper due to take effect in May 2014 and assess the potential impacts of this plan on the recycling depot service. (See Section 10 for more details on BC stewardship programs.)

6.4 Recycling at the Waste Management Centres

Each of the CSWM waste management centres is intended to provide a variety of waste management services to the local community. These services generally include two or more of the following:

- Garbage disposal;
- Appliance recycling;
- Recycling of household materials (paper, plastic containers, metal cans);
- Scrap metal recycling;
- Tire recycling;
- Reuse Centre / Free Store;
- Clean wood waste disposal;
- Yard waste drop off; and
- Household hazardous waste collection.

These facilities are located throughout the CSWM area to ensure reasonable access to recycling and disposal services. To ensure that the servicing levels at these centres meets the needs of the local community, service reviews are being conducted by CSWM on a regular basis.

7. Industrial, Commercial & Institutional (ICI) Waste Management

The following programs and services can increase recycling and diversion of ICI waste.

7.1 Variable Tipping Fees to Encourage Source Separation

To encourage recycling by the ICI sector, waste loads arriving at the Regional waste management centres that contain recyclable materials such as cardboard, paper and metal are currently charged a tipping fee that is significantly greater than the standard waste tipping fee. In contrast, recyclables are charged only 25% of the regular tipping fee and yard waste and scrap metal are received at no charge. This application of variable tipping fees provides a significant financial incentive to ICI waste generators to source-separate recyclable materials which increases diversion of waste to recycling and away from landfill disposal.

This incentive mechanism will continue to be used and the range of materials defined as “recyclable” may be expanded at CSWM facilities. As new recycling opportunities are established, adding those materials to the list of recyclable materials will be considered. The fee structure will be reviewed by CSWM staff and consultation with the construction industry will occur before changes in tipping fees authorization is sought. Before implementation of tipping fee increases, industry outreach will occur.

7.2 Mandatory Recycling by the ICI Sector

As a means to reinforce disposal policies and further encourage recycling at ICI locations, local government policies to encourage businesses to recycle may be considered by communities where ICI recycling services are readily available. Similar to encouraging recycling in multi-family buildings, it is anticipated that each local government will determine the most effective approach to recycling by the ICI sector based upon the local government’s current waste management protocols. The following are examples of the approaches that are being effectively employed by other local governments:

- Require all ICI buildings to implement a recycling collection service by a defined date. Under this approach, each ICI building would contract recycling services to meet their specific needs.
- Provide recycling collection services to ICI buildings as a local government service.
- A combination of the above two approaches:
 - Small ICI buildings that can be serviced by the same collection vehicle that collect recyclables from the residential sector are included in the curbside program.
 - Larger ICI buildings that cannot be serviced by the curbside program may be required to contract recycling services directly with a collection company.

This service, if operated as a local government service, would be funded through local user fees, as is the case with existing municipal curbside collection services.

CSWM will provide promotion and education assistance to support the ICI collection programs to ensure consistency in communications related to ICI recycling.

Through application of the above methods in the communities of Campbell River, Comox, Courtenay and Cumberland, it is estimated that by improving local ICI recycling, up to 5,000 additional tonnes of waste can be diverted from landfilling per year.

7.3 ICI Technical Assistance Program

To support waste reduction and source-separation of recyclables by the ICI sector, a CSWM technical assistance program specific to the ICI sector may be required. This program may include:

- Web-based recycling directory;
- Zero Waste audit and certification program;
- Zero Waste Awards program (this could be undertaken in association with local Chambers of Commerce);
- Awareness campaigns targeting specific commercial generators (e.g., retailers, restaurants, garages);
- Working with local business associations to provide education and outreach in the commercial sector;
- Working with tourism / hotel associations to develop a program and support materials for tourists; and
- Developing tools and information specific to different types of businesses (office, retail, restaurant, etc.).

7.4 Local Government Leadership

As role models for the ICI sector, CSWM will strive to implement “zero waste” initiatives within their own buildings and operations, and aim to build “zero waste” requirements into all Regional District contracts.

7.5 Mandatory Space Allocation in New Buildings

A common barrier to setting up recycling in multi-family and ICI buildings is the lack of space allocated to store collection containers. It is recommended that in communities where recycling services exist, that all new construction of multi-family and ICI buildings should be designed and constructed to accommodate collection of three material streams including:

- Residual waste or garbage
- Recyclables
- Organics

Mandatory space allocation in multi-family and commercial buildings has been adopted by several municipalities including the City of Surrey (for multi-family buildings) and the City of Seattle (for multi-family and ICI buildings). This space requirement has been applied as part of the process to obtain a development and / or construction permit. CSWM will consult with industry and local governments to develop a model bylaw for space allocation for consideration across the service area.

8. Recyclable Material Processing

Across the CSWM service area, much of the processing of recyclable materials is currently undertaken by the private sector as a part of a managed contract. When possible, a contract requirement for regular reporting by recycling contractors about the market destination of materials collected through local government collection services should be considered for all local government recycling processing contracts.

9. Organics Management

The organic fraction of landfilled waste is estimated to be 35% by weight. Much of this fraction can be diverted to organic waste processing facilities that use composting or anaerobic digestion technologies. By redirecting the recoverable organic fraction from residual landfilling, the following can be achieved:

- Reduction of the amount of waste landfilled (increasing landfill life);
- Reduction of the production of leachate;
- Reduction of the production of landfill gas and reducing greenhouse gas emissions; and
- Development of useful end-products such as compost, gas or energy.

9.1 Develop Organics Processing Capacity

To achieve organics diversion, organic processing capacity that can manage food waste and yard waste will need to be developed. This may be achieved by contracting with the private sector, the public sector building and operating its own facility, or through a public-private partnership. Operating models and technologies will need to be assessed before authorization. Based on a pre-feasibility study for a composting facility prepared for the CVRD in 2010/2011, the following steps are planned by CSWM in support of developing composting capacity:

1. Establish a pilot project to divert organics from the landfill.
2. Assess a location, including Campbell River, for a regional organics processing facility.
3. Assess organics management technologies, including composting and anaerobic digestion.
4. Partner with other local governments where possible.

9.1.1 Feedstock Supply

To be successful, the organics processing facility will need a guaranteed supply of feedstock (food waste and / or yard waste). CSWM and any participating municipalities will need to consider the following steps to support an organics diversion program:

1. Add food waste to the curbside collection program and possibly ban food waste from the curbside garbage collection service.
2. Have commercial generators of food waste participate at an acceptable level.
3. Include food waste in the list of “recyclable materials” that triggers higher tipping fees at the regional disposal sites.
4. Consider providing municipal food waste collection for small and medium size ICI food waste generators that can be serviced through the curbside food waste collection programs.
5. Consider food waste collection from rural areas.

It is estimated that roughly 7,000⁷ tonnes per year of organic waste can be diverted from landfilling in the CSWM through the development of organics processing capacity. At this time, it is estimated that organics diversion costs for the CSWM may be in the range of \$60 to \$90 per tonne, depending on the operation model and technology employed.

⁷ The estimate of 7,000 tonnes is based on 3,250 tonnes of residential food waste and 3,675 tonnes of ICI organic waste, as shown in Table 17-1.

9.2 Yard Waste Collection

Many areas within the CSWM service area have curbside yard waste collection or yard waste drop off depots. Through increased promotion of yard waste collection programs CSWM would be able to:

- Reduce the cost of composting organics by increasing needed bulk material;
- Reduce the quantity of yard waste in the landfill;
- Reduce the amount of “illegal dumping” of yard waste through the availability of alternatives; and
- Improve air quality by reducing the residential burning of yard waste.

CSWM plans to promote the availability of municipal and regional district yard waste collection services to the public.

9.3 Backyard Composting

Backyard composting is one of the most effective methods of reducing the amount of waste that enters the solid waste management system. A recent study by the North Shore Recycling Program indicated that an average home diverts 361 kg of organic waste per year when they use a backyard composter and that the diversion increases to an average of 452 kg when provided with educational support.⁸ Consequently, encouraging backyard composting through our education programs is one of the most cost-effective means of achieving waste diversion. CSWM plans to maintain the existing backyard composting program to encourage residents to compost at home. The program currently includes:

- Backyard composter sales;
- Promotion of backyard composting, grass cycling and xeriscaping through the website, advertising, community events and print materials; and
- Two staffed compost education gardens.

CSWM will seek solid waste Board authorization to enhance the backyard composting program by:

- Linking backyard composting education with Bear Aware information;
- Increasing the time that compost educators are available to the public during the year; and
- Improve composting education sites as necessary.

For estimating costs and diversion potential, it was assumed that CSWM will distribute 300 backyard composters annually for 5 years and that each composter could divert 400 kg per year. This would divert roughly 1,800 tonnes of organic waste over that time and another 1,800 tonnes would continue to be diverted annually for as long as those composters are in use.

⁸ *Backyard Composting Undervalued. North Shore Recycling Program, May 2011.*

10. Product Stewardship Programs

“In British Columbia, Industry-led Product Stewardship is a government strategy to place the responsibility for end of life product management on the producer and consumers of a product and not the general taxpayer or local government.” (MoE Product Stewardship website).

Product Stewardship programs play an integral and increasingly significant role in the management of municipal solid waste in BC. Most existing Product Stewardship programs, also referred to as Extended Producer Responsibility (EPR) programs, have been established by producers and brand owners of products in accordance with requirements set out in the BC Recycling Regulation. Other programs have been set up voluntarily by individual companies and industries (e.g. for milk containers).

In accordance with the BC Recycling Regulation, mandatory waste management programs have been established for the following categories of products (or will be as per regulatory phase-in schedules as indicated below):

1. Beverage Containers
 - All ready-to-drink beverages except milk and milk substitute products
2. Electronic and Electrical Equipment
 - Televisions, computers, monitors, printers and computer peripherals
 - Audio-visual and consumer equipment, thermostats, cell phones, residential fluorescent lamps, batteries used in these products
 - Small appliances, smoke detectors, and batteries used in these products
 - (July 2012) Large appliances, electrical and electronic tools, medical devices, automatic dispensers, lighting equipment, toys, leisure and sports equipment, monitoring and control instruments, information technology (IT) and telecommunications equipment, and batteries used in these products
3. Lead Acid Batteries
4. Used Lubricating Oil, Filters and Containers
 - Oil - any petroleum or synthetic crankcase oil, engine oil, hydraulic fluid, transmission fluid, gear oil, heat transfer fluid or other fluid used for lubricating purposes in machinery or equipment
 - Oil filters, any spin-on or element oil filter used in hydraulic, transmission or internal combustion engine applications - includes diesel fuel filters but does not include gasoline fuel filters
 - Oil containers - any plastic container with a capacity of less than 30 litres that is manufactured to hold oil
5. Pharmaceuticals
 - All unused or expired consumer medications, as defined in the Food and Drugs Act (Canada) except for veterinary drugs and drugs from hospitals, health clinics or doctor's offices. Medications are prescription drugs, non-prescription drugs and natural health products that treat, prevent or alleviate symptoms of illness or disease
6. Paints
 - Household paint such as latex and alkyd paint, coatings, sealers, glazes, primers, shellacs, undercoats, varnishes, paint aerosols and many other paint products

7. Solvents, Flammable Liquids, Gasoline and Pesticides

- Flammable liquids include acetone, BBQ lighter fluid, kerosene, paint thinner and flammable aerosols
- Pesticides are accepted in liquid, solid or aerosol form, however, only domestic pesticides labeled with a poison symbol and a "Pest Control Product" registration number (e.g., PCP Reg. #2464) are accepted. Gasoline products include gasoline leftovers, spoiled gasoline, or old gas contaminated with oil or water

8. Tires

- Pneumatic or solid tires designed for use on a motor vehicle, farm tractor, trailer, or other equipment or machinery

9. Packaging and Printed Paper

- In May 2011, the MoE added packaging and printed paper to the Recycling Regulation. The implementation of this program is expected to take place at the end of 2014; during the term of this Plan. The definitions used regarding packaging and printed paper appear to cover all materials currently collected by municipal and CSWM residential recycling programs.

The collection infrastructure for existing product stewardship programs consists of return-to-retail and / or stand-alone depot systems. Stewardship agencies which have been set up by industry to manage the collection system may directly operate their collection and systems themselves or under contract to service providers including local government. In accordance with the BC Recycling Regulation, the costs of collection and management of Product Stewardship programs are to be borne by producers and consumer, not by local governments or tax payers.

Most stewardship programs charge separate fees at the point of purchase to cover the costs of managing the discarded product, and the fee is shown on the sales receipt as an "eco-fee". These fees are applied by producers / brand owners as part of the price of the product; they are not government-applied taxes. The stewardship agencies are responsible for educating consumers regarding their programs and for providing information about collection options, fees, and handling practices.

In support of current Product Stewardship programs and to encourage the expansion of product stewardship to other waste products and materials the CSWM plans to:

- Incorporate product stewardship education into its "Power of R" education program and promote local options for disposing of products covered by Product Stewardship programs;
- Work directly with stewards and BC MoE to ensure that stewardship agency collection depots meet the needs of the communities;
- Participate on the BC Product Stewardship Council (BCPSC, an association whose membership includes province-wide representation of all Regional Districts) to lobby stewardship agencies to improve services, and for a broader deposit-based return system (deposits provide a financial incentive for generators to return HHW materials to the proper location); and
- Expand solid waste website links to include local locations and information for disposal of stewardship products.

It is estimated that 850 tonnes of additional waste can be diverted away from landfills annually through improved promotion and increased availability of information for product stewardship programs.

11. Household Hazardous Waste (HHW) Management

Household Hazardous Waste (HHW) is not a large volume of the total waste disposed, but because of its toxic nature, its environmental impact can be significant if disposal is not properly managed. To help ensure that these materials are managed appropriately, a combination of promotion and information on product stewardship programs (discussed above), consumer education and HHW collection services will be employed to better collect and manage HHW. Through Board authorization, CSWM plans to:

- Continue to incorporate proper HHW education into its “Power of R” education program and promote local options for properly disposing of HHW;
- Establish and maintain permanent HHW depots at CSWM waste management centers and transfer stations located in Campbell River, Comox Valley, Gold River, Cortes Island and Hornby Island; and
- Hold HHW round up events for smaller communities.

Funding for these services will be shared with the stewardship agencies since they are tasked with funding the collection and disposal of most HHW generated in BC.

12. Construction and Demolition (CD) Waste Management

Waste generated by construction, demolition and renovation projects includes a wide variety of waste materials, including cardboard, plastic, metal and wood. A large portion of the waste is typically reusable, recyclable or can be used for energy recovery in accordance with MoE legislation, regulations and requirements⁹, and therefore this waste stream represents a significant opportunity for waste diversion.

12.1 Construction and Demolition Project Permitting

CSWM will consult with construction / demolition industry and local government Building Permit and Planning Departments to assess the feasibility of policy tools to encourage diversion of CD waste including:

- Variable permit costs (deconstruction vs. demolition) to encourage source-separation, reuse and recycling of CD Waste.
- Solid waste management plans for large construction projects to ensure that waste diversion is considered in the planning for and during construction.
- Recycling of specific waste materials generated during construction and demolition projects.
- Adopting green building standards (e.g., LEED) that will reduce the amount of waste generated during construction and through the life of the building.

It is estimated that diversion of 11,500 tonnes of construction and demolition waste from CSWM landfills can be achieved with increased CD waste separation and diversion.

⁹ E.g. Clean wood waste may be chipped and used as hog fuel at some Vancouver Island pulp mills in accordance with MoE authorizations

12.2 Variable Tipping Fees

CD waste diversion will continue to be supported by variable tipping fees (lower tipping fees on source-separated recyclable / divertable materials). In particular, variable tipping fees may be used as an incentive for generators to source-separate clean and dirty wood waste as well as gypsum drywall.

12.3 CD Waste Recycling Services

At present, the only CD waste recycling opportunities are at publicly-funded landfills and transfer stations. CSWM will encourage the private and non-profit sectors to develop CD waste recycling services locally through:

- Setting appropriate pricing on loads of CD waste that does not compete with private and non-profit recycling efforts; and
- Any project permitting guidelines established through the activities discussed above in Section 12.1.

It is the intention of CSWM to continue to provide areas at the regional waste management centres for source-separated CD waste materials to ensure that there is on-going customer access to CD waste recycling options.

12.4 CD Diversion Promotion and Education

The CD waste diversion initiatives listed above are effective when supported by promotion and education targeting the construction industry. CSWM will work in concert with local industry associations to determine the industry's information needs to provide better promotion and education opportunities. CSWM promotion and education activities may include proven methodologies of increasing diversion such as:

- Developing a construction / demolition industry waste minimization tool kit. Similar tool kits have been developed by Metro Vancouver and Portland, Oregon;
- Providing a CD waste recycling directory on the website and in hard copy; and
- Organizing and holding CD recycling seminars in conjunction with local construction associations.

13. Promotion / Education of Solid Waste Management Programs

The success of waste management programs and policies requires that people know and understand why and how to effectively participate. Promotion and education, therefore, are critical to all components of the solid waste management system.

Promotion and education efforts directly related to municipal waste management services such as garbage collection will continue to be undertaken by the jurisdiction providing the service. However, to reduce costs and to create consistent messaging and branding throughout the service areas, CSWM plans to work with municipalities to develop standardized communications related to new programs such as food waste collection, multi-family recycling, and ICI recycling.

CSWM will continue to provide promotion and education related to their services such as recycling drop-off depots, transfer stations, landfills, as well as promotion and education on waste reduction and reuse, composting, household hazardous waste and product stewardship programs. Promotion and education activities will include the "Power of

R” program and will continue to use a range of promotion and education activities and tools for solid waste management and zero waste education.

14. Wildlife Conflict Management

The CSWM area is home to a large population of bears that are integral to the local ecosystem. Developing and maintaining a solid waste management system that minimizes the potential for human-bear conflict and minimizes bird populations at the waste management centres will continue and be improved.

To minimize potential wildlife conflict, CSWM plans to:

- Work with local Bear Aware groups and the Province to establish an on-going awareness and education opportunities for waste generators that addresses “bear awareness”;
- Encourage local governments to review their waste collection bylaws to consider containerization requirements for garbage and enforced set-out times for curbside collection to minimize wildlife access opportunities;
- Backyard composting education on composting methods that reduce the attraction of wildlife to residential areas and limit access to organic waste near homes;
- Maintain bear fencing around active landfills and all other waste management facilities, including transfer stations and composting facilities, that accept putrescible waste; and
- Maintain and improve bird management at the landfill working face at the Comox Valley waste management center and the Campbell River waste management center. As part of this initiative, a study will be undertaken in 2012 that includes quantitative and qualitative analysis of birds that contact waste at the landfill on adjacent lands and waters.

15. Land Clearing Waste Management

Land clearing waste is the vegetative debris created by the clearing of land, generally undertaken as the first step in a greenfield construction project. Land clearing waste is typically made up of tree stumps, trunks, branches and associated brush. Land clearing waste may be used as firewood, ground / chipped and land applied, composted, or used for energy recovery in accordance with MoE legislation, regulations and requirements. Land clearing waste can also be disposed at a MoE authorized disposal facility or burned on-site in accordance with the *BC Open Burning Smoke Control Regulation* and local requirements.

16. Illegal Dumping Prevention

Illegal dumping of waste is common in the CSWM area and throughout British Columbia. Per board authorization, specific initiatives are proposed to reduce illegal dumping and roadside litter include:

- Continue to waive tipping fees as per the current CSWM solid waste policy for organizations cleaning up illegal dumping sites or conducting community clean-ups;
- Continue to encourage municipalities to provide yard waste collection programs through depots or curbside collection (yard waste is frequently disposed of on back roads and is believed to lead to the dumping of other waste materials over time);

- Provide education to the community on disposal options and to encourage reporting of illegal dumping activities; and
- Require users of the waste management centres to cover their loads in order to prevent litter along transportation routes.

16.1 Enforcement Bylaw

Section 25(3) of BC Environmental Management Act allows BC Regional Districts to make bylaws to regulate the management of municipal solid waste or recyclable material including, bylaws regulating, prohibiting or respecting:

- The discarding or abandonment of municipal solid waste or recyclable material;
- The delivery, deposit, storage or abandonment of municipal solid waste or recyclable material at authorized or unauthorized sites; and
- The requirement of an owner of municipal solid waste or recyclable material, the deposit of which has been prohibited by bylaw, to pay the cost of its disposal in a manner specified in the bylaw.

The Act requires that a regional district indicate in its Plan its intention to undertake consultations with affected stakeholders and to subsequently undertake consultations as outlined in the Act prior to approving a bylaw affording the regional district the powers listed above. Accordingly, the CSWM intends to draft a bylaw to allow for enforcement actions against illegal dumping activities in the CSWM area. In all cases, CSWM will conduct required consultations to satisfy the requirements of the Act.

17. Diversion Estimate

Table 17-1 presents the estimated new waste diversion that can be achieved from each of the proposed plan components upon full implementation.

Table 17-1. Summation of Estimated Increase in Diversion from Plan Components

Plan Component	Estimated Increase in Diversion (tonnes per year)
Reduction and Reuse Actions	400
Residential Recycling	
Food waste collection	3,250
Improved Multi-Family Recycling	740
ICI Recycling	
Improved Recycling by the ICI Sector	5,000
Organic Waste Diversion	
Develop Organic Disposal Capacity	3,675
Backyard composting	1,800
Household Hazardous Waste and Extended Producer Responsibility Programs	2,600
Construction and Demo Waste Management	
CD Project Permitting	11,500
Total Estimated New Plan Diversion	28,615

The current waste diversion rate is estimated to be 51%; with the implementation of the reduction, reuse and recycling components of this plan, the diversion from landfill is expected to reach 70%, as illustrated in Figure 17-1. This would reduce the per capita disposal rate from 0.61 tonnes per year to 0.38 tonnes per year.

Figure 17-1 Current and Targeted Diversion



Table 17-2 shows the projected residual waste quantities for the next 30 years based on population projections and the diversion estimates provided in Table 17-1.

Table 17-2. Residual Waste Projections

Year #	Year	Projected Population	Waste Disposed (tonnes)
1	2011	104,950	62,970
2	2012	106,293	63,776
3	2013	107,587	64,552
4	2014	109,108	53,463
5	2015	110,466	54,128
6	2016	111,847	54,805
7	2017	113,232	37,366
8	2018	114,648	37,834
9	2019	116,082	38,307
10	2020	117,521	38,782
11	2021	118,970	39,260
12	2022	120,383	39,726
13	2023	121,794	40,192
14	2024	123,227	40,665
15	2025	124,549	41,101
16	2026	125,940	41,560
17	2027	127,316	42,014
18	2028	128,659	42,457
19	2029	129,988	42,896
20	2030	131,252	43,313
21	2031	132,539	43,738
22	2032	133,865	44,175
23	2033	135,204	44,617
24	2034	136,556	45,063
25	2035	137,921	45,514
26	2036	139,300	45,969
27	2037	140,693	46,429
28	2038	142,100	46,893
29	2039	143,521	47,362
30	2040	144,956	47,836

18. Residual Waste Management

The long-range vision for residual waste management for CSWM services includes two regional landfills and on-going consideration of waste-to-energy (WTE) technologies. Public consultation for this Plan showed strong regional public support (73% in favour) for expanding both the Campbell River and the Comox Valley regional landfills and strong public support (72% in favour) for continuing to assess the feasibility of WTE technologies as an alternative to landfilling residual wastes.

18.1 Two Engineered Regional Landfills

Expansion of the landfills at the Comox Valley waste management centre and the Campbell River waste management centre is planned to provide future disposal capacity. Expansion of the regional landfills will be in accordance with MoE environmental criteria for new landfills including bottom liners, leachate systems, landfill gas systems, groundwater control systems and groundwater monitoring.

The expansion at the Comox Valley waste management centre will be undertaken before expansion at the Campbell River waste management centre. The land adjacent to the existing Campbell River landfill site will undergo additional assessment of technical feasibility and a conceptual design for expansion will be prepared based on the outcome of the assessment work. The expansion at the Campbell River waste management centre will be undertaken in the future when capacity is required.

In consideration of the EBA May 2012 report to the Board that provided a triple bottom line business case for the long-term disposal options), CSWM plans to continue discussions with the Village of Cumberland and the City of Campbell River for mutually beneficial agreements between the host communities and the CVRD, the landfill owner.

18.2 Waste-to-Energy Technologies

Waste-to-energy (WTE), also defined as thermal processing or thermal treatment, involves the conversion of municipal solid waste into gaseous, liquid and solid products and a concurrent or subsequent release of heat energy. The heat energy is then used in many cases to generate electricity.

WTE best technologies and costs will continue to be explored as an alternative to landfilling residual waste and opportunities will continue to be assessed. CSWM services will communicate and cooperate with other regions in developing potential WTE capacity.

The MoE's policy is for local governments to have a minimum target of 70% reduction of waste before utilizing a WTE facility. The 70% target is calculated only from reduce, reuse, and recycle initiatives. The initiatives outlined in this Plan are projected to increase the diversion rate to over 70%, thereby making WTE a viable option for CSWM services.

Based on present conditions, it is anticipated that WTE may become part of the solid waste management system for CSWM in the future and that solid waste planning must consider WTE technologies and include such consideration in reporting to the Board for all related authorizations.

18.3 Proposed Waste-to-Energy Facility in Gold River

Covanta is a private sector waste management company that proposes to develop a WTE facility in the CSWM area through converting the former Gold River Pulp and Paper Mill into a thermal electric power plant that burns refuse

derived fuel (RDF). The RDF is described by Covanta as MSW that is processed to remove the majority of the recyclable components. The primary source of RDF would be Metro Vancouver waste; however the facility would also accept processed waste from other local governments.

The location of the site is the former Gold River Pulp and Paper Mill in Gold River. Covanta proposes to use many of the physical works and structures that were part of the mill. They plan to replace the existing power boiler with two new RDF boilers and associated pollution control equipment, and fuel and ash handling systems. RDF will be the primary fuel for both boilers. Steam produced in the boilers will drive a turbine -generator to produce 97 MW of electricity for delivery to the BC Hydro grid and in-plant use.

RDF storage and processing is expected to be 2,358 tonnes per day, with a total annual capacity of 750,000 tonnes. The WTE plant will employ the Martin system for refuse combustion and conversion of the RDF into energy. Emissions controls include combustion controls to maintain low levels of carbon monoxide and minimize products of incomplete combustion. Post combustion controls include a semi-dry scrubber followed by a baghouse.

As a by-product of the combustion process, the plant design is expected to produce up to 200,000 tonnes of ash annually from which ferrous and non-ferrous metal are to be recovered. Ash will be treated and landfilled in a private ash landfill located 12.8 km from the power plant.

This project has obtained environmental permits from the Province of BC. However, implementation of this project is subject to obtaining a contract with Metro Vancouver to manage its solid waste.

18.4 Existing CSWM System - Active Residual Waste Landfills

CSWM plans to close all five (Campbell River, Comox Valley, Gold River, Tahsis and Zeballos) of the existing MoE non-compliant landfills per the capital schedule (Table 23-1) of this report. The schedule for the closure of the Tahsis and Zeballos Landfills has been adjusted based upon direction from the MoE. For all landfills, planned environmental improvements include management of leachate, stormwater, improved landfill operations, and gas collection / management systems where required.

Closure plan updates and design of partial closure construction to address MoE compliance issues is scheduled for 2012 for landfills near Campbell River, Comox Valley, Gold River, Tahsis and Zeballos. For these facilities, all completed external slopes are planned to be capped, and improvements to stormwater and leachate management systems are to be completed from 2012 to 2015. Based on the improved facilities and operation plans, compliant with MoE permit criteria, new operations agreements will be required for some landfills. Subsequent and final landfill slope closure is planned as permitted capacity is reached.

18.4.1 Comox Valley Waste Management Centre Landfill

Upon authorization by the CSWM board, design will be undertaken to close and cap the existing landfill cell using engineered systems for managing leachate, landfill gas and stormwater. Partial closure construction for the existing landfill cell is planned to begin in 2013.

Between three to five years of permitted capacity remains at the existing landfill. The CVRD owns property available for expanding the CVWMC and a conceptual plan exists for the expansion to the north east and east of the existing landfill. Further investigative work is required to confirm the findings of the preliminary plan and to develop the details and phasing for landfill expansion. Starting in 2013, planning for a lateral expansion using MoE approved engineered landfill facility design and permitting is recommended at the CVWMC. The property authorized for

landfilling under the Operational Certificate is shown on Figure 18-1. Potential expansion properties to the north east and east are shown on Figure 18-2.

SUBDIVISION PLAN OF PARTS OF THE NW & NE 1/4 SECTION 27, PART OF PLAN 21 R/W,
 PART OF THE SE 1/4 SECTION 34 AND PART OF THE NW 1/4 SECTION 26,
 ALL OF TOWNSHIP 10, COMOX DISTRICT, PLAN 552 H.

B.C.G.S. 92F 065

PLAN VIP 55123

DEPOSITED IN THE LAND TITLE OFFICE
 AT VICTORIA, B.C.
 THIS 2nd Sept 1982

Deputy



06-XS-771+R's



S.E. 1/4

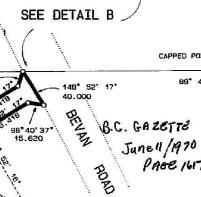
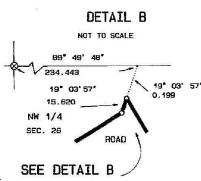
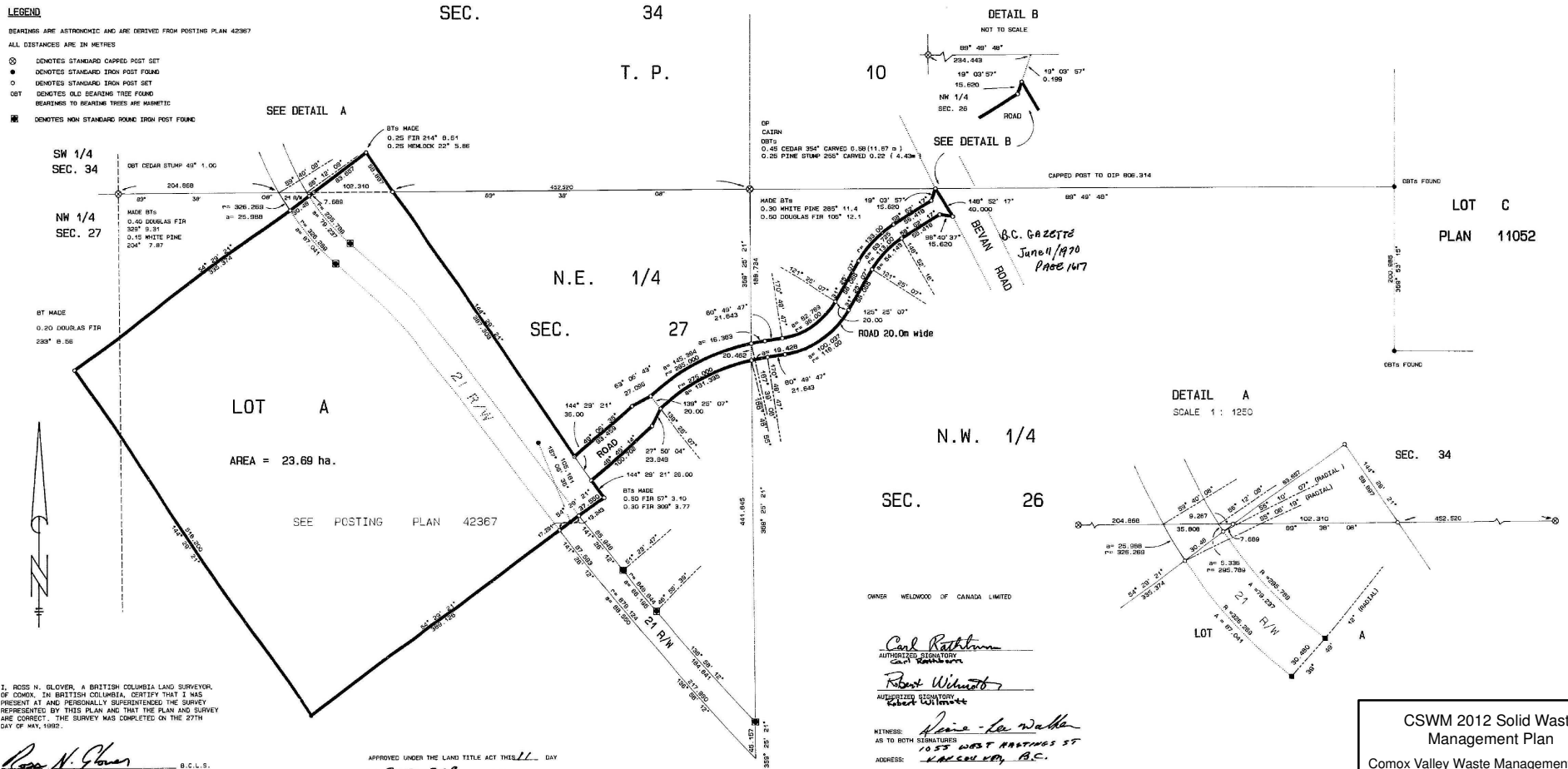
S.W. 1/4

SEC. 35

LEGEND

BEARINGS ARE ASTROMONIC AND ARE DERIVED FROM POSTING PLAN 42367
 ALL DISTANCES ARE IN METRES

- ⊙ DENOTES STANDARD CAPPED POST SET
- DENOTES STANDARD IRON POST FOUND
- DENOTES STANDARD IRON POST SET
- DENOTES OLD BEARING TREE FOUND
- BEARINGS TO BEARING TREES ARE MAGNETIC
- DENOTES NON STANDARD ROUND IRON POST FOUND

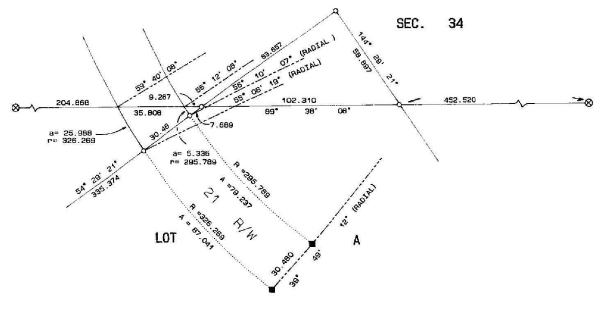


CAPPED POST TO DIP 806.314

LOT C
 PLAN 11052

B.C. GAZETTE
 June 11/1970
 PAGE 147

DETAIL A
 SCALE 1:1250



I, ROSS N. GLOVER, A BRITISH COLUMBIA LAND SURVEYOR,
 OF COMOX, IN BRITISH COLUMBIA, CERTIFY THAT I WAS
 PRESENT AT AND PERSONALLY SUPERINTENDED THE SURVEY
 REPRESENTED BY THIS PLAN AND THAT THE PLAN AND SURVEY
 ARE CORRECT. THE SURVEY WAS COMPLETED ON THE 27TH
 DAY OF MAY, 1982.

Ross N. Glover B.C.L.S.

APPROVED UNDER THE LAND TITLE ACT THIS 11 DAY
 OF SEPTEMBER 1982.

[Signature]
 APPROVING OFFICER
 MINISTRY OF TRANSPORTATION AND HIGHWAYS

THIS PLAN LIES WITHIN THE COMOX-STRAATHCONA REGIONAL DISTRICT

OWNER WELWOOD OF CANADA LIMITED

Carl Rothmann
 AUTHORIZED SIGNATORY
 Carl Rothmann

Robert Wilmut
 AUTHORIZED SIGNATORY
 Robert Wilmut

WITNESS: *Debra-Lee Walker*
 AS TO BOTH SIGNATURES
 1055 WEST HASTINGS ST
 VICTORIA, B.C.

OCCUPATION: SECRETARY

CSWM 2012 Solid Waste
 Management Plan
 Comox Valley Waste Management Centre

**Existing Landfill
 Property**

July 2012
 Project: 60141938

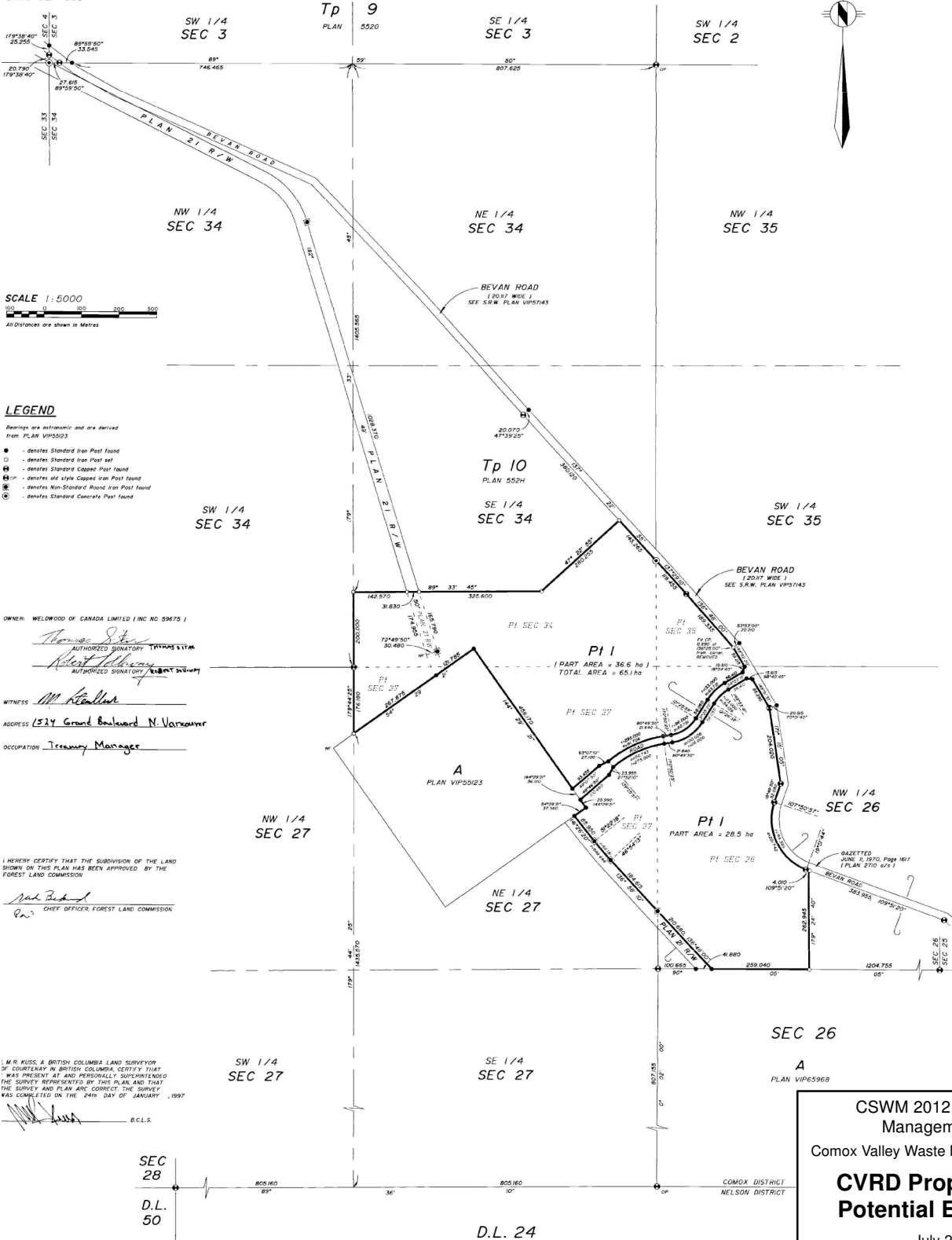
AECOM Figure 18-1

SUBDIVISION PLAN OF PART OF SECTION 26, EXCEPT THAT PART SHOWN OUTLINED IN RED ON PLAN 21RW, AND EXCEPT THOSE PARTS IN PLANS 4222, 11052, 26084, VIP55123 AND VIP65968, AND PART OF SECTION 27, EXCEPT THOSE PARTS SHOWN OUTLINED IN RED ON PLAN 21RW AND EXCEPT THAT PART IN PLAN VIP55123, AND PART OF THAT PART OF SECTION 27, CONTAINING 6.73 ACRES AS SHOWN ON PLAN 21RW EXCEPT THAT PART IN PLAN VIP55123, AND PART OF SECTION 34, EXCEPT THAT PART SHOWN OUTLINED IN RED ON PLAN 21RW AND THAT PART IN PLAN VIP55123, AND PART OF THAT PART OF SECTION 34, INCLUDED IN PLAN 21RW, AND PART OF SECTION 35, EXCEPT THAT PART IN PLAN 11052, ALL OF TOWNSHIP 10, COMOX DISTRICT, PLAN 552H

PLAN VIP 69987

DEPOSITED IN THE LAND TITLE OFFICE
AT VICTORIA, B.C.
THIS 30 DAY OF DEC 1999
McCracken REGISTRAR
EN118007

PURSUANT TO SECTION 99 (111h) L.T.A.
BCGS 92F 065



SCALE 1:5000
All Distances are shown in Metres

LEGEND
Bearings are astronomic and are derived from PLAN VIP5023
● denotes Standard Iron Post found
○ denotes Standard Iron Post set
□ denotes Standard Caped Post found
⊕ denotes old style Caped Iron Post found
⊙ denotes Non-Standard Round Iron Post found
⊗ denotes Standard Concrete Post found

OWNER: WELWOOD OF CANADA LIMITED (INC NO 59475)
Thomas St...
AUTHORIZED SURVEYOR THOMAS ST...
Robert...
AUTHORIZED SURVEYOR ROBERT ST...
WITNESS *M. Keenan*
ADDRESS 1514 Grand Boulevard N. Vancouver
OCCUPATION Inventory Manager

I HEREBY CERTIFY THAT THE SUBDIVISION OF THE LAND SHOWN ON THIS PLAN HAS BEEN APPROVED BY THE FOREST LAND COMMISSION
Paul...
CHIEF OFFICER, FOREST LAND COMMISSION

I, M. R. KISS, A BRITISH COLUMBIA LAND SURVEYOR OF COURTESY IN BRITISH COLUMBIA, CERTIFY THAT I WAS PRESENT AT AND PERSONALLY SUPERVISED THE SURVEY REPRESENTED BY THIS PLAN, AND THAT THE SURVEY AND PLAN ARE CORRECT. THE SURVEY WAS COMPLETED ON THE 24th DAY OF JANUARY, 1997
M. R. Kiss
B.C.L.S.

CSWM 2012 Solid Waste Management Plan
Comox Valley Waste Management Centre
CVRD Properties for Potential Expansion
July 2012
Project: 60141938

AECOM Figure 18-2

Path: P:\60141938\500-Progress Submittals & Deliverables\505-Misc Deliverables\Figure 18-2_CVRD_Properties_20120725.mxd Date Saved: 7/25/2012 12:08:20 PM User: MorellatCM

THIS PLAN LIES WITHIN THE REGIONAL DISTRICT OF COMOX STRATHCONA

18.4.2 Campbell River Waste Management Centre Landfill

Due to existing capacity of the landfill being reached in 2012, a transfer station was completed at the CRWMC in 2012. Residual waste will be transferred to the CVWMC until additional capacity in the present cell at the CRWMC becomes available¹⁰. Closure construction for the existing landfill is planned to begin in 2012 through 2013 and will include engineered leachate collection systems, landfill gas systems, and improvements to the stormwater control system as well as capping completed external slopes to address MoE landfill compliance issues.

In order to expand the landfill laterally, a feasibility assessment is required. The assessment will be used to assess the technical feasibility of landfill expansion and determine whether the additional property can be acquired. The assessment will include determination of the geotechnical and hydrogeological conditions beneath the proposed landfill footprint and development of a preliminary plan for expansion. After the assessment, a conceptual plan should be completed which can be followed by landfill expansion design, permitting and construction. The property authorized for the purpose of landfilling under the Operational Certificate is defined as Block C of District Lot 85, Sayward Land District. The property to the north that may be used in the future for landfill purposes is defined as Block J of District Lot 85, Sayward Land District. Both properties are under Crown Land Leases, with titles being transferred from the District of Campbell River to the CVRD.

18.4.3 Tahsis, Zeballos, Gold River and Sayward Residual Waste Management

Residual waste from the Villages of Tahsis and Zeballos is managed at two small landfills, referred to as the Tahsis and Zeballos Waste Management Centres. After closure of these facilities, residual waste will be delivered to a regional landfill site and the construction of transfer stations may be required. Residual waste from Gold River is currently transferred to the CRWMC and the plan is to continue this residual waste disposal practice. Sayward Valley and the Village of Sayward currently send residual waste to the CRWMC and the plan is to continue this residual waste disposal practice.

A detailed transfer station feasibility assessment report to the Board will be completed in 2012 for the Sayward disposal area and includes an assessment of waste haul options for Gold River and Tahsis. All transportation options will be in accordance with the current CSWM Unified Transportation Plan. Resulting CSWM facility disposal of residual waste for these communities is not anticipated to change as a result of this study.

18.4.3.1 Tahsis Waste Management Centre

To improve the operations of the Tahsis Waste Management Centre, CSWM will update the operations and closure plans in 2012. In the short term, facility and operational improvements will be undertaken, as per the updated plans. Closure and capping of completed areas will begin prior to 2017. The landfill will eventually be closed permanently and may be replaced with a transfer station, if required. Waste will be delivered to the nearest permitted CSWM disposal facility.

18.4.3.2 Zeballos Waste Management Centre

To improve the operations of the Zeballos Waste Management Centre, CSWM will update the operations and closure plans in 2012. In the short term, facility and operational improvements and upgrades will be undertaken, as per the updated plans. Closure and capping of completed areas will begin prior to 2017. The landfill will eventually be closed

¹⁰ It is anticipated that the closure design will create additional capacity on the existing footprint.

permanently and may be replaced with a transfer station, if required. Waste will then be delivered to the nearest permitted disposal facility.

18.4.3.3 *Gold River Landfill*

The Gold River Landfill no longer receives municipal solid waste for disposal. This landfill receives inert waste (e.g. concrete, asphalt shingles) and the plan is to continue this residual waste disposal practice. In 2012 an updated operations and closure plan will be developed. Improvements to the stormwater control system, the leachate control system and the closure and capping of slopes that have reached final grade are scheduled to begin in 2013.

18.5 Cortes Island Waste Management Centre

Residual waste generated on Cortes Island will continue to be delivered at the Cortes Island Waste Management Centre and subsequently transferred to a CSWM regional landfill.

18.6 Hornby Island Waste Management Centre

Residual waste generated on Hornby Island will continue to be delivered at the Hornby Island Waste Management Centre and subsequently transferred to a CSWM regional landfill.

18.7 Kyuquot Waste Management

The Village of Kyuquot is accessed by water and residual waste and recyclables are removed from the community by way of a private barge funded by CSWM. Waste is transferred from Gold River to a CSWM Waste Management Centre. This service will be reviewed by CSWM to ensure that the community's waste management needs can be adequately met into the future and to determine if the services could be expanded to the neighbouring First Nations communities.

18.8 Remote Homes and Businesses

Other than the services previously listed in this chapter, CSWM does not provide solid waste collection services to remote homes and businesses. It is expected that these properties will self-haul their waste to the nearest waste management centre for proper disposal.

18.9 Private Demolition, Land Clearing and Construction (DLC) Waste Disposal Facilities

There are four active private DLC waste disposal facilities authorized by the MoE in the CSWM area:

- Giese Holdings (MoE Authorization 9081), a DLC waste landfill and open burn site near Campbell River;
- Upland Excavating (MoE Authorization 10807), a DLC waste landfill and open burn site near Campbell River;
- Surgenor Landfill (MoE Authorization 8834), a DLC waste landfill near Courtenay; and
- West Shore Aggregates Ltd. (MoE Authorization Permit PR-07730), a DLC waste landfill located across the road from the entrance to the Campbell River Waste Management Centre.

The private DLC waste disposal facilities are expected to satisfy the same standards as publicly-owned facilities in the CSWM. It is expected that each private DLC waste disposal facility will prepare a proposed action plan and schedule to upgrade the facility to satisfy MoE standards or to phase-out and close the facility, and that the MoE will replace / update / amend the existing authorizations with updated permits or operational certificates that reflect the action plans and requirements.

The next Solid Waste Management Plan review / update is expected to include the private DLC waste disposal facilities action plans, schedules, updated authorizations and requirements.

18.10 Disaster Debris Management

A regional disaster debris response plan was prepared in 2010 and provides detailed direction on the removal, sorting, recycling and disposal of disaster debris, including the identification of temporary storage facilities.

In March 2010, the CVRD Board passed a recommendation that the regional disaster debris response plan be adopted as a schedule of the Comox Valley and Strathcona emergency plans.

18.11 Closed Landfills

CSWM is currently undergoing a process to identify and clarify the responsible public entity of former public landfills and dumps that were permitted by the Province (see section 18.12 for more information). Known closed public landfills are located in Sayward and on Cortes Island. In 2013, CSWM plans to prepare closure plans for the Cortes and Sayward landfills. CSWM anticipates that any appropriate closure and post-closure maintenance work will be defined by a qualified professional and that a request to abandon the permits will be sent to the MoE once closure is complete. The schedule for closure will be defined based on the information presented in the closure plan.

18.12 Ownership and Authorization of Public Landfill Sites

The publicly owned landfills discussed in this chapter, either currently operational or closed, are owned by the CVRD, however the legal entity on the landfill authorizing document (permit or operational certificate) and the land title/lease varies from site to site depending on the name of the public body that initiated the original authorizations and agreements. CSWM is currently undergoing the process of having the Comox Valley Regional District named on all public landfill related documentation to provide clarity and consistency regarding their responsibility for the landfills. Table 18-1 lists all of the public landfill sites and the documentation that is being reviewed and revised. All revisions are expected to be completed by the end of 2013.

Table 18-1. Administrative Revisions

Lease/Licence Assignments				
Facility	Crown Lands File Number	Tenure Document Number	Tenure Type	Current Lessee / Licensee
<i>Tahsis</i>	1401702	112889	Licence	Village of Tahsis
<i>Zeballos</i>	1403252	112829	Licence	Village of Zeballos
<i>Gold River</i>	1401708	104678	Lease	Village of Gold River
<i>Campbell River (Landfill)</i>	307020	101210	Lease	District of Campbell River
<i>Campbell River (Block 'J')</i>	1405218	103555	Lease	District of Campbell River
<i>Campbell River (GW Monitoring)</i>	1408983	114345	Licence	Strathcona Regional District
<i>Sayward</i>	1405725	112024	Licence	Strathcona Regional District
MoE Authorization Amendments				
Facility	MoE Authorization#	MoE Authorization Type		Current MoE Permittee
<i>Tahsis</i>	PR-04278	Permit		Village of Tahsis
<i>Zeballos</i>	PR-07496	Permit		Village of Zeballos
<i>Gold River</i>	PR-03825	Permit		Village of Gold River
<i>Campbell River</i>	MR-02401	Operational Certificate		Regional District of Comox – Strathcona
<i>Sayward</i>	PR-04917	Permit		Regional District of Comox – Strathcona
<i>Cortes Island</i>	PR-01696	Permit		Regional District of Comox – Strathcona
<i>Comox Valley</i>	MR-05050	Operation Certificate		Regional District of Comox-Strathcona
<i>Comox Valley</i>	PR-04865	Permit		Fields Sawmill Limited Partnership
Land Title Amendments				
Facility	Parcel ID (PID)	Registered Owner on Title		Status
<i>Comox Valley</i>	017-941-709	Regional District of Comox-Strathcona		Transferred to CVRD Jan/12
<i>Comox Valley</i>	024-670-545	Regional District of Comox-Strathcona		Transferred to CVRD Jan/12

19. Linkages

19.1 Integrated Resource Recovery

As part of plan implementation, CSWM will be considering opportunities for integrated resource recovery (IRR), linking solid waste management options with liquid waste and other biomass management options. In particular, IRR will be considered as part of assessing organics processing and waste-to-energy options.

The CVRD has commissioned a report entitled Integrated Resource Recovery Options for the Comox Valley Regional District (Farallon Consultants Limited, 2012). IRR is based on the view that waste is a resource that can be used to provide economic, social and environmental benefit. The IRR study objectives are to determine viable resource recovery options with the potential to generate revenue and provide other benefits such as greenhouse gas emissions reductions and reductions in the lifecycle costs of waste management.

19.2 Land Use Planning

Waste management facilities, including recycling, composting and disposal facilities are essential elements of a sustainable waste management system. The siting and operation of these facilities should be undertaken in conjunction with long-range community planning at the local government level to protect the environment and minimize the potential for future land use conflicts.

19.3 First Nations

To ensure proper and cost-effective management of municipal solid waste generated in First Nations communities within the CSWM area, it is recommended that CSWM continually liaise with local First Nations to:

- Identify future disposal requirements at CSWM facilities;
- Identify opportunities to work cooperatively for waste management servicing (e.g., garbage and recycling collection);
- Provide opportunities to participate in all programs; and
- Develop service agreements to ensure that First Nations communities are included in the infrastructure services provided, or to be provided, by CSWM or the municipalities.

19.4 Other Regional Districts

To minimize the likelihood of unapproved inter-regional movement of residual waste, it is proposed that CSWM aim for consistency in waste management policies with Vancouver Island regional districts. CSWM will liaise with other Vancouver Island regional districts on a regular basis to share information and, as appropriate, conduct collaborative studies (e.g. WTE studies).

20. Solid Waste Management Facility Review

To ensure that any proposed solid waste management facility is aligned with Solid Waste Management Plan and to ensure that any decisions related to facility approvals are fully informed, all applications or notifications submitted to the MoE or a local government in the CSWM area related to the development of a municipal solid waste management facility should be forwarded to the CSWM board for comment.

21. Provincial Legislation

Solid waste management is subject to provincial Acts and Regulations. The following is a list of BC legislation that influences how solid waste (residual waste, recyclables and compostable waste) is managed by the public, private and non-profit sectors and will need to be considered and adhered to in the on-going operation of CSWM facilities:

- Municipal Government Act;
- Environmental Management Act;
- Contaminated Sites Regulation;
- Hazardous Waste Regulation;
- Landfill Gas Management Regulation;

- Organic Matter Recycling Regulation;
- Ozone Depleting Substances and Other Halocarbons Regulation;
- Recycling Regulation;
- Storage of Recyclable Material Regulation;
- Local Government Act;
- Community Charter; and
- Regional Districts Liabilities Regulation

22. Implementation Schedule

Table 22-1 outlines the planned implementation schedule for the Solid Waste Management Plan from 2012 to 2022. Board authorization is required for all diversion and education programs or plan changes as well as for capital improvements per existing financial policy. For new or significant changes to programs, one year is suggested for program development and start-up (shown in orange on the table). For new infrastructure and other capital projects, one or more years may be required for additional research and / or design.

Table 22-1. Proposed Implementation Schedule

PLAN COMPONENT	2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022
General Policies											
3 Stream Waste Management											
Disposal Bans on Recyclable and EPR Wastes											
Financial Incentives To Encourage Source Separation											
Long-Range Planning for Waste Management Infrastructure											
Reduction and Reuse											
On-Line Recycling/Reuse Directory											
Reduction and Reuse Promotional Campaign											
Encourage Professional Salvage											
Residential Recycling											
Maintain current curbside services											
Food waste collection											
Mandatory Multi-Family Recycling											
Recycling Depots											
Recycling at Waste Management Centres											
ICI Recycling											
Variable Tipping Fees											
Recycling by the ICI Sector											
ICI Technical Assistance Program											
Local Government Leadership											
Mandatory Space Allocation											
Organic Waste Diversion											
Develop Composting Capacity											
Yard waste collection											
Backyard composting											
Household Hazardous Waste and EPR											
Construction & Demo Waste Management											
CD Project Permitting											
CD Waste Recycling Services											
Promotion and education											
Promotion & Education											
Bear Human Conflict Management											
Illegal Dumping Prevention Plan and Bylaw											
Residual Waste Management											
Comox Valley WMC											
Closure of filled area											
Landfill Expansion (Lined Cell 1)											
Campbell River WMC											
Closure of filled area											
Transfer station											
Tahsis Waste Management Centre											
Closure of filled area											
Zeballos Waste Management Centre											
Closure of filled area											
Gold River Waste Management Centre											
Closure of filled area											
Regional Transfer Stations											
Closure of Cortes and Sayward Landfills											
Waste to Energy											
Administrative Revisions for Public Landfills											
First Nations Linkages											
Waste Composition Study											
SWMP Update											

Legend:

Program development or infrastructure planning

On-going program/activity



23. Budget

23.1 Estimated Expenditures

The estimated operating costs of the solid waste management system are not expected to change significantly as a result of the actions listed in this Plan.

Table 23-1 shows the estimated capital expenditures from 2012 to 2022 for the recommended solid waste management options. These costs were developed by CSWM staff. The most significant capital expenditures are associated with the closure of the completed slopes at the two regional landfills and the development of new regional landfill space that meets BC Landfill Criteria.

23.2 Funding Mechanisms

MoE guidelines for solid waste management plans indicate that major municipal and regional district funding sources for solid waste management services be included in the plan. The funding mechanisms that will be utilized to fund the Plan's implementation include:

- User fees;
- Landfill tipping fees;
- Sales (e.g., backyard composters, compost, recyclables);
- Reserve funds generated by tipping fees; and
- Grants (if available).

Table 23-2 shows the anticipated funding mechanisms for each proposed Plan component. The funding of CSWM services is primarily through user fees and tipping fees (as per CVRD Bylaw 170). Opportunities for sponsorship and grants will be explored to assist in funding.

23.3 Authority to Borrow Funds

It is anticipated that existing reserves will not be adequate to fund all of the capital projects listed in this plan and consequently, borrowing of funds will be required. The *Environmental Management Act* (Section 24(7)) states that once the Plan is approved by the Minister of Environment, funds that have been identified in the plan for its implementation do not require any additional public approvals, such as a referendum, to borrow funds to implement the plan. However, as with all borrowing for capital projects by local governments, the borrowing of funds to implement a solid waste management plan requires preparation of a bylaw that must be approved of the inspector of the municipalities.

Table 23-1: Estimated Capital Expenditures (based on 2012 \$)

Plan Component	Local or Regional Service	Estimated Future Capital Expenditures (2012 \$)										
		2012	2013	2014	2015	2016	2017	2018	2019	2020	2021	2022
Residential Recycling												
Food waste collection containers	Local				\$ 962,000							
Mandatory Multi-Family Recycling	Local	Depends on local services offered (Services are intended to be full user pay)										
ICI Recycling												
Mandatory Recycling by the ICI Sector	Local	Depends on local services offered (Services are intended to be full user pay)										
Organic Waste Diversion												
Develop Composting Capacity	Regional			\$ 300,000	\$ 200,000							
Residual Waste Management												
Comox Valley WMC												
Closure of filled area	Regional	\$ 205,000	\$ 2,591,000	\$ 1,195,000					\$ 353,000			
Landfill Expansion - Lined Cell 1	Regional		\$ 215,000	\$ 72,000	\$ 72,000	\$ 357,000	\$ 4,779,000	\$ 4,779,000				
Campbell Rver WMC												
Closure of filled area	Regional	\$ 2,830,000	\$ 4,467,000					\$ 365,000			\$ 944,000	\$ 920,000
Transfer station	Regional	\$ 1,500,000										
Tahsis Waste Management Centre												
Closure of filled area	Regional	\$ 44,000	\$ 143,000				\$ 100,000	\$ 226,000	\$ 120,000			
Zeballos Waste Management Centre												
Closure of filled area	Regional	\$ 44,000	\$ 177,000				\$ 100,000	\$ 200,000	\$ 120,000			
Gold River Waste Management Centre												
Closure of filled area	Regional	\$ 51,000	\$ 108,000	\$ 225,000					\$ 266,000			
Regional Transfer Stations	Regional			\$ 50,000	\$ 200,000			\$ 100,000	\$ 100,000			
Closure of Cortes and Sayward Landfills	Regional		\$ 16,000	\$ 184,000								
Total Estimated Expenditure		\$ 4,674,000	\$ 7,717,000	\$ 2,026,000	\$ 1,434,000	\$ 357,000	\$ 4,979,000	\$ 5,670,000	\$ 959,000	\$ -	\$ 944,000	\$ 920,000

Note: Debt is anticipated to be incurred for some capital projects.

Table 23-2. Anticipated Funding Mechanisms

Plan Component	Local or Regional Service	Proposed Cost Recovery Mechanisms				
		User Fees	Tipping Fee Revenue	Tipping Fee Reserves	Sales	Grants (Potential)
Reduction and Reuse Activities	Regional		✓			
Residential Recycling						
Curbside Services	Local	✓				
Food Waste Collection	Local	✓				✓
Mandatory Multi-Family Recycling	Local	✓				
Multi-Family Recycling Education Program	Regional		✓			
Recycling Depots	Regional		✓			
Recycling at Waste Management Centres	Regional		✓			
ICI Recycling						
Mandatory Recycling by the ICI Sector	Local	✓				
ICI Technical Assistance Program	Regional		✓			
Organic Waste Diversion						
Regional Composting Facility	Regional		✓		✓	✓
Yard Waste Collection	Local	✓				
Backyard composting	Regional		✓			
Construction & Demo Waste Management						
CD Waste Recycling Services	Regional		✓			
Promotion and Education	Regional		✓			
Promotion & Education	Regional		✓			
Bear Human Conflict Management	Regional		✓			
Illegal Dumping Prevention	Regional		✓			
Residual Waste Management						
Comox Valley WMC	Regional					
Operation			✓			
Closure of Filled Area	Regional			✓		
Expansion	Regional			✓		
Campbell River WMC	Regional					
Operation			✓			
Closure of Filled Area	Regional			✓		
Tahsis Waste Management Centre	Regional					
Operation			✓			
Closure of Filled Area	Regional			✓		
Construction of Transfer Station	Regional					
Zeballos Waste Management Centre	Regional					
Operation			✓			
Closure of Filled Area	Regional			✓		
Construction of Transfer Station	Regional					
Gold River Waste Management Centre	Regional					
Operation			✓			
Closure of Filled Area	Regional			✓		
Kyuquot	Local		✓			
Closure of Cortes and Sayward Landfills	Regional			✓		
Waste Composition Study	Regional		✓			
SWMP Update	Regional		✓			

24. Plan Target

Based on the schedule provided in Table 22-1 and the estimated diversion listed in Table 17-1, this plan targets an increase of the diversion rate from 51% to 70%, as represented in Figure 23-1. Achieving this target will be based on reducing the per capita waste sent for disposal in CSWM landfills from 0.61 tonnes per capita (2011) to 0.38 tonnes per capita by 2022.

25. Plan Monitoring and Measurement

25.1 Plan Monitoring

The Regional Solid Waste Management Advisory Committee will be responsible for monitoring the implementation of the Plan. The objective of this committee is to aid in the management of the regional solid waste management services at the administrative level and to provide a forum for review of all recommendations to the Solid Waste Board. This committee typically meets four or five times per year.

Meeting minutes, as well as the year-end summary report on the progress of the Solid Waste Management Plan will be provided on an annual basis to the Solid Waste Board and the MoE. The terms of reference for the committee will be included in the first annual report submitted to the Ministry.

25.2 Plan Measurement

Measurement of progress towards the plan's target of "0.38 tonnes of waste disposed per capita per year in CSWM landfills" will be done through an annual review of scale data from the waste management centres.

Progress will also be measured through waste composition studies (described below) that will provide an indication of how successful the plan's programs and policies are at affecting diversion of specific waste streams (e.g. wood, organics, cardboard) and from specific sectors (e.g. single-family residential, multi-family residential, ICI). Waste composition studies will also indicate where enhancements can be made to improve diversion program performance.

25.2.1 Waste Composition Studies

In 2013, a multi-location waste composition study on the residual waste disposed in CSWM landfills is planned to establish a current baseline specific to CSWM. A follow-up composition study is planned for 2017, in advance of the next SWMP update, to assist in assessing the success of this plan in achieving specific diversion measures, as well as to identify future opportunities for additional diversion. Each composition analysis will be a "moment in time" look at the waste stream, and as such, should be conducted during the summer so that the prevalence of yard waste in the waste stream can also be estimated.

25.3 Annual Operating and Monitoring Reports

Annual operating and monitoring reports will be prepared for CSWM landfills in accordance with the specific Operational Certificate/Permit requirements for each site. Each landfill report will be for the preceding 12 months from January 1 to December 31st of each year and will include:

- An executive summary;
- A review of the preceding year of operation, plans for the next year and any new information or proposed changes relating to the facility;

- A summary of the landfill operation equipment;
- The tonnage of each type of waste discharged into the landfill;
- The remaining site life and capacity update;
- An updated estimate of the MSW disposal per capita;
- A waste area population table including adjusted projected population for the estimated facility life;
- A certified aerial survey including volume changes, on required frequency; and
- Certified updates to the landfill financial assurance report (part of Landfill Operational Plan) as required per the Public Sector Accounting, Section PS 3270 and related to the most current Landfill Closure Plan.

25.4 Plan Updates

The provincial *Guide to the Preparation of Regional Solid Waste Management Plans by Regional Districts* recommends that a review and update of the Solid Waste Management Plan be undertaken every five years to ensure that the Plan reflects the current needs of the CSWM area. The next plan review is anticipated to begin in 2017 and any necessary plan amendments and updates will be done as part of the review.

25.5 Plan Flexibility

Costs provided in this plan are estimates in 2012 dollars and may not reflect actual costs at the time of implementation. As a result, programs and infrastructure may undergo further assessment, including an assessment of costs and continued community support, by the Solid Waste Management Advisory Committee prior to implementation.

The Plan implementation schedule is intended to be flexible to allow for changes in priorities and available funding for CSWM services. Notwithstanding, the contents of this Plan are subject to legal requirements and, as a result, guidance and the direction from the MoE will be sought in regards to the level of flexibility, as appropriate.

25.6 Dispute Resolution

It is recognized that disputes may arise among stakeholders during Plan implementation. Disputes will first be presented to the Regional Solid Waste Management Advisory Committee for review, consideration and for recommendations to the Solid Waste board. Parties involved in the dispute will be given the opportunity to speak to the Regional Solid Waste Management Advisory Committee and to the Solid Waste board to present their viewpoints. Disputes may be settled by the Solid Waste board. The dispute resolution is limited to the following types of disputes:

- Administrative decisions made by CSWM staff.
- Interpretation of a statement or provision in the Plan.
- Any other matter not related to a proposed change to the actual wording of the Plan or an Operational Certificate.

Disputes that cannot be resolved at the Solid Waste board level or that are beyond the scope of the disputes described above may be referred to the Regional Manager of the Ministry of Environment or an independent arbitrator, who will make a final, binding decision.

Disputes between CSWM and member municipalities or First Nations in relation to implementation or interpretation of this Plan will be referred to the Regional Manager of the Ministry of Environment to assist in resolving the dispute. Disputes that cannot be resolved by the Regional Manager will be referred to an independent arbitrator, who will make a final, binding decision. The costs for this arbitration will be split between the parties in dispute.

26. Board Resolution

On September 13, 2012 the Comox Strathcona waste management board adopted a resolution to:

- Approve the 2012 Comox Strathcona solid waste management plan (SWMP);
- Forward the 2012 SWMP to all participating local government jurisdictions requesting a letter of support per Ministry of Environment requirements;
- Forward the 2012 SWMP to all First Nation communities requesting a letter identifying any comments or concerns; and
- Forward the 2012 SWMP to the Minister of Environment requesting SWMP approval.

December 7, 2012

Thomas A. Boatman, PE, Manager of Solid Waste
Comox Valley Regional District
600 Comox Road
Courtenay, BC V9N 3P6

Dear Mr. Boatman:

Project No: 60141938-10
Regarding: 2012 Solid Waste Management Plan

AECOM, in association with Maura Walker and Associates and Jan Enns Communications, are pleased to submit the 2012 Solid Waste Management Plan for the Comox Valley Regional District. This report describes the programs, policies and infrastructure for managing the solid waste generated in the Comox Valley Regional District and Strathcona Regional District geographic areas and is designed to achieve 70% waste diversion upon implementation. In addition, recommendations are made concerning development of future waste disposal facilities that will be needed for the Comox Strathcona waste management services to continue to dispose of residual waste in the future.

Sincerely,

AECOM Canada Ltd.



Todd Baker, P.Eng.
Senior Environmental Engineer
Todd.Baker@aecom.com

MW:cap
Encl.

Signatures

**Report Prepared
By:**



Todd Baker, P.Eng.
Senior Environmental Engineer
AECOM



Maura Walker
Senior Environmental Planner
Maura Walker and Associates

Glossary / Acronym List

Term	Definition / Description
Aseptic containers	Juice boxes, dairy and dairy substitute containers, soup boxes, etc.
Board	Comox Strathcona waste board
CD waste	Construction and demolition (CD) materials consist of the waste generated during the construction, renovation, and demolition of buildings, roads, and bridges. C&D materials often contain bulky, heavy materials, such as concrete, wood, metals, glass, and salvaged building components.
Carpet	Carpet, rugs
Clean wood waste	<p>Uncontaminated wood or wood products, from which hardware, fittings and attachments, unless they are predominantly wood or cellulose, have been removed (e.g., clean wooden shakes and shingles, lumber, wooden siding, posts, beams or logs from log home construction, fence posts and rails, wooden decking, mill work and cabinetry). Clean wood waste excludes:</p> <ul style="list-style-type: none"> ▪ Any engineered or chemically treated wood products, such as products with added glues or those treated for insect or rot control (oriented strand board, plywood, medium density fiberboard, wood laminates or wood treated with chromate copper arsenate, ammoniacal copper arsenate, pentachlorophenol or creosote); ▪ Upholstered articles; ▪ Painted or varnished wood articles or wood with physical contaminants, such as plaster, metal, or plastic; and ▪ Any wood articles to which a rigid surface treatment is affixed or adhered. <p>Clean wood waste also excludes other materials found in the construction and demolition waste stream such as gypsum or drywall, fiberglass, asphalt or fiberglass roofing shingles, metals or plastics.</p>
Composite materials	<ul style="list-style-type: none"> ▪ Packaging (dog food bags, food packaging such as cylindrical cardboard and metal tubes). ▪ Household goods (children's toys). ▪ Diapers, sanitary products ▪ Bulky items (luggage, sporting goods)
Compostable paper products	Compostable packaging, coffee cups, paper bags, tissues, paper towel
CSWM	Comox Strathcona waste management
CVRD	Comox Valley Regional District
Dirty wood waste	Treated wood, painted wood
Disposal	Waste that is sent to landfill
DLC waste	Demolition, land clearing and construction waste.
Diversion	Waste that is generated and managed through recycling or reuse instead of being disposed.
E-waste	Electronic waste. All products operating by AC or DC current.
Extended Producer Responsibility (EPR)	An environmental policy approach in which a producer's responsibility (physical and/or financial) for a product is extended to the post-consumer stage of a product's lifecycle. There are two key features of EPR policy: (1) the shifting of responsibility (physically and/or economically, fully or partially) upstream to the producer and away from local governments, and (2) to provide incentives to producers to take environmental considerations into the design of the product.
Glass	<p>Includes:</p> <ul style="list-style-type: none"> ▪ Beverage containers (deposit and non-deposit bearing containers) ▪ Glass food containers (jars) ▪ Non-container glass (broken glass, picture frames, etc.)
Gypsum	Drywall
Household Hazardous Waste (HHW)	Toxic waste products generated by residential premises which include items covered by stewardship programs (such as paint, solvents, pesticides, used oil and containers, and batteries) and similar items not covered by stewardship programs.
ICI waste	Waste generated by institutions (such as schools), commercial establishments (such as stores, restaurants) and industrial establishments (light manufacturing)
Inert waste	Waste that does not biodegrade, including dirt, rocks, ash, concrete, stone

Term	Definition / Description
Land Clearing Waste	Debris created by the clearing of land, typically done as the first step in a construction or development project. Land clearing waste is typically made up of tree stumps, trunks, branches and associated brush.
Metal	Includes: <ul style="list-style-type: none"> ▪ Beverage containers (deposit and non-deposit bearing containers) ▪ Metal food containers (cans) ▪ Household metal (keys, nails, hangers etc.) ▪ Non-household metal (siding, pipes)
MoE	BC Ministry of Environment
MSW	Municipal solid waste. Includes predominantly household and commercial waste. MSW generally excludes hazardous wastes.
Organics	Includes: <ul style="list-style-type: none"> ▪ Food waste ▪ Yard waste (grass clippings, yard trimmings) ▪ Organic matter as defined by the Organic Matter Recycling Regulation
(The) Plan	Comox Strathcona Solid Waste Management Plan
Plastics #1-7	<p>#1 PET: soda bottles and water bottles</p> <p>#2 HDPE: milk bottles, detergent bottles and grocery/trash/retail bags</p> <p>#3 PVC: loose-leaf binders and plastic pipes</p> <p>#4 LDPE: dry-cleaning bags, produce bags and squeezable bottles</p> <p>#5 PP: medicine bottles, aerosol caps, drinking straws and food containers (such as yogurt, ketchup bottles and yogurt tubs)</p> <p>#6 PS: compact disc jackets, packaging Styrofoam peanuts and plastic tableware</p> <p>#7 Other: reusable water bottles, certain kinds of food containers, plastic consumer goods</p>
Product Stewardship	A term used in British Columbia to describe a government strategy to place the responsibility for end of life product management on the producer and consumers of a product and not the general tax payer or local government.
Reduction	Waste that is prevented from being generated. This may be achieved through changes in consumption habits or changes in the way products are sold.
Residential waste	Waste generated by households.
Residual waste	The waste that has not been captured through reuse, recycling or composting programs and requires disposal.
SRD	Strathcona Regional District
SWMP	Comox Strathcona Solid Waste Management Plan
Textiles	Clothing, rags, cloth material.
Waste management hierarchy	A concept that refers to the 5Rs of waste management: reduce, reuse, recycle, recover, residuals management. The hierarchy places greater emphasis on up-stream waste management activities, such as reduce and reuse.
Waste-to-energy (WTE)	Waste-to-energy (WTE) is the process of generating energy in the form of electricity or heat from the incineration of waste source. WTE also refers to a range of processes) where the waste is burned, gasified or digested at a high temperature. Most WTE processes generate electricity directly through combustion, or produce a combustible fuel commodity, such as methane, methanol, ethanol or synthetic fuels.

Statement of Qualifications and Limitations

The attached Report (the "Report") has been prepared by AECOM Canada Ltd. ("Consultant") for the benefit of the client ("Client") in accordance with the agreement between Consultant and Client, including the scope of work detailed therein (the "Agreement").

The information, data, recommendations and conclusions contained in the Report (collectively, the "Information"):

- is subject to the scope, schedule, and other constraints and limitations in the Agreement and the qualifications contained in the Report (the "Limitations");
- represents Consultant's professional judgement in light of the Limitations and industry standards for the preparation of similar reports;
- may be based on information provided to Consultant which has not been independently verified;
- has not been updated since the date of issuance of the Report and its accuracy is limited to the time period and circumstances in which it was collected, processed, made or issued;
- must be read as a whole and sections thereof should not be read out of such context;
- was prepared for the specific purposes described in the Report and the Agreement; and
- in the case of subsurface, environmental or geotechnical conditions, may be based on limited testing and on the assumption that such conditions are uniform and not variable either geographically or over time.

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City of Campbell River
From the Office of the Mayor

Comox Valley Regional District

RECEIVED

File:



DEC 03 2012

To:

cc:

30 November, 2012

Mr. Tom Boatman
Manager of Solid Waste
Comox Valley Regional District
600 Comox Road
Courtenay, BC V9N3P6

RE: Regional Solid Waste Management Plan Update

Dear Mr. Boatman:

This letter is to acknowledge the City of Campbell River's support of the Regional Solid Waste Management Plan completed in the fall of 2012. This letter is intended for inclusion in the submission of the regional plan to the Ministry of Environment.

The plan aligns with the City of Campbell River's community goals set forth in the Sustainable Official Community Plan (2012), which support efforts to move toward a long-term objective of zero waste, and a shorter term objective of achieving a 70% waste diversion rate.

The plan sets forth key recommendations around waste reduction, as well as guidance for addressing landfill capacity issues, the increasing demand for organics diversion, and enhanced recycling facilities. As indicated in our Sustainable Official Community Plan, Campbell River is especially interested in upcoming opportunities to encourage organics diversion across residential and commercial sectors, and the expansion of recycling services to include multifamily residential units.

The City of Campbell River appreciates the Comox Strathcona Waste Management staff's efforts to ensure the inclusion of the City throughout the plan development. The City appreciated the opportunity to have Council and staff participation on the plan steering committee, as well as the presentations and public engagement efforts made by Comox Strathcona Waste Management in our community.

We look forward to continuing to work with Comox Strathcona Waste Management on the implementation of the plan.

Sincerely,

Walter Jakeway
MAYOR



TOWN OF COMOX

OFFICE OF THE MAYOR

Comox Valley Regional District
RECEIVED

File: 5360-02

OCT 14 2012

To: [unclear]

cc: [unclear] [unclear] [unclear]
[unclear]

File No: 12-344 / 5360-02

October 10, 2012

Mr. Edwin Grieve, Chair
Comox Valley Regional District
600 Comox Road
Courtenay BC V9N 3J6

Dear Mr. Grieve:

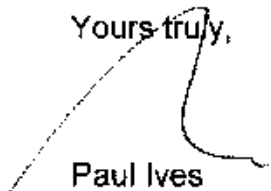
Thank you for your September 20, 2012 letter requesting that Council review the final Solid Waste Management Plan, and provide support by way of a resolution. Please be advised that Council passed the following at its Regular Meeting of October 3, 2012:

That the September 20, 2012 letter from Edwin Grieve, Chair of the Comox Valley Regional District, requesting Council's review and support of the Solid Waste Management Plan, be received; and further,

That Council support the Solid Waste Management Plan.

If you require anything further, please do not hesitate to contact Town staff or me at 250 339-2202.

Yours truly,


Paul Ives
Mayor

THE CORPORATION OF THE CITY OF COURTENAY

*Corporate Services Department
830 Cliffe Avenue
Courtenay BC V9N 2J7*



*phone: (250) 334-4441
fax: (250) 334-4241
jward@courtenay.ca*

December 11, 2012

Comox Valley Regional District
600 Comox Road
Courtenay, B.C.
V9N 3P6

Re: Approval of the Solid Waste Management Plan

This is in reply to your letter of September 20, 2012. Please be advised at its regular Council meeting held December 10, 2012 passed the following resolution:

“That Council review and provide a letter of support to the Comox Valley Regional District for the Comox Strathcona Solid Waste Management Plan.”

I hereby certify that the above resolution was adopted by the Council of the City of Courtenay on December 10, 2012.

Dated this 11th day of December, 2012 at Courtenay, B.C.

A handwritten signature in black ink, appearing to read 'T. Manthey', written over a horizontal line.

T. Manthey, BA, CGA
Director of Financial Services/Deputy CAO



Corporation of the Village of Cumberland

2673 Dunsmuir Avenue
P.O. Box 340
Cumberland, BC V0R 1S0
Telephone: 250-336-2291
Fax: 250-336-2321
cumberland.ca

February 26, 2013

Chair Edwin Grieve
Comox Valley Regional District
600 Comox Road
Courtenay, BC V9N 3P6

5360-30/SWMP

Web

DOakman

JWarren

ISmith

TBoatman

Dear Edwin,

**Re: Comox Strathcona Waste Management - 2012 Solid Waste Management Plan
Village of Cumberland Letter of Support**

The Village of Cumberland Council adopted the following resolution at its February 25, 2013 meeting:

THAT the Village of Cumberland provide a letter of support for the Comox Strathcona Waste Management – 2012 Solid Waste Management plan as per Ministry of Environment requirements.

The Village of Cumberland is happy to provide this letter of support, with the acknowledgement that the contents of the 2012 Solid Waste Management Plan, in conjunction with the accompanying host community benefit agreement, offers the possibility for the Village of Cumberland and Comox Strathcona Waste Management to work together in a progressive and positive manner for years to come.

The Village looks forward to moving through the approval process for the execution of the agreement and once again we thank you for your help in helping to create a positive working relationship.

Sincerely,

Leslie Baird
Mayor
Village of Cumberland

cc: Honourable Terry Lake, Minister of Environment
Sundance Topham, Chief Administrative Officer, Village of Cumberland
Debra Oakman, Chief Administrative Officer, Comox Valley Regional District

REGIONAL CONTEXT STATEMENT

Town of Comox Official Community Plan Bylaw 1685

1.6 Regional Context Statement

A Regional Growth Strategy for the Comox Valley Regional District was initiated under section 431 of the *Local Government Act*. The purpose of a Regional Growth Strategy is to guide decisions on growth, change and development within the Regional District.

A regional growth strategy is required to cover a period of at least 20 years from the time of its initiation and must include:

- a comprehensive statement on the future of the region, including the social, economic and environmental objectives of the board in relation to the regional district;
- population and employment projections for the period covered by the regional growth strategy;
- actions proposed for the regional district to provide for the needs of the projected population in relation to housing, transportation, regional district services, parks and natural areas, and economic development; and
- targets for the reduction of greenhouse gas emissions in the regional district, and policies and actions proposed for the regional district with respect to achieving those targets.

If a regional growth strategy applies to all or part of the same area of a municipality as an official community plan, the official community plan must include a regional context statement. The regional context statement must specifically identify the relationship between the official community plan and the matters referred to above and if applicable, how the Official Community Plan is to be made consistent with the Regional Growth Strategy over time.

Comox Valley Regional District Regional Growth Strategy Bylaw No. 120, 2010 consists of five parts including an introductory section.

Part 2 contains 11 major trends which shape the Regional Growth Strategy (RGS). The Comox Official Community Plan recognizes the importance of these major trends and all are addressed in the land use designations, objectives and policies. While all are important, population growth due to in-migration from other parts of B.C. and other provinces, demographic changes due to an aging population, environmental protection and the need to increase transportation choices are key drivers of the Comox OCP. The RGS projects a population increase of 19,800 from 2010 to 2030 for the CVRD. An annual growth rate of 1.6% is projected to slowly decline to 1.0% over the 20 year timeframe of the RGS. This is consistent with the OCP which anticipates a lower growth rate for the Town of Comox than the CVRD as a whole due to a limited land base and a strong desire for incremental change from Comox residents.

An aging population will generate the need for a variety of more compact housing forms regardless of population increase from outside Comox. The Comox OCP proposes to address these population and demographic changes primarily through increasing opportunities for densification in Downtown Comox and within easy walking distance of transit corridors, located along arterial and collector roads.

'Greenfield' development will serve as a secondary means of accommodating growth but will not be sufficient due to a limited land supply and environmental constraints. Comox is blessed with numerous environmentally sensitive areas. The OCP proposes to accommodate very limited growth in these areas and only subject to extensive development permit regulation.

Improving transportation choices is of critical significance in order to take advantage of existing underutilized infrastructure, forego extensive capital expenditures, and reduce GHG emissions. The Comox OCP anticipates over three quarters of the population of Comox in 2030 will be within 400 metres of existing transit routes.

The time frame of the RGS is the 20 year period from 2010 to 2030.

Goals of the Regional Growth Strategy

Part 3 of the RGS contains eight over-riding goals. Each goal includes a series of objectives, targets and supporting policies. The OCP supports each goal through the following objectives and policies:

RGS Goal 1: Housing	Comox OCP Supporting Objectives/Policies
<i>Ensure a diversity of housing options to meet evolving demographics and needs.</i>	<i>Section 2.1.1.2 Residential Objectives: 1, 2 & 3</i> <i>Section 2.1.1.3 Residential Policies: a, b, c, d, e, g, n, l & m</i>
	<i>Section 2.1.2.3 Affordable, Rental and Special Needs Housing Policies: a, c, d & e.</i>
RGS Goal 2: Ecosystems, Natural Areas & Parks	Comox OCP Supporting Objectives/Policies
<i>Protect, steward and enhance the natural environment and ecological connections and systems.</i>	<i>Section 2.1.8.2 Parks, Trails & Open Space Objectives: 1, 2, 3, 5, 6, 7 & 12.</i> <i>Section 2.1.8.3 Parks, Trails and Open Space Policies: a, b, c</i>
	<i>Section 2.1.9.3 Agriculture and Aquaculture Supporting Policies: b & g.</i>
	<i>Section 2.3.12 Environmental Objectives: 1, 2, 3, 4 & 5.</i> <i>Section 2.3.13 Environment Policies: a, l & v.</i>
RGS Goal 3: Local Economic Development:	Comox OCP Supporting Objectives/Policies
<i>Achieve a sustainable, resilient and dynamic local economy that supports Comox Valley businesses and the region's entrepreneurial spirit.</i>	<i>Section 2.1.9.3 Agriculture and Aquaculture Supporting Policies: k & l.</i>
	<i>Section 2.1.10.3 19 Wing Comox Supporting Policies: b & c.</i>

It should be noted that the analysis of the municipal infrastructure by Koers & Associates indicated the anticipated growth in the OCP could be accommodated by existing infrastructure except in new development areas where developers would be responsible.

RGS Goal 6: Food Systems	Comox OCP Supporting Objectives/Policies
<i>Support and enhance the agricultural and aquaculture sectors and increase local food security.</i>	<i>Section 2.1.9.2 Agriculture and Aquaculture Supporting Objectives: 1 & 2</i> <i>Section 2.1.9.3 Agriculture and Aquaculture Supporting Policies: b, c & i.</i>
RGS Goal 7: Public Health and Safety	Comox OCP Supporting Objectives/Policies
<i>Support a high quality of life through the protection and enhancement of community health, safety and well-being.</i>	<i>Section 2.4.2.3 Water Service Supporting Policies: a</i> <i>Section 2.4.2.5 Sanitary Sewerage Service: a</i>
RGS Goal 8: Climate Change	Comox OCP Supporting Objectives/Policies
<i>Minimize regional greenhouse gas emissions and plan for adaptation.</i>	<i>Section 2.3.13 Environment Policies: j & k</i>

The overall approach of the OCP is to encourage redevelopment through infill making full use of existing infrastructure. Implementation of the Plan is anticipated to result in a reduction of per capita GHG emissions of 28% by 2030, compared to the 2007 baseline.

RGS Growth Management Principles

Part 4 of the RGS addresses Growth Management. Included are 16 growth management principles, all of which are supported in the Comox OCP, where applicable. These reinforce the goals identified in Part 3 of the RGS. In addition, the RGS requires areas of existing development on the fringes of municipal areas to be identified that should eventually be incorporated through boundary extensions and provided with publicly owned water and sewer services in order to address existing public health and environmental issues. There are no known existing public health or environmental issues on the fringe of Town of Comox which meet those criteria and therefore no such areas have been identified within the Comox OCP.

The RGS also requires areas to be identified on the fringes of municipal areas that should eventually be incorporated through boundary extensions and provided with publicly owned water and sewer services in order to allow for new long-term growth opportunities in a phased and orderly manner. The Comox OCP can accommodate all projected development without any expansion in the boundaries of the Town of Comox. However the Town’s small land base, varying degrees of interest amongst property owners in proceeding with development at any given time and land assembly issues could lead to restrictions in land supply relative to demand. To address this possibility, the OCP identifies three areas where potential growth could be accommodated in a phased and orderly manner. Area A is


located in the Butchers Road surrounded on three sides by the Town of Comox. Area B is located west of Anderton Road in northwest Comox. Area C is located east of Anderton Road and extends north of Knight Road past Ryan Road to the ALR boundary. Other potential expansion areas are not identified due to environmental constraints, ALR status, and limited development potential.

The Town Centre for Comox as identified in the RGS MG Policy 1.3 consists of Downtown Comox as shown on *Map 1 – Land Use Designations*.

Compliance with RGS MG Policy 2B-1 is achieved as no ALR exclusion is proposed and all policies are in accordance with the Agricultural Land Commission Act and regulation under the Act.

Part 5 of the RGS addresses implementation measures, monitoring and amendment procedures. The OCP identifies policies with specific implementation measures requiring additional action outside of the OCP update process; these are contained in Part 4 of the OCP. Measures to monitor the OCP are also included in Part 4. These are identified generally as it is anticipated specific measurement indices may change over time. Annual monitoring of some indices should take place while a broader review of the context statement should occur within five years.

ATTACHMENT 10
COUNCIL POLICY CCL-069

 TOWN OF COMOX		POLICY AND PROCEDURE MANUAL	
AFFORDABLE HOUSING AMENITY CONTRIBUTION POLICY			
Section: COUNCIL	Number: CCL-069	Office of Primary Responsibility: CORPORATE SERVICES	
Type: <input checked="" type="checkbox"/> Policy <input checked="" type="checkbox"/> Procedure	Authority: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Administrative	Approved By: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Chief Administrative Officer <input type="checkbox"/> Department Head	
Date Adopted: March 16, 2022	Date Last Amended: June 1, 2022	Date to be Reviewed: March 2023	
Manner Issued: Website, Internal Memo, Upon request			

1 PURPOSE

- 1.01 The purpose of this policy is to seek developer contributions for Affordable Housing as an amenity at time of zoning amendment.
- 1.02 The purpose of this policy is to direct developer contributions collected under this policy towards either the acquisition of units owned by the Town and managed by a non-profit housing provider or to provide funds to non-profit housing providers in a partnership for the provision of affordable housing.

2 POLICY STATEMENT

- 2.01 Affordable Housing remains a challenge for many residents within the Town of Comox.
- 2.02 The number of households in core housing need is increasing within the Town.
- 2.03 The construction of non-market units helps meet the need for Affordable Housing.
- 2.04 The Town may work through non-profit housing providers to facilitate the management of Affordable Housing.
- 2.05 The Town may seek an Affordable Housing amenity for rezoning applications for four or more principal residential dwellings.

3 DEFINITIONS

- 3.01 "Affordable Housing" means housing where the cost of accommodation does not exceed 30% of a household's gross income.
 - 3.02 "Affordable Housing Operator" means a non-profit housing development corporation whose primary function is the provision and/or management of Affordable Housing units.
 - 3.03 "Cellar" means that portion of a building situated between the top of any floor and the top of the floor next above it, having no more than 0.6 metres of its height above grade, and a height from floor to ceiling of less than 2.1 metres.
 - 3.04 "Multifamily Development" means development consisting of more than one unit on a parcel excluding secondary suites and coach houses.
 - 3.05 "Rental Development" means development where units are limited to residential rental tenure in accordance with section 481.1(1) of the *Local Government Act*
 - 3.06 "Single-family Development" means development consisting of one dwelling unit on fee simple or bare land strata lots with or without a secondary suite or coach house.
-

4 SCOPE

- 4.01 This policy applies to all applications for rezoning where rezoning yields 4 or more additional dwelling units excluding secondary suites and coach houses.
- 4.02 As of the date this policy is adopted by Council, this policy only applies to applications which have not submitted a complete application.
- 4.03 Any amendments to this policy will apply to applications which have not submitted a complete application as of the amendment adoption date, unless otherwise directed by Council.

5 POLICY

- 5.01 As part of an amenity negotiation for rezoning, the Town will seek a developer contribution rate of:
 - (a) \$73 per square metre for Multifamily Development,
 - (b) \$50 per square metre for Rental Development, and
 - (c) \$7,300 per lot for Single-family Development.

The per square metre contribution will be based on the interior floor area of each residential unit, including stairwells and elevator shafts but excluding vehicle parking areas, Class II bicycle parking spaces and Cellars.

- 5.02 The developer contribution rate may be adjusted in consideration of the following factors:
 - (a) The impact of the proposed development on the community, and
 - (b) The size of the proposed units.
- 5.03 The Town will seek to expend contributions collected under this policy either through the purchase or acquisition of units in new developments or through project partnerships with non-profit affordable housing providers. Purchase or acquisition will be at market rate excluding real estate fees.

6 PROCEDURES

- 6.01 Purchase or acquisition of Affordable Housing units: the Town will seek units which meet the following criteria:
 - (a) One bedroom units shall have a floor area of 58 square metres or alternative as accepted by the Town;
 - (b) Two bedroom units shall have a floor area of 75 square metres or alternative as accepted by the Town;
 - (c) All units shall meet the Town’s Adaptable Housing Standards in section 5.20 of the Comox Zoning Bylaw 1850.
- 6.02 Units purchased under section 5.03 will be owned by the Town and offered for lease to an Affordable Housing Operator selected at Council’s discretion.

Amendment Date	Section Amended or Description of Amendment	Resolution Number
March 16, 2022	Policy adopted	2022.117
April 6, 2022	Amended to contain a hybrid long-term acquisition and non-profit provision method (1.02 and 5.07 added).	2022.133
June 1, 2022	Convert multifamily contribution rate to per square meter, add rental development contribution rate and increase single-family contribution rate.	2022.207- 2022.209

ATTACHMENT 11

APPLICANT'S REQUEST TO WAIVE THE AFFORDABLE HOUSING CONTRIBUTION



October 19, 2023

Mr Robin Pallett, Planner
Town of Comox
1809 Beaufort Avenue
Comox, BC V9M 1R9

Re: **Affordable Housing Policy**
Development Application
Highland Village - Retail and Rental Residential Project
1966 Guthrie Road, Comox

Further to our development application, we would like to outline our approach to providing rental housing for the Comox area at as affordable price points as possible. In doing this, we are responding to the Town's Affordable Housing CAC Policy.

Our development is founded on being a transit, walking and bicycle oriented community, where residents can live, work and shop either on the property or close by. We are purposely minimizing the number of underground car parking stalls we are building to keep the project cost lower – why should residents who don't own a car, pay for parking stalls that cost more than \$60,000 per stall to construct. In lieu of building more underground parking we are offering two at-grade secure bicycle storage rooms equipped with e-bike charging outlets, bike wash station and repair workbench – this is unprecedented in that bicycles are usually forced to go underground in the unfriendly car parkades – our proposal prioritizes bicycles and their users to be above grade with natural light and progressive services.

Our location, at Anderton and Guthrie, is on a major bus route and BC Transit has indicated that as the population in the area grows, enhanced bus service will be provided to even better serve the residents and visitors to the area.

We are offering a wide range of units sizes from compact studio suites as small as 342 sqft for singles up to 3-bedroom family units of 1200 sqft in size. This wide range of unit sizes will mean there will be a unit type and price point for a diverse mix of residents including singles, couples, seniors, first time renters, CFB Comox workers and families. With 205 units available, renters will be able to find a price point that meets their income level.



In reading the Town's Affordable Housing Policy, it appears that it is based on the assumption that new housing will be market owned rather than rental. In our experience in developing rental versus owned, only developments that are selling condos have the ability to subsidize an Affordable Fund or donate units. Rental projects don't have the ability to fund affordable housing projects.

It should also be noted that we are in a unique position as a developer in that our cost base for the land is very low – it was bonus land when we acquired the retail plaza. This is the main reason why we can work at making a rental development feasible. If we had to buy the land today at market, it would not work.

As such, we are proposing that by providing a diverse range of housing types in a cost effective building (minimizing the amount of car parking) with transit/biking/walking opportunities, we are providing housing that will include a variety of affordability levels. In fact, as noted in our other letter regarding Town and Regional District Development Cost Charges, we are asking for a reduction in these fees to make our rental project financially feasible.

We respectfully ask that the Town accepts a 100% rental project as providing various levels of affordability and that no financial contribution is required.

Yours truly,

A handwritten signature in black ink, appearing to read 'Norman Laube', is positioned above the typed name.

Norman Laube

Owner's Representative

Avtar Properties Management Group

ACI Comox Investments Ltd.

ATTACHMENT 12

PROPOSED ALR BUFFER VS. THE EXISTING ALR BUFFER COVENANT & ZONING

PROPOSED ALR BUFFER		EXISTING ALR COVENANT (FB150103)	ZONING BYLAW
Proposed design element	Location (on Lot 1)		
a 73.4 m ² portion of the NE outdoor amenity area for tenants of the building programmed for garden plots	in the northwestern area of the buffer	Not permitted	Permitted (s. 8.13(2)[i])
gravel pathway and benches	Along the full length of the buffer, east of the split-rail fence	Not permitted	Not prohibited
bollard lighting and underground electrical conduits	along pathway	Not permitted	Not prohibited
stair encroachments for accessing the gravel pathway directly from at-grade rear residential patios (projecting up to 1.4 m into the buffer)	at grade on the north elevation	Not permitted	Permitted (s. 5.12[2])
upper-storey balcony encroachments (projecting up to 0.61 m into the buffer)	at the 2nd to 5th storey on the north elevation	Not permitted	Permitted (s. 5.12[2])
drainage infrastructure, including: <ul style="list-style-type: none"> • a stormwater swale; • underground pipes carrying drainage <ul style="list-style-type: none"> ○ from parking lots (to the swale); and ○ to an underground holding tank (from the swale, for storm water overflow); and • an oil/grit separator 	Swale: down the middle of the buffer in the east side yard (east of the proposed split-rail fencing). Plumbing & separator: underground	Not permitted	Not prohibited
maintenance path	in the outer layer of the buffer abutting the lot line	Not permitted	Not permitted via s. 8.13(1)(j) (variance required)

PROPOSED ALR BUFFER		EXISTING ALR COVENANT (FB150103)	ZONING BYLAW
Proposed design element	Location (on Lot 1)		
low split-rail fencing for separating the inner strip of the buffer (containing passive recreational uses, from the masking layer of trees)	Down the middle of the buffer in the east side yard (west of the proposed swale).	Not permitted	Not prohibited
1.8 m high chain-link fencing	along the north and east lot lines	required	Not prohibited
1.5 m high chain-link fencing	along the south frontage of the ALR Buffer (behind the BC Hydro kiosk), and between the gravel path and the CRU service access sidewalk, and ending behind CRU 1.	permitted	Not prohibited
Native Shrub and Tree Species that are not reflected on the table in Appendix B1 of the Zoning Bylaw	within 8.0 metre of lot lines shared with land within the ALR	permitted	Not permitted via s. 8.13(1)(b) (variance required)
Spacing of shrubs up to 1.8 m apart	within 8.0 metre of lot lines shared with land within the ALR	permitted	Not permitted via s.8.13(1)(f) (variance required)
Location of masking trees	buffer in the east side yard	Required*	Permitted (s. 8.13(1)[b])*

* Proposed trees appear to be in conflict with the applicant's draft civil drawings.

EXISTING ALR BUFFER LANDSCAPING SPECIFICATIONS
(SCHEDULE "A" OF ALR BUFFER COVENANT FB150103)

COVENANT COPY

REGISTERED VIFB150103 RCVD:2008-02-29 PRNT: 2008-03-12-12.55.30.783921

8

SCHEDULE "A" - LANDSCAPING SPECIFICATIONS

from BC Agricultural Land Commission publication entitled Landscaped Buffer Specifications, dated March 1993.

SCHEDULE A: BUFFER TYPES

A.3 Airborne Particle and Visual Screen

a) Yearly Screen
b) Summer Screen

Buffers agricultural operations from trespass and vandalism while offering a greater physical setback between potential conflicting land uses, visually screening uses from one another and minimizing the exchange of undesirable airborne particulate matter between incompatible land uses. (Note: Coniferous trees should be used in the buffer in situations where visual and particulate screening is required on a year round basis. Solution A.3a)

A1 Yearly Screen
Minimum double row deciduous/coniferous trees. (continuous as shown)
-See Schedules B.2, B.6, C.1 & C.3
-Underplant and fence as shown below

A2 Summer Screen
Minimum double row deciduous trees. (continuous)
-See Schedules B.1, B.6, C.1
-Underplant and fence as shown below

Minimum triple row trespass inhibiting shrubs. (continuous)
-See Schedules B.5 & C.5

Minimum double row screening shrubs. (continuous)
-See Schedules B.6, C.6a & C.6b

Fence as per Schedule B. of Fencing Specifications.

Additional shrub groundcover as required.

Minimum distance from property line to 1st row of trees, 3.0m.

Minimum buffer width, 18.0m or as specified by the commission.

Agricultural Operation

END OF DOCUMENT

Not to Scale

A.3

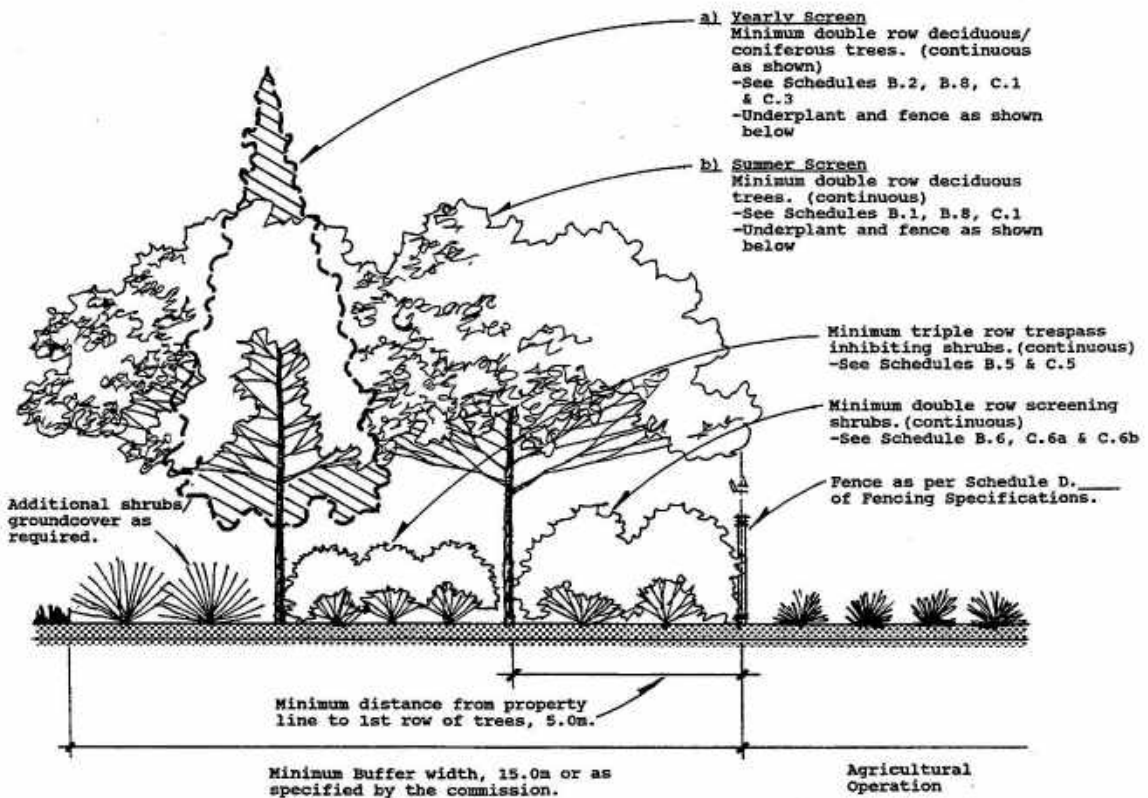
LEGIBLE COPY (FROM THE SOURCE PUBLICATION)

SCHEDULE A: BUFFER TYPES

A.3: Airborne Particle and Visual Screen

- a) Yearly Screen
- b) Summer Screen

Buffers agricultural operations from trespass and vandalism while offering a greater physical setback between potential conflicting land uses, visually screening uses from one another and minimizing the exchange of undesirable airborne particulate matter between incompatible land uses. (Note: Coniferous trees should be used in the buffer in situations where visual and particulate screening is required on a year round basis. Solution A.3a)

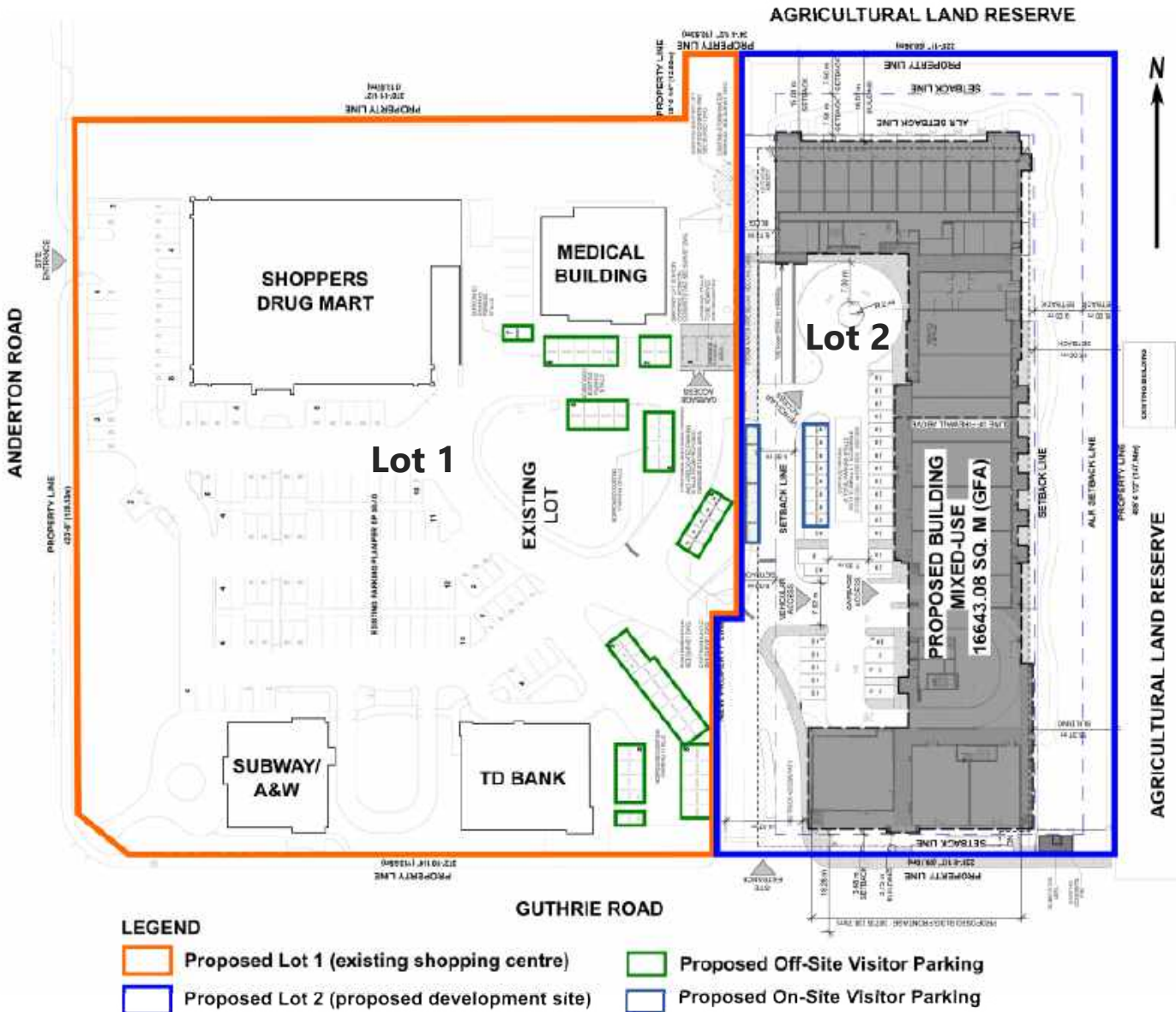


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A.3

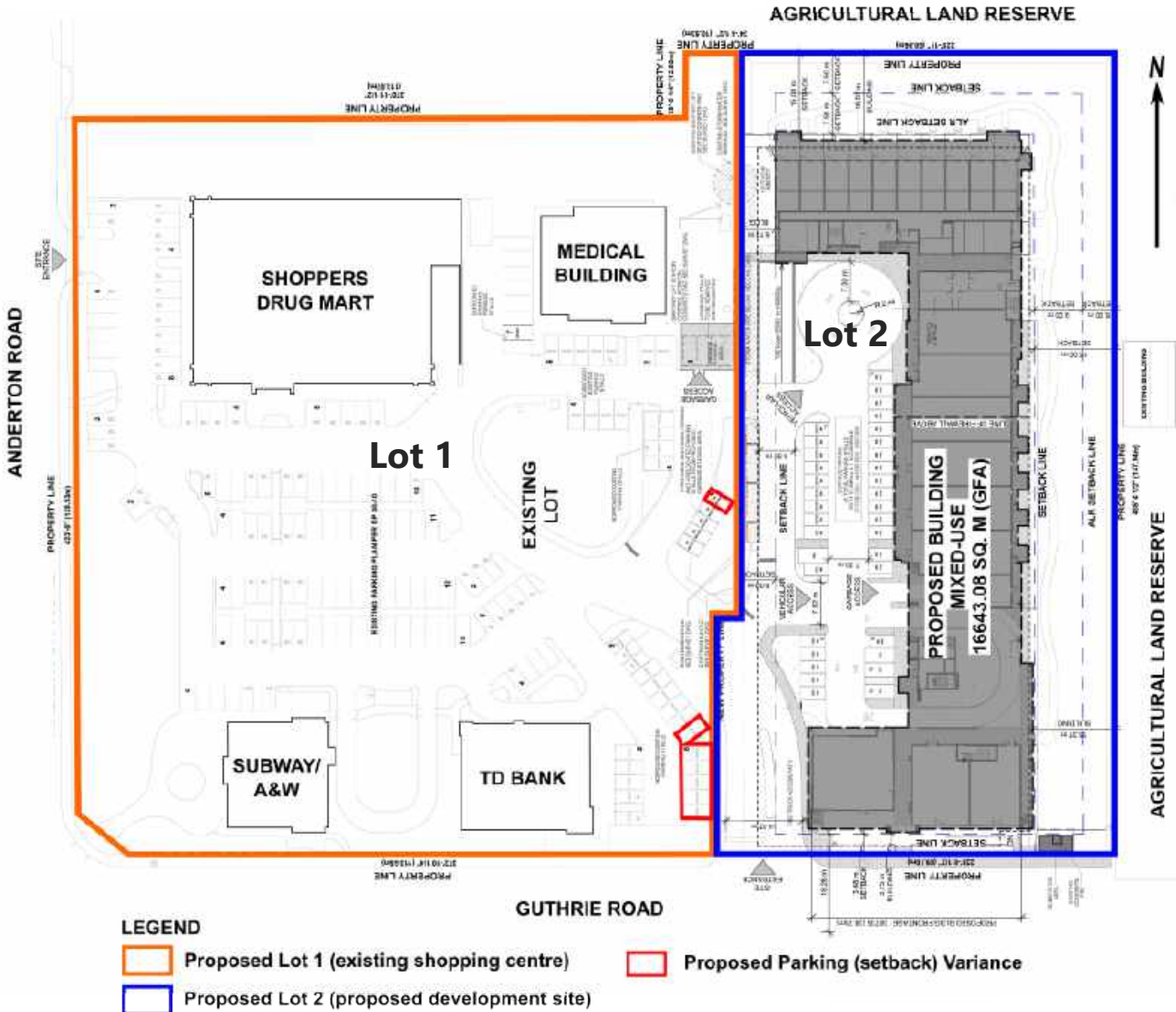
ATTACHMENT 14

Proposed Off-Site Parking Plan



ATTACHMENT 15

Lot 1 Parking Spaces Requiring Variance



ATTACHMENT 16

Tree Retention Policy CCL-067



TREE RETENTION POLICY

Section: COUNCIL	Number: CCL-067	Office of Primary Responsibility: CORPORATE SERVICES	
Type: <input checked="" type="checkbox"/> Policy <input checked="" type="checkbox"/> Procedure	Authority: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Administrative		Approved By: <input checked="" type="checkbox"/> Council <input type="checkbox"/> Chief Administrative Officer <input type="checkbox"/> Department Head
Date Adopted: February 16, 2022	Council Resolution No: 2022.071	Date to be Reviewed:	
Manner Issued: Website, Internal Memo, Upon request			

1 PURPOSE

1.01 The purpose of this policy is to provide staff direction and encourage Approving Officer consideration of securing long term retention and protection, or replacement and protection, of a minimum 30% target of existing trees taking into consideration site and development specific opportunities and constraints, including in relation to the number of existing trees retained vs replaced, tree species, habitat value, and community significance.

2 POLICY STATEMENT

2.01 Compact urban development at densities necessary to reduce greenhouse gas emissions and reduce development pressures on environmentally sensitive ecosystems, rural, agricultural and resource lands, alters the hydrology and reduces the amount of open space. This restricts the opportunity for tree retention and replacement to specific locations, resulting in a tree retention/replacement of significantly less than 30% of existing trees on highly treed sites. Consequently, the retention and planting of trees within road rights-of-way, greenways and parks is integral to achieving a target minimum of 30% tree retention/replanting.

2.02 Native trees provide important habitat for a variety of species including bald eagles.

2.03 Trees promote environmental sustainability by supporting passive heating and cooling, rainwater transpiration, evaporation and infiltration, and greenhouse gas reduction.

2.04 Trees are a major urban amenity in promoting social sustainability.

2.05 Key to long term tree retention is:

- (a) avoiding conflicts with buildings and above and below ground infrastructure; and
- (b) balancing the multiple and varying expectations of the public regarding both private and public open space.

2.06 Tree type, size, spacing and canopy are key determinants of impact and compatibility.

2.07 The Approving Officer:

- (a) is independent from Council; and
- (b) in forming an opinion about the public interest, the Approving Officer may interpret public policy as expressed in Town policy statements, but the Approving Officer must not take specific instruction from Council in regard to a subdivision application.

3 DEFINITIONS

- 3.01 “Greenfield Development” is development on land previously undeveloped for urban uses.
- 3.02 “Feature Tree” means a tree that grows to a minimum height of 9.0 m at maturity.
- 3.03 “Mid-Level Tree” means a tree that grows to approximately 4.5 m in height at maturity.

4 SCOPE

- 4.01 This policy applies to:
- (a) all lands within the Town, private and public, including road rights-of-way and parks; and
 - (b) infill development, redevelopment, and Greenfield Development.

5 POLICY

- 5.01 At time of development, the Town seeks a minimum 30% tree retention/replacement of existing trees of a diameter at 1.5 m above the tree base of 20.0 cm or more, and their long-term protection, based on parcel area net of road rights-of-way.
- 5.02 Existing tree and site-specific characteristics will be used to determine if the 30% applies to canopy cover or number of trees.
- 5.03 The focus of tree retention and replacement of large habitat and native trees will be in parks, greenways, environmentally protected areas, and large lot multi-family developments.
- 5.04 Selection of trees for retention/replacement will consider the impacts of climate change and the ability to support passive heating through winter sun penetration, and cooling by shading in summer.
- 5.05 The Approving Officer is encouraged to consider this policy in ensuring applications do not injuriously affect the established amenities of adjoining or reasonably adjacent properties and are not against the public interest. It is recognized that this policy may result in restrictions beyond those established by Town bylaws including building siting, setback, density and design, tree retention, and landscaping.

6 PROCEDURES

- 6.01 At time of rezoning and in application of relevant Development Permit guidelines, for multi-family, commercial, and institutional development:
- (a) the Town will seek tree retention/replacement and long-term protection along the periphery of the parcel;
 - (b) along street frontages, the Town will seek to create a tree canopy on the parcel comprised of Feature Trees and Mid-Level Trees to provide a continuous streetscape amenity and facilitate passive heating and cooling for both pedestrians and residents;
 - (c) the focus for Feature Tree location will be parcel corners, and primary vehicle and pedestrian access points. The number of Feature Trees on a parcel will be dependent upon the street frontage length of the parcel; and
 - (d) on large parcels, the Town will seek long term retention of existing trees, preferably in groves of trees on the periphery of the parcel, and customized buildable areas to facilitate tree retention.

6.02 At time of rezoning and in application of relevant development permit guidelines, for single and two-family dwellings, and industrial development:

- (a) the Town will seek retention and long-term protection of trees of community significance, including Garry Oak and eagle or heron nest trees, and along the periphery of the parcel; and
- (b) for single and two-family dwellings on large parcels, the Town will seek tree retention and long-term protection of existing trees, preferably in groves of trees on the periphery of the parcel and customized buildable areas to facilitate tree retention.

6.03 To offset less than 30% retention/replacement on treed parcels, to provide a continuous streetscape amenity, and to facilitate passive heating and cooling for both pedestrians and residents the Town will seek to:

- (a) retain and plant trees within road boulevards, parks, and greenways; and
- (b) retain, plant, and ensure long term protection of trees in excess of 30% of existing trees on sparsely treed development sites.

7 REFERENCES AND RELATED STATEMENTS OF POLICY AND PROCEDURE

Amendment Date	Section Amended or Description of Amendment	Resolution Number